

# THE COMMERCIAL MOTOR

FRIDAY, FEBRUARY 20, 1959  
ONE SHILLING

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- \* Vacuum-hydraulic or air-pressure brakes

*The middleweight with  
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ON and OFF  
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- 1** Safety Steel Shield — Over 40,000 steel threads protect the vital tread area from cuts, ruptures, groove cracking, separation. Provides unequalled heat dissipation. Assures remould after remould.
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- 6** New Multi-Edged Tread Design — Sharp biting edges increase highway skid control, provide positive traction.
- 7** Deep, Wide Shoulder Lugs — Dig in for positive traction off the road. Reduce running temperatures on the road.
- 8** Work-Moulded Contour — Carcass moulded to inflated shape. Works relaxed, without "inflation strain".

## Act now—delay costs money!

Remember that 90% of tyre troubles occur in the last 10% of tread wear. Tyre failures, tyre repairs and truck downtime pile expense upon your operating costs. Start your trucks rolling on U.S. Royal Super Fleetmasters and start saving right away!

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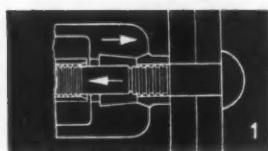
# REVOLUTION IN COACHBUILDING



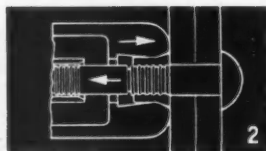
HUCKBOLTS speed production on the A-E-C/Harrington 'Indiaman'.



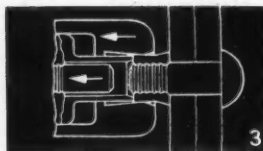
When Thomas Harrington Ltd., constructed coachwork for the 'New Indiaman' they used HUCKBOLTS the revolutionary new fasteners. HUCKBOLTS were specifically chosen to withstand the arduous conditions which the coach would encounter on its journey through Europe and Asia. Huckbolts give positive locking with high clench and sealing properties; what's more, they can be fixed at speeds up to 30 bolts per minute by unskilled labour. The bolt driving is automatic, each fastening a quality job.



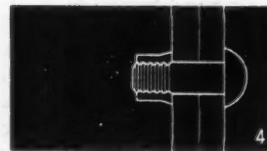
High clenching action clamps sheets together.



Collar swaged and locked into grooves.



Bolt breaks at breakneck groove.



Installed lock bolt.

## HUCKBOLT\*

**FASTENERS**

AVIATION DEVELOPMENTS LIMITED  
Kingsbourne House · 229/231 High Holborn · W.C.1 · Tel: CHAncery 8601

\* Manufactured under licence from the Huck Manufacturing Co. of Detroit, U.S.A. Huck Patents 629098, 766137, 790785

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CMF

(above)  
oil engine

ERF

Telephone

# HEAVYWEIGHT CHAMPIONS



*supreme for big loads  
and bulky loads*



(above). Typical example of a tanker on the ERF 68G chassis for bulk transport of glucose.



(right). Another ERF 68G with special containers for a particular industrial need.

Here is just a small selection of the many ERF Models 68G which are now in operation all over the world. Wherever there are big loads and bulky loads to be carried there is no more efficient and economical way than with the ERF 68G. This strong, powerful chassis is suitable for a wide selection of specialised bodywork to meet all requirements. When you are considering transport in the biggest sizes—write for full details of the ERF 68G.



(above). The ERF 68G shown here, powered by Gardner 6-cylinder oil engine, is another big-capacity, roomy vehicle on this chassis.

(below). This well-known company operates many ERF vehicles. One of them, a 3,200-gallon articulated frameless tanker with ERF 64G chassis is shown here.



**ERF LIMITED • SUN WORKS • SANDBACH • CHESHIRE**

Directors: D. FODEN

E. P. FODEN

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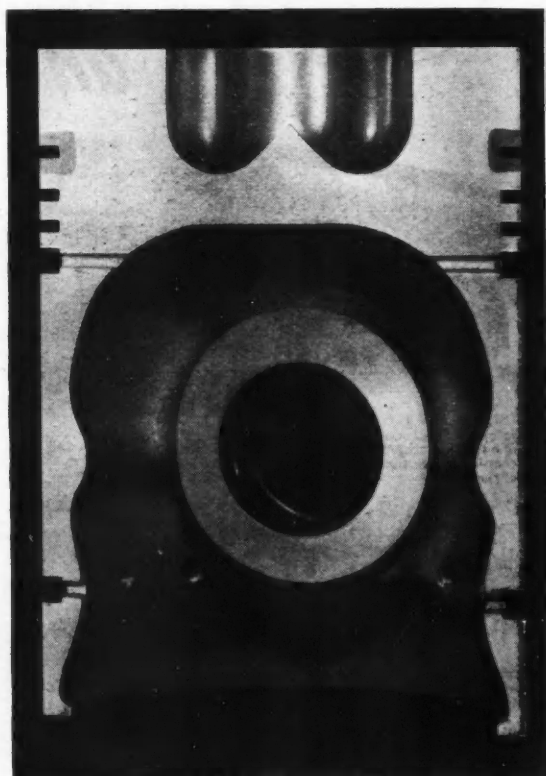
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# Wellworthy **AL** **FIN** armoured ring groove pistons

*... more than pay  
for themselves  
in the first year!*

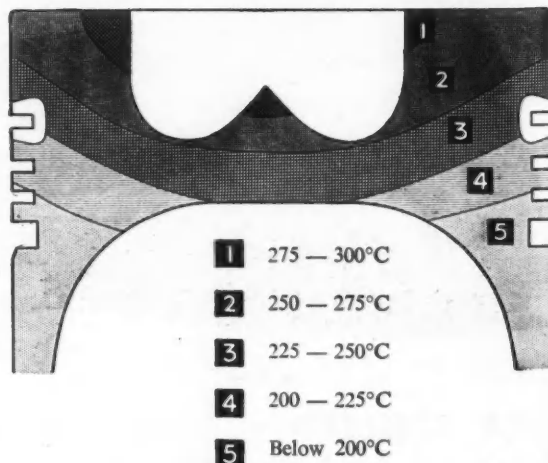
The top ring groove of the Al-Fin Piston has an austenitic cast iron insert bonded to the alloy giving strength where it is most required. This iron bonded securely to the alloy during the casting of the piston will stand up to the most severe usage. That means piston life is increased by at least 100%. This double mileage saves you the cost of one overhaul *and* the cost of another set of pistons! Write for leaflet D 20/2.



## Isothermal Piston Design.....

Wellworthy use Isothermal Survey in diesel engine piston design to ensure:

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- Determination of correct running clearances.
- Avoidance of localised hot areas.
- Correct gudgeon pin alignment.
- That head thickness, coupled with correct blending into piston wall overcomes distortion of ring grooves. This has considerable bearing on blow-by and efficient oil control.



**WELLWORTHY LIMITED · LYMINGTON · HANTS**



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# REFRIGERATED VEHICLES



- Leyland "Comet" Chassis.
- Body built throughout in Homalloy Light Alloy section and plate.
- 5" thick all-round insulation on walls and roof.
- One-piece waterproof floor, over 7" insulation.
- 18" wide roller conveyor runs full length of body.
- Overall dimensions 19' 10" x 7' 6" x 10' 9½" high.

**THIS REFRIGERATED  
VEHICLE BODY  
WAS BUILT TO  
OPERATE AT**

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**SPECIALISE IN  
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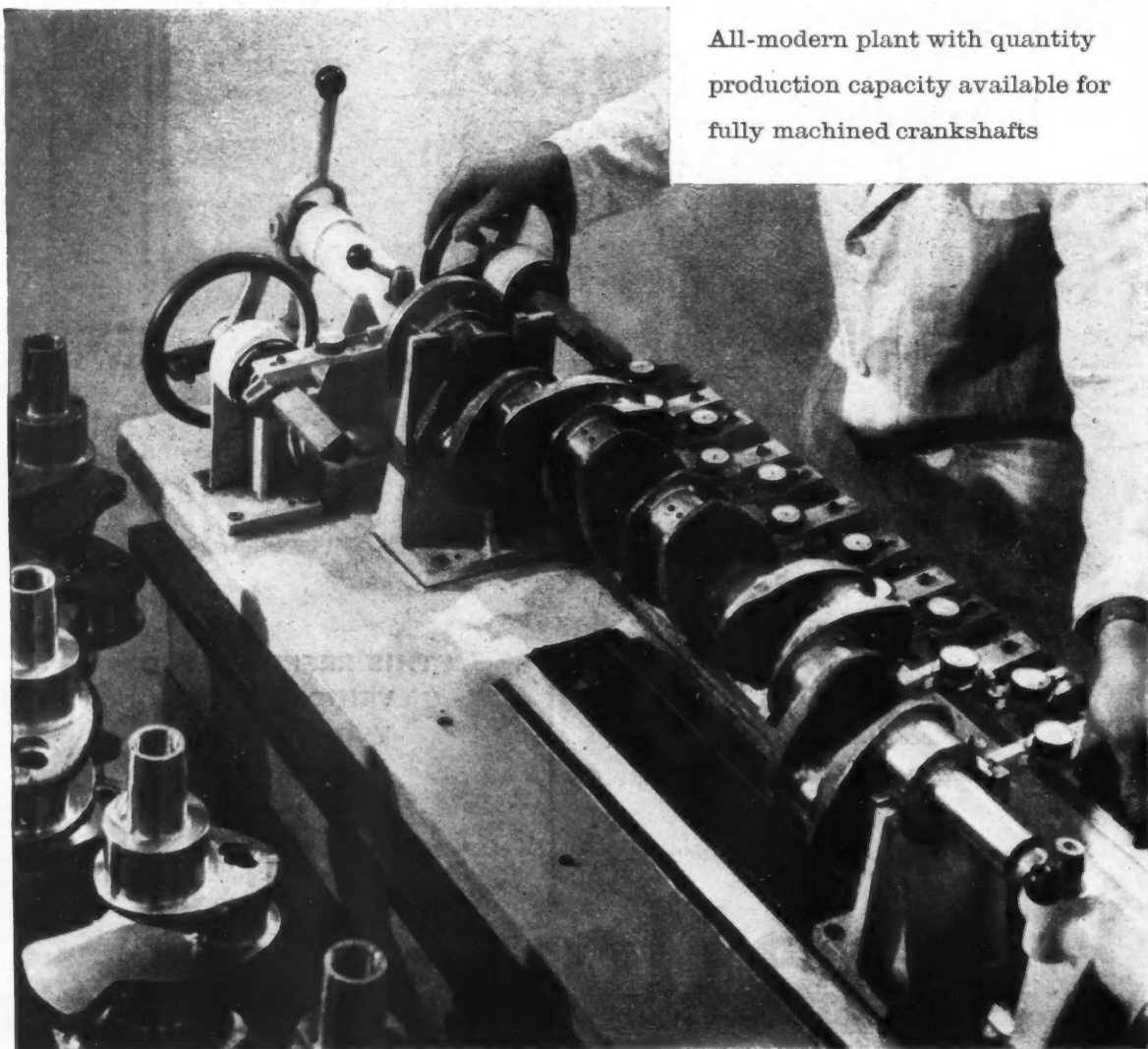
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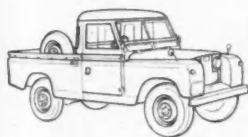
Below are some of the 9 styles of rust-proof alloy body available in the Land-Rover range



Regular Land-Rover  
—canvas top



Regular Land-Rover  
—hard top



Long Land-Rover  
—truck cab



Long Land-Rover  
Station Wagon

- \* Reliability and toughness born of eleven years' practical experience in all conditions and climates.
- \* A light-weight, non-corrodible, alloy body that is completely rust-proof.
- \* A choice of engines—Petrol or Diesel.
- \* A choice of chassis lengths—Regular or Long.
- \* A choice of 9 body styles, 8 colours and 76 optional extras for specialised duties.
- \* A draught-proof canvas hood or truck cab, both with rubber-sealed doors and sliding glass windows.
- \* Ample ground clearance over rough country.
- \* **AND SUCH BRILLIANT PERFORMANCE.** The new 2½ litre petrol engine developing 77 b.h.p. gives vivid acceleration, a road speed of over 70 m.p.h and a maximum drawbar pull of 4,000 lbs. Recent developments of the Rover Diesel have produced an engine which is outstanding for its economy, reliability and quiet running.

There's no substitute for the  
4-wheel drive Petrol or Diesel

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ROVER**



By Appointment  
to Her Majesty Queen Elizabeth II  
Manufacturers of Land-Rovers.  
The Rover Co. Ltd.

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and DEVONSHIRE HOUSE · PICCADILLY · LONDON



Albion "Clydesdale" chassis cab with Sparshatt patent insulated van body and self-contained refrigerator unit.

**Times Food Manufacturing Co. Ltd., manufacturers of "TEMPO" quick frozen dairy cream sponges, operate this large capacity refrigerated van for bulk deliveries.**

(INSULATED CONTAINER TO PATENT No. 6310/57)

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**Taskers** latest advance in semi-trailer design . . . the outstanding

## DOUBLE SAFETY D-S

### Automatic COUPLING

(World Patents Applied for)

#### SEMI-TRAILER PORTION

Can be used with prime movers already fitted with existing types of automatic gear, or can be fitted as a replacement for existing coupling gears. Double-Safety features include:—

1. Two positive safety locks to prevent collapse.
  2. Auxiliary support legs.
- Also incorporates replaceable king-pin; hand-brake with spring-loaded ratchet preventing accidental release; graphitized fabric with gutter to obviate entry of grit.

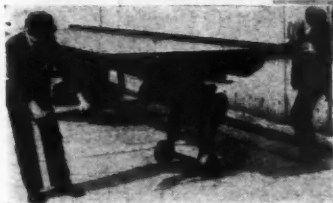
#### PRIME MOVER PORTION

Can be operated with semis already fitted with existing types of automatic gear.

Coupling hooks operate downwards, completely securing load rollers which cannot escape accidentally. Double locks are incorporated on coupling cams. Automatic safety catch is fitted to release lever. Brake movement ratios match-up with other makes of automatic coupling, where applicable.

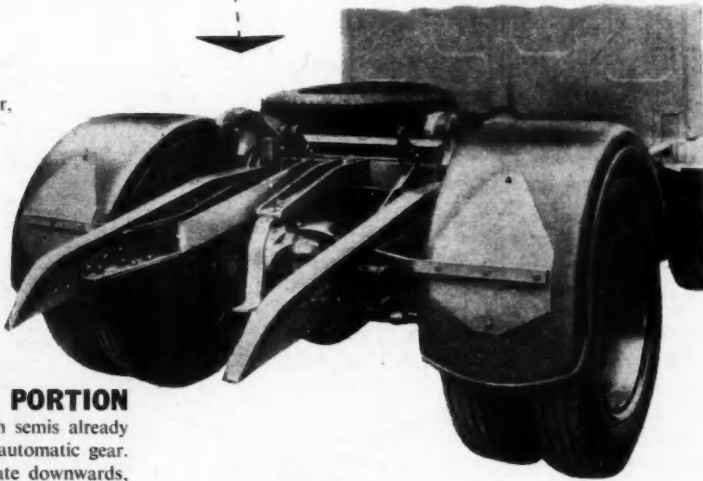
#### DOUBLE SAFETY

Auxiliary legs prevent absolutely any collapse further than the artificially-produced position shown here.



**EASE OF HANDLING**  
and degree of turn . . . steering of detached semi-trailer to well over 90 degrees lock is easily accomplished by steering bar.

**WRITE FOR PUBLICATION No. 1210**



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**TASKERS OF ANDOVER (1932) LTD.**

ANDOVER, HANTS. Telephone: Andover 2312

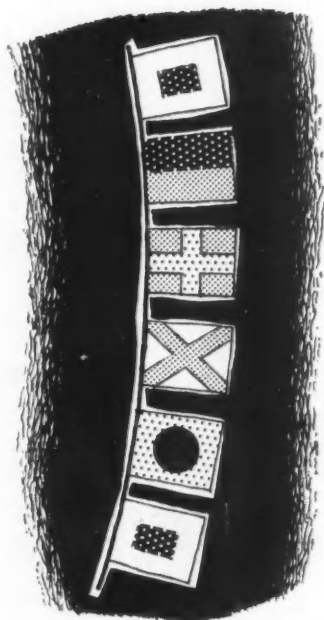
Telex: Andover 47-539, Manchester 66-249.

London Office: 36 Victoria Street, S.W.1.

Telephone: ABBey 2202.

Manchester Office: 26 Corporation Street,

Manchester, 4. Telephone: Deansgate 6009.

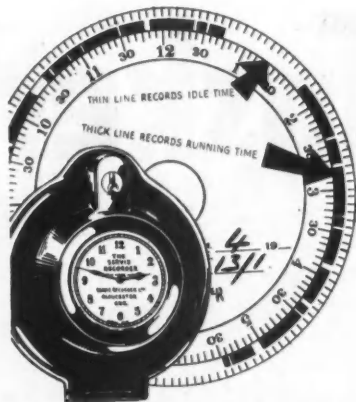


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Dependable 'intelligence' is as vital for the control of fleets on land as at sea. The transport manager needs the exact information which Servis Recorders provide — when and for how long each vehicle in his fleet is at work every day.

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service is quick and reliable in all sizes of Giant and Commercial tyres with treads for mileage and traction.

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**ONDURA**  
*deeper-tread*  
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*Look out*  
ice on the roads



## BONALLACK refrigerated bodies keep food—safe

Spoiled food is not only bad for health but bad for business, and is causing the attention of the food industry to be focused increasingly on the question of refrigerated transport.

A typical example of BONALLACK refrigerated vehicles is illustrated above. It was built to the order of Monument Foods Ltd., of 16 Monument Street, E.C.3 and the interior design is to their own exact requirements.

The light alloy body is mounted on a B.M.C. forward control type chassis, and the fully insulated load compartment has the famous BONALLACK patent interlocking Dekaloy floor, complying with the latest Ministry of Food regulations.

The construction of insulated bodywork is a highly specialised field, posing a host of difficult problems, particularly the elimination of metal to metal contact in order to ensure as near perfect insulation as possible. A clean and hygienic interior that will not in course of time become foul is another

essential. In all this BONALLACK have a wealth of practical experience and their refrigerated vehicles are widely distributed both at home and abroad—many of them operating under exacting tropical conditions.

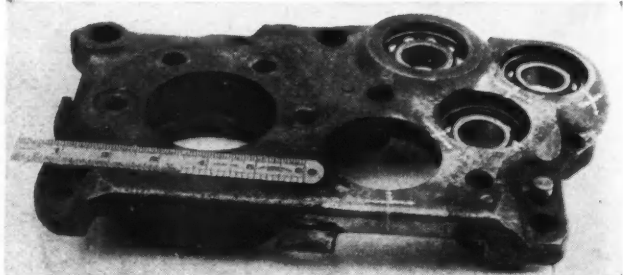
Refrigerated, insulated, or just plain box van or, for that matter, anything in the transport field from Tanker to Television Van, from Flat Platform to Fire Engine, whatever the particular requirements of your business, BONALLACK experience for over 130 years combined with the largest and most progressive plant in Europe ensure the vehicle you need at the most economic price.

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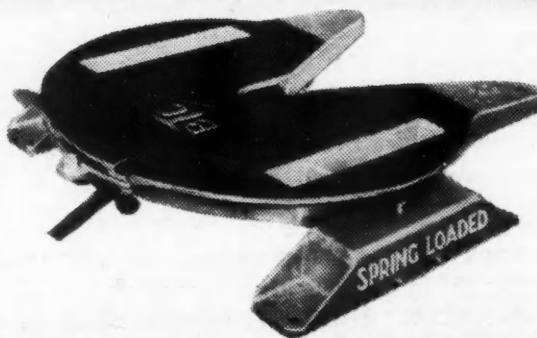


Positive, Safe, Single-handed release action. Full width large diameter Cross-shaft. Can be fitted to any make of Popular Type Truck.



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**LESS  
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## VACROM

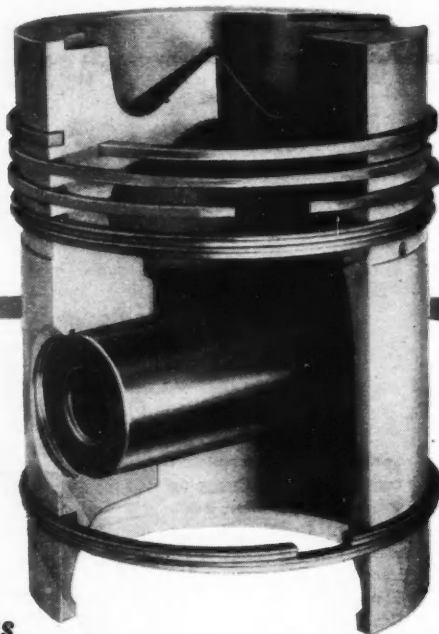
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KSS OIL CONTROL RINGS consist of two identical segments, chromium plated on the periphery to resist wear. The two segments are completely independent of each other, thus do not lift simultaneously from the cylinder wall by piston rock or hydraulic force. Oil control efficiency is double that of the equivalent orthodox grooved and slotted rings.



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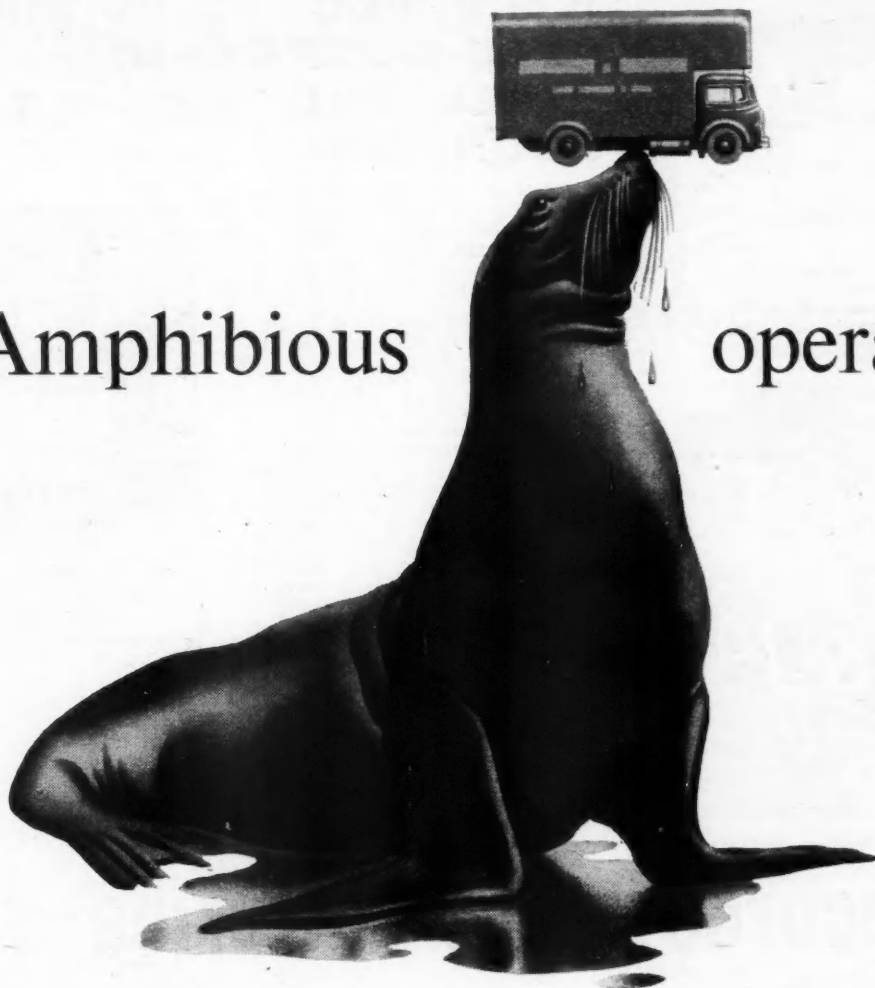


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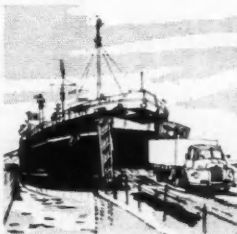
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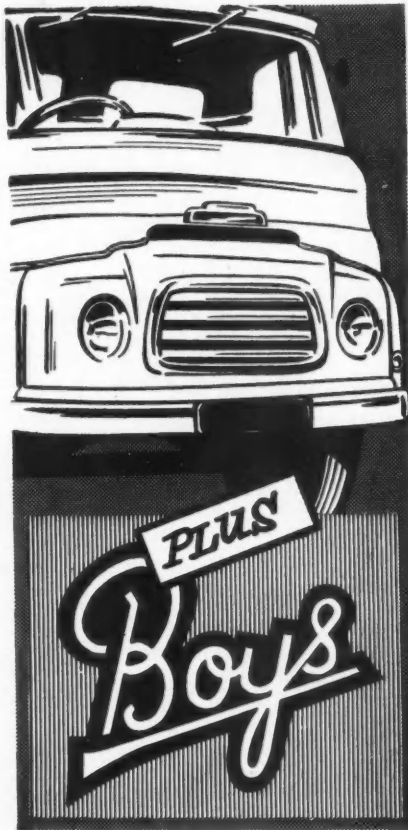
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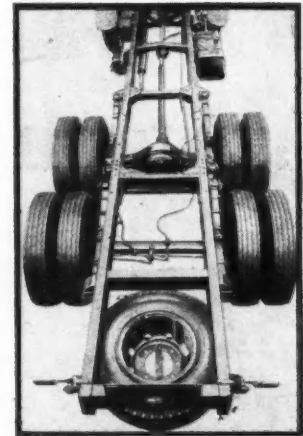
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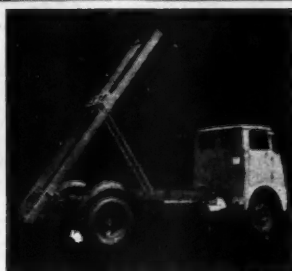


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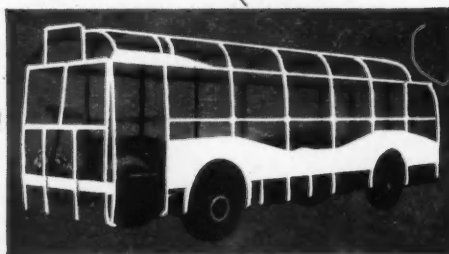
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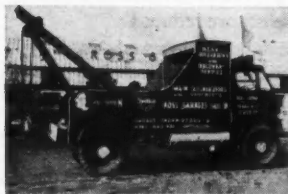
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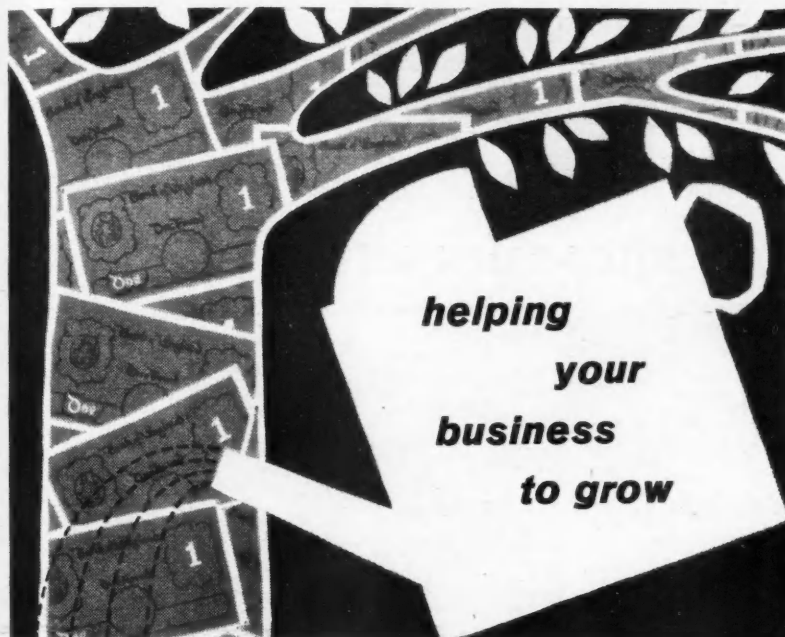
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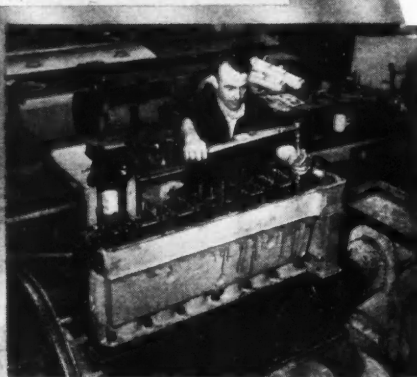
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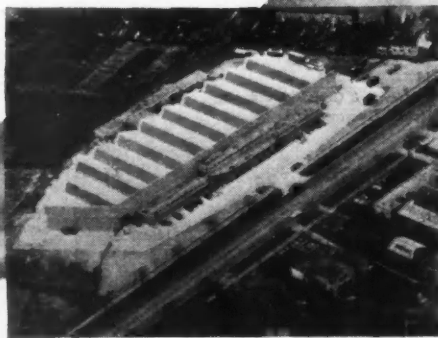
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### ROUND THE YEAR . . .

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(Centre) Leyland's re-conditioned engine exchange scheme is an important activity of their service. Every exchange unit carries the same guarantee as a new unit.

(Left) An aerial view of Leyland's newest service depot at Oldbury near Birmingham which alone carries £½ million stock of spares.

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journal. Comment and brief extracts  
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## Vapour Trails

**T**HE relationship between atmospheric pollution and lung cancer is again in the news. Sir Walter Fergusson Hannay, an eminent consulting physician, writing in the January 30 issue of *The Petroleum Times*, places part of the blame on the oil-engined road vehicle.

On the other hand, investigations by Prof. F. C. Pybus, of Durham University, who has been engaged in cancer research for more than 30 years, suggest that smoke from garden bonfires may be a contributory factor. In a recent test he found that such smoke contained 70 parts of benzpyrene—a substance known to cause cancer—per million of free carbon.

He expressed the belief that 90 per cent. of cancer was caused by benzpyrene and other carcinogens in the atmosphere. It is significant that the 180m. tons of coal burned annually produce 2½m. tons of smoke and 750 tons of benzpyrene.

By these standards, the contribution to the dangerous pollution of the atmosphere made by motor vehicles must be small indeed. Why the oil engine rather than the petrol unit should be singled out for attack is not certain. Mr. Arnold Fitton, principal scientific officer of the Fuel Research Station, affirmed at a conference organized in 1957 by the Institution of Mechanical Engineers that under varying conditions combustion in oil engines was better than in petrol engines, and caused less atmospheric pollution.

### What the Eye Sees

The reason for popular antagonism towards the oil engine is possibly that when it is poorly maintained the fumes emitted are not only acrid but visible, and create a nuisance. There is, however, a big difference between fumes that are unpleasant and those that cause death, and so far there is no conclusive proof that even dense clouds of oil-engine smoke are anything more than an inconvenience.

Sir Walter certainly does not supply it. He quotes from reports on the subject—not all favourable to his argument—but does not adduce a scrap of new or original evidence. His expressed "hope" of showing that the increase of pollution by diesel fumes could be a contributory factor in the cause of cancer implies a prejudice which his pleading fails to justify.

### *Prejudice Stimulates the Unproved Theory that Oil- engine Smoke Causes Cancer*

He quotes the view of the Beaver Report, published in 1954, that there was increased pollution of the atmosphere by motor traffic and that smoke from diesel exhausts could be a serious "nuisance." The report did not rate the effects of exhaust fumes higher than that.

Obviously, the growth of internal-combustion-engined vehicles must be accompanied by an increase in air pollution, but there is no proof that it will endanger health. Sir Walter does not define the quantity of benzpyrene in oil-engine fumes, but merely describes it as "considerable"—a word that can mean anything or nothing, and has no place in scientific argument.

He finally destroys confidence in his case by the statement: "As a result of the recent bus strike . . . Londoners have become aware of the ever-increasing menace of air pollution from diesel fumes, and have recognized the resultant sapping of their physical and mental energies."

### An Airy Assertion

This observation is made without any supporting authority and can be dismissed as wishful thinking. As everyone knows, since the strike the number of buses in use in London has been substantially reduced, and their contribution to air pollution has been diminished accordingly. Their withdrawal has been made necessary by the increasing use of private petrol-engined vehicles, which, according to Mr. Fitton, cause greater atmospheric pollution than do oil-engined buses.

Investigations, made by a group associated with the Medical Research Council, into air pollution in London bus garages showed that there was no excess of lung cancer among London Transport staff, such as would have been expected if oil-engine exhausts seriously contributed to cancer.

Earlier, Dr. Richard Doll and Prof. Bradford Hill, who had been conducting research into the causes of lung cancer for seven years, reported that the proportion of cancer patients who drove vehicles and worked on the roads or in garages was no greater than that of persons in other occupations. Association with motor vehicles, they found, appeared to carry no specific risk of lung cancer, and it seemed unreasonable to them to attribute to exhaust vapours the high rate

of mortality from this disease in towns. Their findings were supported by research conducted in America, which had failed to establish an excessively high proportion of lung-cancer patients among men engaged in the transport industry.

The nuisance caused by excessive smoke from oil

engines cannot be denied and, as the Beaver Committee recommended, the law on the subject should be rigorously enforced, in the interests not only of the public in general but of good transport operators in particular. It can be avoided by careful maintenance and the proper instruction of drivers.

## Tyres a Delicate Problem

INVESTIGATIONS conducted by *The Commercial Motor* show that vehicle manufacturers and operators agree that tyres are potentially the biggest problem in the effective use of Britain's future motorways. The main difficulty is to prevent the building up of heat during sustained fast travel.

At moderate speeds, tyre wear is likely to be reduced because of the relative infrequency of braking and the absence of scuffing when cornering. Above the critical limit of speed, wear will be rapid and blow-outs will occur unless a suitable cover is used.

There is ample evidence to support these contentions. For two years an engine manufacturer has been running 7-tonners at high speed for nearly eight hours a day, five days a week, on the Motor Industry Research Association's proving ground. Cruising speeds of 55-60 m.p.h. have been maintained to produce a 50 m.p.h. average.

In the early stages of the tests and during hot weather, tyres were apt to burst every four hours. By adopting a more suitable tyre, mileage life has been extended from about 10,000 to 20,000, and the average rate of bursts is now one in nine months.

Other phenomena revealed are frequent rim cracking and stud fractures, thought to be caused by centrifugal overloading. Obviously, wheels and tyres are among the controlling influences on safety on motorways and demand urgent attention.

The delicacy of the problem is emphasized by the difficulties which the Tyre Manufacturers' Conference have tried to place in the way of *The Commercial Motor*

in investigating it. After a representative of this journal had communicated individually with all the leading tyre makers he received a message from the Conference (whose views had not been asked) that "the subject can best be dealt with on an industry basis, and steps to this end are being taken."

A big manufacturer who had arranged an interview to discuss the matter then said that the visit would have to be approved by the T.M.C.—an extraordinary sacrifice of liberty—and a visit to another maker suddenly became mysteriously inconvenient. According to the Conference, there were several "legal aspects" regarding tyres to be fitted to vehicles to be used on motorways, and they asked for a draft of any article on the subject to be submitted to them before publication.

This demand was refused. *The Commercial Motor* does not accept censorship by any interest, vested or otherwise, and upholds the right to collate opinions and comment on any matter of public importance. The suppression of free discussion of technical and other subjects is the most repugnant form of restrictive practice, and this journal will not be a party to it.

The article on the whole question of the use of motorways, which appears in this issue, abuses no confidences, but is of vital concern to every operator. If the readers in whose interests it has been prepared are satisfied, *The Commercial Motor* will be amply rewarded.

## Passing Comments

### F.B.I. Examines Industrial Trends

A FOURTH inquiry into the condition of British trade, covering four months, is reported by the Federation of British Industries. Replies from executives of companies broadly representative of the different industries and of large and small firms, numbered 675. They show that there is some foundation for the earlier belief that there would soon be an improvement in conditions, and for the first time since the scheme was instituted the numbers reporting an increase in the rate of new orders slightly outweigh those reporting decreases. This should indicate better times for hauliers, for improvements in trade inevitably provide more and bigger loads.

During two previous inquiries declines in the levels of output were reported. Now the tide seems to have turned, despite the present slight rise in unemployment. There have, however, been further declines in new orders for export, but it must be remembered that this covers the industry as a whole, not necessarily the commercial-vehicle side, which shows trends materially better than average.

B28

### Keeping Castrol in the Van

SUCCESS in developing lubricants for the particular uses to which they are to be put must depend to a great extent upon extensive research and development work. As an example of this, in the past 10 years these activities have risen 300 per cent. at the Castrol laboratories at Hayes, Middx.

Further to increase them a large extension, equipped with the most modern apparatus employed for research on lubricants, has been opened by C. C. Wakefield and Co., Ltd. There is a new spectrographic laboratory which includes apparatus for analysing the metallic constituents of oil samples, whilst in the radio-chemical department research is conducted with the aid of radioactive isotopes. Equipment and services have also been provided for more routine work in this field.

In the organic laboratory each chemist has his own supply of compressed air, gas, vacuum and nitrogen, whilst a high-velocity extraction system removes odorous fumes at bench level.

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### Striking an Old Note

It is often said that truth is stranger than fiction, but who would believe that a 10-shilling note would be discovered vulcanized into a commercial-vehicle tyre? This is, however, exactly what happened recently at the Dundee branch of Tyres (Scotland), Ltd. A member of their fitting staff, dealing with a Goodyear Hi-Miler truck tyre, noticed within the cover in the bead area something which he thought to be a stamp. Further examination showed it to be the note in question almost completely moulded into the casing and covered by a thin film of rubber. Its removal, however, showed that this would have no effect on the tyre's potential life, although it had lost something of its value!

On the note being returned to the Goodyear concern at Wolverhampton an inquiry revealed that a "bagger-upper" (a good one for What's My Line!) in the vulcanizing department, Mr. R. Benton, had missed the note about a year before. At that time he was moving stocks of "green" tyres prior to their vulcanization, and the note, which had been earmarked to pay a bill on his way home, must have fallen from his overall pocket.

It is understood that as this was a chance in many millions, searches amongst other tyres are unlikely to prove remunerative.

### Geneva as a Meeting Place

APART from a considerable growth each year the Geneva Motor Show is becoming a focus for quite a number of associations in respect of their annual meetings. As examples, the European Coachbuilders Union is holding



Mr. Ray Benton (left), a Goodyear "bagger-upper," thanks Mr. F. Tompson (production manager, tyres and tubes) for the return of his lost 10-shilling note.

there its event of this type on March 17 and 18. This will include delegates from Great Britain. Also, the Bureau Permanent International des Constructeurs d'Automobiles is organizing a meeting of the directors of all European motor shows, where, for the first time, they will be afforded the opportunity of discussing the many problems concerned with these.

Some British and foreign manufacturers use Geneva as a venue where their executives and members of the Press can meet their Swiss agents and representatives.

## One Hears—

That there are almost enough drivers' "log" cases to start a timberyard.

That the condition of flues may be largely responsible for the 'flu epidemic following smog.

Of an imaginative reader saying that covering Regent's Park with Triplex glass would at least keep people off the grass.

That the incidence of attacks on oil-engine exhausts makes them appear suspiciously like a campaign by other interests.

From a leading Japanese industrial designer concerned with anything from rice-polishing machines to motor vehicles, that he spends 30 per cent. of his time on design work and 70 per cent. in selling the idea of industrial design.

That Russia is planning to build 43,500 miles of new roads in the next seven years.

That the longest will be a 620-mile highway between Moscow and Leningrad.

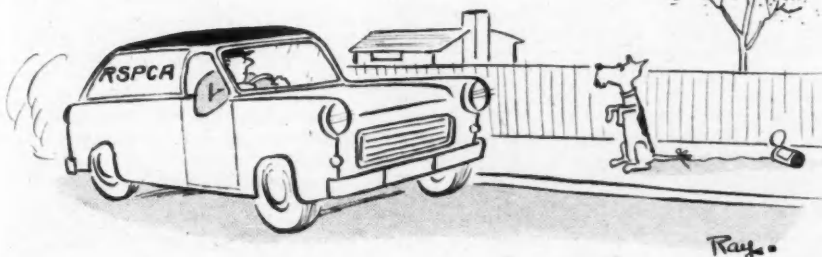
That most will be concrete or cement surfaced with four traffic lanes having green strips separating the carriageways.

That automatic chassis lubrication ensures freedom from "dry" working, reduces running maintenance and saves time wastage in the repair shop.

Regarding an American scientist, that for each month of laboratory work he spends a year on a full-scale report, a paper for a scientific society, half a dozen speeches and contributions to the Press.

That the fatigue life of coil springs made from vacuum-melted steel wire is twice as great as when piano wire is used.

That this is because there are fewer and smaller "inclusions" to interrupt the structural pattern.



# Big Changes in Vehicles Demand Licence Variation

BY A SPECIAL CORRESPONDENT

**L**ICENSING AUTHORITIES are becoming concerned about the effects of substantial increases in the unladen weights of vehicles brought about by alterations. Both Mr. J. A. T. Hanlon, Northern Licensing Authority, and Mr. W. P. James, West Midland Licensing Authority, have, during the past week, suggested that if a vehicle is altered so that its weight is greatly increased, it is not the vehicle originally specified in the licence.

On Monday, at a sitting at Newcastle upon Tyne, Mr. Hanlon recommended operators who made extensive changes to existing A-licence vehicles to apply for the variation of their licences.

If there was a material change in carrying capacity or in type, it could not be said to be the same vehicle, although no hard-and-fast rules could be laid down, he observed. Merely because the 1933 Act did not define the circumstances, a Licensing Authority was not prevented from exercising his discretion in deciding whether the vehicle was the same as originally specified.

The contention that, if it had the same registration number it must be the same vehicle, was nonsense. A complete change could be made while retaining the original registration.

Mr. Hanlon's decision to publish as an A-licence variation an application by S. Jewett and Son, Ltd., Stocksfield, to increase the unladen weight of three existing vehicles on A licence, was challenged by the Road Haulage Association.

Mr. Frank Milton, Northern Area secretary, said the company had proposed to convert the three vehicles from four-wheelers into either six- or eight-wheelers and had written to the Licensing Authority for guidance last October. The application was published as a variation and drivers' records and registration books were requested. It had now

been decided not to proceed with the conversions, as the cost would be prohibitive, and an application for new vehicles would be submitted.

## Test Case Wanted

They were seeking guidance, added Mr. Milton, as the Association's legal advisers believed there was nothing in the Act or the regulations which said a variation application for the same vehicle could be made. This had been pointed out to the Licensing Authority's office in December, but they were still waiting for a reply. It might be necessary to go elsewhere for a decision and they would like a test case.

Mr. Hanlon said he was satisfied that, in certain circumstances, an existing vehicle could no longer be regarded as the one originally specified on the licence. The private haulier was not being dealt with any differently from the British Transport Commission, whose applications were receiving the same scrutiny. On the question of log sheets, Mr. Hanlon considered them to be the best guide to normal user.

[Mr. James' comments are reported on page 74.]

## Take-over Refused: Licence Revoked

**T**HE purchase of an A-licence vehicle by turning the business into a limited company, acquiring the shares and then operating it as part of the purchaser's existing fleet without disclosing the change of user, resulted in the refusal of a section 11 (3) (b) take-over application and the revocation of the original licence by the Northern Licensing Authority, Mr. J. A. T. Hanlon, at Newcastle upon Tyne on Monday.

Mr. J. Croft, for the applicants, J. W. Snee, Ltd., Newcastle upon Tyne, who were seeking to take over an A-licence vehicle in the name of William Parker (Haulage), Ltd., said the Parker business was turned into a limited company in November, 1957, when Mr. Parker's health became bad. Shortly afterwards Snee's acquired the shares.

The vehicle had operated with their fleet of 18 lorries for the last 11 months of 1958 and the figures for the year showed earnings of more than £3,300. There had been a big change from the previous normal user, "mainly civil-

engineering contractors' plant and equipment, Tyneside, Wearside and Teesside," but the same type of goods had been carried.

Mr. J. W. Snee said that in the early part of 1958 the vehicle had worked for the same customers as previously, although under his company's control. During the last quarter, however, 90 per cent. of its work had been for Domestos, Ltd.

Questioned by Mr. Hanlon, he agreed that he was managing director of both companies and had signed the Parker application form stating the normal user in December, 1957.

Refusing the take-over application, Mr. Hanlon said the statement of intention signed on behalf of William Parker (Haulage), Ltd., by Mr. Snee in December, 1957, was false and the Parker A licence would be revoked.

An application by Mr. Croft for the revocation to be suspended until the expiration of the time allowed for appeal was granted.

## Gen. Russell to be Member of B.T.C.

**W**HEN Lord Rusholme retires from his position of full-time member of the British Transport Commission on September 30, he will be replaced by Maj.-Gen. G. N. Russell, chairman of British Road Services. Gen. Russell is this year's president of the Institute of Transport.

Mr. K. W. C. Grand, general manager of the Western Region of British Railways, has been appointed whole-time member of the Commission to fill the vacancy caused by the death of Mr. J. W. Watkins.

Whole-time members of the B.T.C. receive £7,500 a year.

## UNITED CANNERS SWITCH TO RAIL

**I**N implementing plans to distribute some 20,000 tons of canned products throughout the Greater London area, Beaulah Benedict Sales, Ltd., are to switch deliveries from their cannery at Boston, Lincs, from road to rail.

The factory, United Cannery, was previously served by the company's own lorries and those of local haulage concerns. New plans, however, include the use of a floor of the East Smithfield goods depot, which is railway property.

Here, the contents of a dozen wagons will be delivered daily, returning to Lincolnshire with loads of imported fruit. Other imported consignments for the Boston factory will be landed at Immingham and conveyed by rail to the cannery.

## "R.H.A. LEAVE RAILWAYS AND B.R.S. TO OBJECT"

**A** WITNESS from British Road Services told Maj. F. S. Eastwood, Yorkshire Licensing Authority, at Leeds, last week, that the Road Haulage Association were leaving the railways and B.R.S. to "carry the guns" and object to licence applications by private-enterprise operators.

He made this statement when Mr. J. Walker asked him why no private haulier had objected to an application by B. Booth, Ltd., East Morton, Keighley, for an A-licence vehicle. The application was refused.

[A previous hearing of the application was reported in *The Commercial Motor* on October 17, 1958.]

## PERKINS DEAL UNCONDITIONAL

**T**HE offer made on behalf of Massey-Ferguson Holdings, Ltd., for the share capital of F. Perkins, Ltd., has now become unconditional. It has been accepted by holders of 89 per cent. of the shares. A price of 17s. 3d. has been offered for each of the 5.2m. Perkins 10s. ordinary shares.

## "TAX OIL: CUT PETROL DUTY"

**T**HE Yorkshire area council of the National Union of Mineworkers on Monday agreed to press for a 5 per cent. tax on fuel oil and a similar reduction in the duty on petrol. An almost similar proposal was made by *The Commercial Motor* on January 16.

## Little Opposition by B.T.C. to Special-A Licence "Renewals"

BY A SPECIAL CORRESPONDENT

**C**OLLABORATION between the Road Haulage Association and the Licensing Authority in the North Western Traffic Area, in ensuring that proper figures and other information are presented by applicants when applying for new A licences upon the expiry of special-A licences, has resulted in excellent progress. The expected challenge by the British Transport Commission to wide normal users appears to have developed into a damp squib, and objections have been the exception rather than the rule.

At Manchester, last week, 11 of these transfers, either without objection or following agreement at the road and rail negotiating committee, were granted.

One surprising feature was British Railways' failure to challenge normal users tantamount to "general goods, Great Britain" in applications by Cusick Transport, Ltd., F. Rose, Ltd., Blackrod, Harris Road Services, Ltd., and Smith's of Eccles, Ltd.

Mr. J. A. Dunkerley submitted on behalf of Cusick Transport, who sought to add two special-A vehicles of 7½ tons unladen to an A licence, that their business, with offices in many parts of Great Britain, was countrywide. The North Western fleet had to be interchangeable with vehicles licensed in the Northern, Metropolitan and Scottish Areas, and sub-contracting was enormous.

The operations of Harris Road Services, Smith's of Eccles, and F. Rose were also described as countrywide, involving every description of goods.

During the proceedings, the North Western Licensing Authority, Mr. F. Williamson, pointed out that where in a small fleet the existing normal user on an A licence differed from the one sought on a new licence although all the vehicles were working together, steps should be taken to put the original user right.

In cases where former special-A vehicles are being added to an existing A licence and there is no objection, Mr. Williamson has not challenged normal users, which include terms such as "general goods, Great Britain," now described by the Transport Tribunal as obsolete. But he intends to do so on renewal.

## New Albion Light Six-wheelers

**M**EETING payloads falling between those of the maximum-capacity four-wheeler and the 20-ton six-wheeler, the Albion Reiver lightweight six-wheeler is superseded by a new range of 15-ft. and 12-ft. 2-in.-wheelbase models. The longer type, designated the RE.27N, has an overall length of 27 ft. 2 in., to carry a 21-ft. 6-in. body. The shorter model (RE.27T) is 21 ft. 8 in. long overall and can carry a body up to 16 ft. 6 in. The gross vehicle weight of both models is 15½ tons.

The new range is powered by Leyland O.375 vertical six-cylindrical oil engines developing 105 b.h.p. at 2,200 r.p.m. The maximum torque is 285 lb.-ft. at 1,100 r.p.m. The 14-in.-diameter single-dry-plate clutch has a friction area of 182 sq. in. and is hydraulically operated by a long-stroke slave cylinder with automatic adjustment for wear.

The heavy-duty five-speed gearbox has straight-toothed gears for first and second speeds and helical gears for the higher ratios. A helical-toothed overdrive sixth speed with a ratio of 0.76 to 1 can be built into the rear of the box.

A bogie with a trailing axle takes the place of the former two driven axles. The new driven axle is lighter than its predecessor and is of spiral-bevel design, with hub reduction gears. The reduction ratio is 4 to 1 and is provided by epicyclic gears giving overall ratios of either 6.933 to 1 or 7.248 to 1.

Steering is by Marles cam-and-double-roller gear.

The long-wheelbase model has a turn-

ing circle of 58 ft. and the shorter model, 50 ft. The two-leading-shoe brakes are assisted by air-pressure and hydraulically operated through two brake chambers with hydraulic master cylinders.

Tyre sizes are 9.00-20 (14-ply) at the front and 8.25-20 (10-ply) at the rear.

Stay bolts and pressed-steel cross-members brace the pressed-steel channel-section side members. The welded cab is of new design and is mounted on a sub-frame of box-section pressings. The low entrance step is located forward of the front wheel and the door opens through an angle of more than 90°. The one-piece windscreen is curved.

### SIR GILMOUR'S SUCCESSOR

**D**EPUTY secretary at the Ministry of Supply since 1953, Mr. L. J. Dunnett is to become permanent secretary to the Ministry of Transport in succession to Sir Gilmour Jenkins. Sir Gilmour is to retire from public service on March 31. Mr. Dunnett is 45.

## Applicant Warned of Severe Penalty

**W**HEN Mr. D. MacLennan, Main Street, Aberchirder, applied to Mr. A. Robertson, Scottish Deputy Licensing Authority, at Aberdeen last week, to continue an A licence with a change of normal user, Mr. Robertson reserved his decision but warned the applicant that a severe penalty would be imposed because of infringements of the licence.

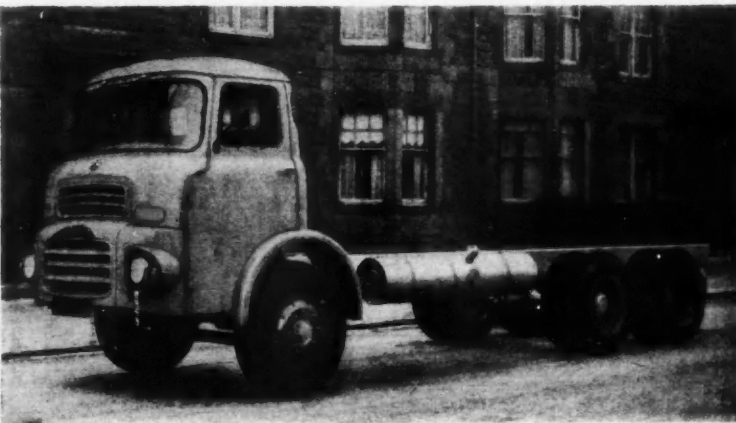
"Here is a man who bought over a business, and it is questionable whether we should have allowed the take-over to start with," said Mr. Robertson. "The business was in Inverurie, but it is quite evident to me that he had at least one vehicle operating from Aberchirder. Apart from that, I note that he has been breaking his normal user consistently by going twice a week to Leith."

Mr. MacLennan sought to include in the normal-user terms the transport of timber from Rhynie to the south of Scotland, feeding stuffs from Edinburgh and Leith to Aberdeenshire, and occasional runs to Banffshire with general goods. British Railways objected.

A witness from Levers (Cattle Foods), Ltd., Bridge of Marnoch, said that goods had to be delivered there from Leith. The applicant provided a first-class service.

Mr. Robertson later refused the application, which concerned two vehicles of 6 tons unladen. He indicated, however, that he would grant a short-term A licence for one vehicle.

Mr. MacLennan is expected to lodge a fresh application for both lorries, and attempt to prove need for the work which he has been doing irregularly.



The new Albion Reiver RE.27N six-wheeler with trailing axle.



## Men in the News

MR. C. T. BAYLISS has been appointed a director of Renold Chains, Ltd.

MR. K. E. COMFORT, a director and general manager of Brown Brothers, Ltd., has retired because of ill-health.

MR. LESLIE HARTRIDGE, chairman of Leslie Hartridge, Ltd., is leaving today for an extended tour of the U.S.A. and Canada, meeting manufacturers and agents.

MR. R. W. CREBER has been appointed technical sales manager of G. Beaton and Son, Ltd., succeeding the late Mr. CHARLES BURR. He has been with the company for 16 years.

MR. A. J. WORSTER, who has been manager of the Tyseley group of factories of the Rover Co., Ltd., since 1932, has been appointed a director of the company responsible for production.

MR. B. POTTS has been elected chairman of the Hereford Area of the Traders' Road Transport Association, with Mr. I. L. PRICE and Mr. EUSTACE PRICE as vice-chairmen. Secretary is Mr. E. S. WRIGHT.

MR. A. J. AGG and MR. P. J. AGG have joined the board of Trojan (Holdings), Ltd., and MR. H. J. RICKWOOD and MR. H. DARRELL have resigned. Mr. A. J. Agg has succeeded Mr. B. MONK as chairman.

MR. A. HAROLD, a director of Conveyancer Fork Trucks, Ltd., and chief engineer of Electro-Hydraulics, Ltd., and MR. R. EDWARDES, a director and secretary of Conveyancer and secretary of Electro-Hydraulics, have been appointed directors of Electro-Hydraulics, Ltd. MR. T. C. WRIGHT, a director and works manager of Electro-Hydraulics, has joined the Conveyancer board.

MR. D. J. HARROWELL has become managing director of Bray Construction Equipment, Ltd., succeeding Mr. R. W. T. BRAY, who has severed his connection with the company. Mr. Harrowell was formerly technical director. Mr. W. J. COLLARD has been appointed commercial director. Previously he was director and general manager of Harold Andrews Sheepbridge, Ltd., and he is replaced there by the former sales manager, Mr. B. C. GOODWIN.

MR. T. PARROTT, former Western Region manager of Tecalemit, Ltd., has moved to Brentford to become service manager, and MR. D. E. RIDGERS has been appointed his assistant. MR. J. G. H. MAXFIELD is the new industrial manager (users) and MR. B. C. CURRAN becomes industrial manager (manufacturers). MR. L. HALLIWELL is now northern region sales manager, MR. G. C. ABBOTT is assistant marketing manager and MR. G. AXWORTHY becomes personal assistant to the general sales manager, MR. R. R. PARKER. MR. D. A. MONAGHAN has been appointed sales director of Foamite, Ltd., a group company.

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MR. S. H. OLIVER has been appointed assistant manager of the purchasing department of the Wakefield Castrol Group.

MR. R. A. BONALLACK has been appointed a director of Bonallack and Sons, Ltd. He will continue as manager of the Manor Park body-repair works.

MR. JAMES WILSON, who has been transport manager of Hamilton Central Co-operative Society since 1944, is to retire early next month after nearly 30 years' service.

MR. BERNARD A. RIDLEY, assistant divisional manager of the South-Eastern Division of British Road Services since 1957, has been appointed manager of the London District in succession to Mr. H. ROSSINGTON, who resigned on medical grounds.

### OBITUARY

WE regret to announce the deaths of MR. J. L. S. MELVILLE, MONSIEUR JULES PEUGEOT, MR. WILLIAM ARTHUR EWER, MR. JAMES A. PICKTHALL and MR. JOHN OLIVER LUCAS.

Mr. Melville, who was 61, was south-western regional sales manager of Leyland Motors, Ltd. He joined the company in 1913 and resumed his association with Leyland at Bristol in 1920, after war service. He became regional sales manager there in 1930.

Monsieur Peugeot, who was 77, was the eldest of three brothers who built up the French manufacturing concern bearing their name.

Mr. Ewer was a director of George Ewer and Co., Ltd. He was 66.

Mr. Pickthall was a pioneer haulier in Cumberland, and when he retired a few years ago was head of one of West Cumberland's largest transport contracting businesses, J. A. Pickthall and Sons, Cleaton Moor. He was 75.

Mr. Lucas was to have rejoined the Joseph Lucas export organization on March 1 after a long illness, but died of pneumonia last Sunday in a Birmingham hospital.



Mr. R. W. Creber, technical sales manager of G. Beaton and Sons, Ltd.



Mr. R. A. Bonallack, now a director of Bonallack and Sons, Ltd.

## Not Surprising Railways Lost £80m.

BECAUSE British Railways, when they closed Rossington station, near Doncaster, to passenger traffic, failed to tell the public that excursions were to be run from it this year, Rossie Motors (Rossington), Ltd., were last week granted additional Sunday excursions from Rossington to Scarborough, Bridlington, Cleethorpes and Skegness.

Mr. J. Evans, appearing for the applicants, said the application was made because a notice regarding the closure of Rossington station to passenger traffic had been posted there and no rail passenger service was now run.

Other grounds were that there was a lack of excursion facilities for a growing town of 10,000 population, and that all the excursion business was being "scooped up" by private parties organized by operators based outside Rossington.

In evidence for the railways, who objected, Mr. J. Shaw said excursions would be operated from the station this year and would be advertised there a few weeks in advance.

Maj. F. S. Eastwood, chairman of the Yorkshire Traffic Commissioners: "But nobody will go to the station. You have closed it. It seems a funny way of going on."

Mr. Shaw then said the excursions would be advertised also in the Press.

Reading from a railway notice, Maj. Eastwood remarked that the wording was such that Rossington people would automatically assume that there would be no passenger service from the station. In the circumstances it was not surprising that the railways were £80m. "in the red."

Mr. J. N. Vallance, for the railways, submitted that the Commissioners should not take into account any ambiguity in the railway notices.

The licence was granted for Sundays only from Easter to September.

Rossie Motors are the only coach operators based in Rossington village.



## Round at Stoke: Weymouth Plans

IT is now confirmed that a round of the Lorry Driver of the Year Competition will be held at Stoke-on-Trent, probably late in July.

The competition season will open at Weymouth on April 18. Vehicles will assemble at Devon County Council's repair depot, Charnminster, where maintenance tests will be conducted. Competitors will then follow a 15-mile route to Westham Coach Park, Weymouth, where manoeuvring tests will be performed. Much more space will be available than last year.

Entries are due to close on March 28. The organizer, Mrs. J. P. Hall, 36 Broadmeadow Road, Wyke Regis, Weymouth, hopes that there will be more entries of articulated vehicles than last year.

The Southend round, organized by Mr. E. J. Barber, Essex Carriers, Ltd., 109-113 London Road, South Benfleet, Essex, is to take place on Sunday, June 28.

## PERKINS BOARD CHANGES

FOLLOWING the take-over of F. Perkins, Ltd., by Massey-Harris (Holdings), Ltd., board changes are announced. Mr. Frank Perkins, who has resigned the office of joint managing director, remains chairman, and Mr. M. I. Prichard, joint managing director, has been appointed managing director.

Col. W. E. Phillips, chairman and chief executive officer of Massey-Ferguson, Ltd., Toronto, and Mr. A. A. Thornbrough, president of Massey-Ferguson, become directors following the resignations of Sir Richard Yeabsley and Mr. G. P. Ritchie.

## High Rail Rate Influences Grant

"I HAVE to accept the evidence given by Colodense that the railway rate between Bristol and Leeds is nearly £3 10s. higher per ton than the road rate, and this is one of the matters which Section 9(3) of the Transport Act, 1953, empowers me to take into account," said Mr. S. W. Nelson, Western Licensing Authority, at Bristol on Tuesday, when he granted Reliance Motor Transport (Leeds), Ltd., Bitton, near Bristol, an extra vehicle of 3½ tons on A licence.

The company had applied for two extra vehicles to carry general goods normally between Bristol and Leeds.

Their gross receipts in the 10 months ended December, 1956, amounted to £11,809. For the year 1957 they totalled £19,695, and in 1958, £20,844. During these periods hiring charges had risen from £1,746 to £2,442 and £3,027. The applicants submitted that their hiring figures were now so high that it was necessary for them to increase their fleet, and that the businesses of old-established customers had increased.

Supporting the application, Mr. N. Hunter, of Colthurst and Harding, Ltd.,

## Cheap Tours for Elderly People Resisted

DECISION was reserved after the adjourned hearing before the Yorkshire Traffic Commissioners, at Leeds on Tuesday, of an application by Mr. William Tetley, Leeds, to operate seven-day tours to Ramsgate on 10 occasions this year. At a previous hearing, Mr. J. Evans, for Mr. Tetley, said he proposed to offer a tour at an inclusive price of £9 18s. 6d. deliberately designed to attract elderly people with limited incomes.

British Railways, West Yorkshire Road Car Co., Ltd. (on behalf of the Yorkshire Pool), Happiways Tours (Manchester), Ltd., Shearings Tours, Ltd., Heaps Tours, Ltd., Wallace Arnold Tours, Ltd., and Feather Bros. (Tours), Ltd., objected.

Mr. P. G. Nelson, aged 67, treasurer of the Evergreen Club at Seacroft, Leeds, said one of Mr. Tetley's coaches was already booked for a tour to Ramsgate and he knew of eight people who still wanted to go, but who could not get accommodation on the coach.

Mr. A. G. Davies, of British Railways, said there was a service by rail to Ramsgate from Leeds, via King's Cross and Victoria, London.

Mr. J. M. Barr, assistant managing director of Wallace Arnold Tours, declared that the tours would cause wasteful competition. Certain operators had created the demand for journeys to that part of the country and in his opinion Mr. Tetley was getting in on their backs.

## LINCOLN BUS STATION OPENED

A NEW bus station, at St. Mark's, Lincoln, was brought into use on Monday by the Lincolnshire Road Car Co., Ltd. Its construction had occupied a little over a year.

Bristol, said they paid Reliance £2,839 last year, compared with £1,816 in 1956. Their traffic carried by British Road Services and the railways would not be affected by a grant.

Mr. M. Monks, of Colodense, Ltd., said his company's business had increased by about 25 per cent. last year. Amounts paid to Reliance had risen from £378 in 1956 to £583 last year. They did not use the railway service to Leeds because the charges were too high. The rail rate for a ton was 156s., whereas the road rate was 87s. 6d.

Mr. Edwin Smith, of British Railways, said they could give delivery next day to Leeds, but he could not enter an argument on rates.

Mr. Leslie Jenkins, on behalf of B.R.S., said they had licensed facilities which were not fully employed, and two idle vehicles which could be licensed.

Mr. Nelson said the applicants had been operating the trunk route for a number of years and had built up their business on service. B.R.S. and the railways would not suffer.

## Country Bus Garage Design Standardized

THE new bus garage at St. Albans Road, Hatfield, brought into use by London Transport on Wednesday, is the first of a standardized design for their country depots. Its facilities are particularly suitable where expanded services are to be provided for the growing New Towns.

An open forecourt is bounded by the operating and welfare block. The buildings, pear-shaped turn-round and garden layout present an attractive appearance which can be adapted easily to sites of differing shapes.

The covered bus park occupies a space of 185 ft. by 98 ft. This is large enough to accept 49 buses. With open-air parking for a further 16 vehicles, the total capacity of 65 buses is considered to be the smallest that can use flow-line servicing and washing methods.

The main parking area is spanned by tubular steel trusses, giving unimpeded space for movement of vehicles. Flanking the parking area are foremen's offices, workshops, stores and toilet accommodation. Standard automatic refuelling plant is provided, served from three 5,000-gal. tanks, and there are facilities for bus washing, lubricating and vacuum-cleaning. There are three maintenance pits, a compressed-air ring main and fluorescent lighting to supplement the natural light supplied by extensive glazing.

The new Hatfield garage replaces an earlier building housing 22 buses.

## BOARD PLAN FOR WEST MIDLANDS

THE formation of a West Midland Area Authority, to take over various functions including fire and ambulance services and bus operation, is proposed by the local regional council of the Labour Party. It has not been welcomed by the Socialist members of some Labour councils in the area.

The bus undertakings of Birmingham, Wolverhampton, West Bromwich and Walsall Corporations, as well as those of companies including the Birmingham and Midland Motor Omnibus Co., Ltd., are embraced by the proposal.

## NEW TRENT APPOINTMENT

MR. J. C. CLYMO, Swansea area traffic superintendent of South Wales Transport Co., Ltd., has been appointed assistant traffic manager of Trent Motor Traction Co., Ltd. He will take up his duties on March 1.

He was awarded the Sir William Chamberlain Scholarship by the Institute of Transport in 1955, and under its terms studied transport in the United States.

## LOW BUSES FOR SHEFFIELD

FORMING part of Sheffield Corporation's tram-replacement fleet, six A.E.C. 76-seat buses were officially handed over to the Lord Mayor of Sheffield, Ald. J. W. Holland, on Tuesday. The new vehicles are 30 ft. long, 8 ft. wide and only 13 ft. 5 in. high. They cost £5,975 each.

## "Artics" Unsuitable for Local Smalls Service

ARTICULATED units were not suitable for the collection and delivery of smalls in congested built-up areas, and Midland Parcels (Stoke), Ltd., had received many complaints from customers and threats of police prosecution because of delays and obstruction, the West Midlands Licensing Authority, Mr. W. P. James, was told at Hanley last Friday. The company applied to replace two articulated units by three rigid vehicles, each of 3½ tons unladen, on A licence.

Mr. J. A. Dunkerley, for the applicants, said they operated 10 vehicles on a parcels service between the Midlands and Manchester. The carrying capacity of the articulated outfits was 16 tons and that of the replacements totalled 15 tons.

Granting the application, Mr. James said three small vehicles would promote economy and flexibility, and there were no objections to the change.

## DAMAGES FOR BROKEN CONTRACT

DAMAGES of £214 17s. 6d. for breach of contract were awarded last Friday to Shukers of Sheffield, Ltd., 58 Broad Street, Sheffield. Giving judgment at Sheffield County Court, Judge Ernest Ould said Mr. W. F. Shuker, managing director, was "a persistent, pushful salesman." But, he said, "there is nothing wrong in that."

The amount claimed was the profit lost when an order for a tipper was cancelled by Charles Munro Smith, Southgate, Eckington, near Sheffield. Mr. Shuker said he ordered the vehicle for Mr. Smith after a telephone conversation, and confirmed the deal in writing. Five days later Mr. Smith cancelled the order.

He claimed that he had not ordered the vehicle from Shukers. He had obtained a lorry from another concern because Shukers had been unable to supply one by January 1.

## IMPROVE ROADS TO REDUCE ACCIDENTS, SAYS SURVEYOR

WARWICKSHIRE requires more roads with dual carriageways or three lanes for traffic to reduce accidents, which last year reached a record, Mr. D. Watson, county surveyor, has reported to the county roads committee. He said that during the past 30 years only 48 miles of the 365 miles of trunk and first-class road in the county had had either a dual carriageway or a third traffic lane added to it.

It was also reported that the speed of traffic on the completed stretch of the London-Birmingham motorway between Stonebridge and Allesley was so great at peak periods that it was difficult for vehicles to enter the road.

## TECALEMIT'S NEW INTEREST

HALF the ordinary shares in British Filters, Ltd., has been purchased by Tecalemit, Ltd. Mr. S. G. Gates, chairman, and Mr. P. R. Scutt, managing director of Tecalemit, will join the board of British Filters. Mr. T. E. Worth is the chairman and technical director.

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## MUNICIPAL OPPORTUNITIES

Liverpool Health Committee require a meals van. Pudsey Corporation seek to obtain a Guy lorry. Workington Streets Committee require a Commer

oller. Saddleworth Urban District Council require a tower wagon.

Durham City Council are to buy a refuse collector from Ansa Motors, Ltd.

Hull Corporation are to buy a tower wagon from the Triangle Motor Co., Ltd.

Eccles Highways Committee recommend the purchase of a Bedford 2-3-tonner.

Portsmouth Cleansing Committee wish to borrow £56,700 to buy vehicles next year.

Wrexham Corporation are to purchase two Karrier Gamecock refuse collectors.

Carlisle Fire Brigade Committee seek approval for the purchase of a B-type tender.

Wandsworth Borough Council are advised to buy an Eagle gully-emptier from Rootes, Ltd.

Leeds City Council are recommended to buy a Thames Trader from Magnetic Motors, Ltd.

Salford Watch Committee propose to order a Thames 12-seater from Collins Auto Co. (Lancs), Ltd.

Wallasey Works Committee recommend the purchase of a Karrier Gamecock and two S.D. refuse collectors.

Sheffield City Council are advised to purchase a Morris 1½-ton van and a 3-ton tipper from Kennings, Ltd.

Rugby Corporation are recommended to buy a Thames-Yorkshire gully-cesspool emptier from Furrows, Ltd.

Prestrich Corporation are to purchase a light tower wagon from H. E. Nunn and Co., Ltd., and a 3-tonner from Dennis Bros., Ltd.

Scunthorpe Corporation are to buy a Thames Trader from the Lincolnshire Motor Co., Ltd., and a Bedford Workabus from Laynes Garage, Ltd.

Doncaster Parks Committee recommended that Edwards Motors (Doncaster), Ltd., should supply an Austin Omnivan.

The corporation are to seek to borrow £4,595 to buy an S.D. bulk loader and £3,420 for a Karrier refuse collector.

## 51% of Work Can be Normal User

DURING an application, last week, by the Glasgow Haring Co., Ltd., for the renewal of the A licences for their entire fleet, the railways, as objectors, suggested that the applicants should demonstrate that all the work they were doing was, in fact, in accordance with their normal user. Mr. W. Quin, Scottish Licensing Authority, ruled that 51 per cent. of a haulier's operations could constitute his normal user, and he granted the application after an analysis of revenue, tonnage and mileage had been submitted.

It was agreed that the company's normal user would be regarded as the carriage of 70 per cent. of their traffic within 25 miles of Glasgow, and the balance as required.

## REVOCATION INQUIRY

AN inquiry to consider whether to revoke or suspend a contract A licence held by W. Hogger will be held by the Metropolitan Licensing Authority at Romney House, Marsham Street, S.W.1, on March 3.

On the previous day he will consider whether to delete a vehicle from the licence of J. Edwards and Sons, because it has ceased to be used.

## Lo.T.A. CONFERENCE PLANS

THE Institute of Traffic Administration will hold their annual conference in Southampton from May 29-31. Mr. A. C. West, chief constable to the British Transport Commission, will be the principal speaker. Lord Merrivale will preside. There will be a visit to Southampton docks and the Ocean Terminal.

## Conductor Used Two Ticket Machines

A BUS conductor employed by the South Wales Transport Co., Ltd., was sentenced to six months' imprisonment at Swansea Justices' Court after pleading guilty to stealing a ticket machine and embezzling money from his employers.

He was Kenneth Gregory Williams. Broughton Avenue, Portmead, Swansea, who admitted stealing a ticket machine valued at £50 8s. and to two charges of embezzling sums of £1 19s. 8d. and £221 6s. 2d. received by him for the company.

Mr. John Barratt, prosecuting, said the machine was missed on October 14 last and found in Williams' possession on February 5. In a statement, Williams said he picked up the machine thinking that it was his own, and it was not until later that he realized that he had two.

He then used the second machine on single journeys and left the way-bill blank in respect of the tickets. He added that he had used his official machine to issue tickets on the return trip and then spread the tickets issued to cover both the inward and outward journeys.

Williams, who had no previous convictions, told the justices that he had been sick for a total of two years and had fallen in arrears with hire-purchase payments.

## HAULIER'S RECORD "PRETTY POOR"

"YOUR reputation as a haulier is pretty poor," Maj. F. S. Eastwood, Yorkshire Licensing Authority, last week told Mr. Lawrence Mather, Woodlands, Wood Bottom, Lower Hopton, near Mirfield. "Besides having been prosecuted for operating illegally, you have now removed your vehicle from the contract licence illegally," he added.

Mr. Mather had applied for a B licence, but Maj. Eastwood refused to grant the application in full. Mr. Mather said he had a vehicle on C licence and another on contract-A licence. The contract vehicle was engaged on seasonal work for building contractors, and he had removed it from the licence some time ago.

He admitted having been prosecuted for earning £101 by illegally running a lorry and had had 10 charges taken into consideration. He was short of money at the time, he said, and had to find work. He had been trying out the lorry to discover whether it would take the load and had intended to apply for a licence.

## £185,000 LOSS FOR LIVERPOOL?

ALTHOUGH they expect a surplus in the current year (ending next month), Liverpool Passenger Transport Committee are budgeting for a loss of about £185,000 next year. Last year's wage awards to platform staff and craftsmen total £166,000 in a full year. In addition, the cost of fuel has risen by £19,000 a year, and traffic is declining.

Mr. W. M. Hall, general manager, says reductions in services or increases in fares will have to be considered.

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## "Public Should Not Pay for Wage Increase"

**A**N application to the Yorkshire Traffic Commissioners at Leeds on Monday by the Yorkshire Traction Co., Ltd., to vary fares by not more than 1d. on 169 routes, was resisted by eight local authorities. Mr. W. A. Goss, representing the objectors, said it was another attempt to pass on to the public the whole of a wage increase.

The objectors were Doncaster and Wortley Rural District Councils, Worsborough, Hoyland Nether, Mexborough, Hemsworth and Dearne Urban District Councils, and Barnsley Corporation.

Mr. W. R. Hargrave, for the company, said that in September, 1957, fares were fixed on mileage scales for the first time. If the application were granted, those scales would in certain respects be raised.

The dominant reason for the application was the increase in costs in a full year of £36,950, of which £31,000 was caused by a wage award. There was also an additional £4,000 to be found for fuel, but this had been offset by reduced mileage.

During a 17-week period between October last year and February this year and a comparable period between October, 1957, and February, 1958, receipts had dropped by £15,000. The gross receipts for the later of those two periods were £485,000, compared with £500,000 for the previous period.

Mr. N. H. Dean, general manager, agreed in evidence that directly wages rose, he asked for higher fares. He also agreed that last year the company had a "grand year." Last year's profit was the best since 1953. One of the reasons for the decrease in receipts was the reduction in the conveyance of miners on a Saturday morning because of a five-day week.

Mr. Goss remarked that with a "grand year" recently ended, the application was a little odd. Decision was reserved.

## HIGHER FARES FOR MACBRAYNE

**P**ERMISSION to raise fares was granted last week by the Scottish Traffic Commissioners to David MacBrayne, Ltd. They originally sought to increase all fares, but at the hearing they amended their application to link with recent increases awarded to Scottish Omnibuses. The result is that only fares over 5d. are affected.

Mr. John R. Cameron, assistant traffic manager, said the bus section of the company was operating at a loss and would continue to do so even with the higher fares. They could find no further ways of economizing. Under the terms of the amended application, people travelling regularly on shorter routes would not be affected.

Inverness and Argyll County Councils objected

## BEDFORDS VISIT EUROPE

**F**OURTEEN Bedford vehicles left Dunstable on Monday on an 8,000-mile sales-promotion tour of Europe. They ranged from a 10-cwt. van to a 7-ton lorry and a coach.

## PROFIT AND LOSS

**Mann and Overton, Ltd.**, £84,247 net profit after £96,726 tax. Year's dividends 1s. 1½d. per 5s. share, including 3d. bonus.

**Trent Motor Traction Co., Ltd.**, £142,418 net profit after £136,078 depreciation and £111,007 tax. Year's dividends 10 per cent.

**Salford Transport Department**, £12,136 loss. Total working expenses, £1,363,707 (33,98d. per bus-mile); total income, £1,509,643 (37,62d. per bus-mile); debt and other charges, £158,271; contributions to capital expenditure, £9,621. Passengers, 90.3m.; mileage, 9.6m.

## Hauliers Fined £275 and Drivers £30

**F**INES totalling £275 were imposed last week on Leverington Transport, Ltd., Station Road, Terrington St. Clement, for offences relating to drivers' records.

Terrington magistrates also fined five drivers a total of £30, with £4 14s. 9d. costs, on 15 charges of failing to keep correct records or driving for excessive hours, or both. One driver was fined for not having 10 consecutive hours' rest, as well as for driving for more than 11 hours on the same day.

The company were fined £5 on each of 55 charges referring to incidents during the two-month period from the middle of last July. They pleaded guilty to all charges.

Mr. S. J. Green, prosecuting, said that in 18 of the charges the complaint against the company was that items in respect of goods carried were left blank and in the remaining 37 cases details of the journeys were incomplete. Those responsible for looking after the company's affairs had not given proper supervision.

Mr. J. R. Kratt, defending, said all the offences related to two vehicles. The company were, in effect, in the hands of the drivers in the matter of records. Some had been dismissed for not keeping proper records.

## SALFORD TRAFFIC 6½% DOWN

**A** DECLINE of more than 6½ per cent. in the number of passengers carried was experienced in the year ended March 31 last by Salford Transport Department. Gross receipts dropped by 5½ per cent., mileage by about 3½ per cent.

Mr. Charles W. Baroth, general manager, says in his annual report that the decrease of £85,638 in receipts in a year was not a true indication of the extent of the recession in traffic, because revenue in the year ended March 31, 1957, was inflated by the special circumstances of the Suez situation.

The favourable factors which enabled the undertaking to record a surplus of £109,202 in 1956-57 disappeared last year, and a loss of £12,136 was incurred.

## PROFIT CLAIM CUT

**W**HEN Mr. M. J. L. Henderson, haulier, 3 Crown Terrace, Mount Vernon, Barnsley, a witness at Sheffield West Riding Court last Friday, claimed £10 loss of profit on a load that he would have taken to Liverpool had he not been required to appear in court, the amount was reduced to £2. His lorry had been involved in a collision with a car, the driver of which was fined for careless driving.

## Home Secretary Wants Traffic Wardens

**A**LTHOUGH, as reported in *The Commercial Motor* last week, the proposed scheme for Nottingham to have a corps of traffic wardens has run into difficulties because the finance committee there are disinclined to pay for it. Mr. David Renton, Under-Secretary of State for the Home Office, stated in the House of Commons on Monday that the Home Secretary would welcome an experiment of this kind. His approval, however, would be necessary if a grant were to be paid towards the extra expenditure.

The wardens would be under police supervision, but would not have powers of direction or prosecution. It would be for the police to charge offenders and bring them before the courts. A trial scheme would provide valuable evidence of the contribution which could be made by a separate force of wardens in helping to keep the streets clear of stationary vehicles.

Mr. Renton said that there had been informal discussions between Home Office officials and representatives of Nottingham Watch Committee with the chief constable on the proposal. Not all chief constables and police authorities were in favour of even a limited scheme on the lines suggested at Nottingham.

## £100,000 ORDERS FROM U.T.A.

**C**ONTRACTS to the value of nearly £100,000 have been placed with the Leyland Group by the Ulster Transport Authority for the supply of 68 goods-vehicle chassis.

One order is for 25 Leyland Comet-Scammell tractor chassis of 8-ft. wheelbase, with Scammell automatic coupling gear. Albion are to supply 43 light-weight Claymore chassis with 72 b.h.p. underfloor oil engines. The chassis have a wheelbase of 11 ft. 10 in., and are suitable for bodywork giving an internal length of 16 ft. 6 in.

U.T.A. are equipping 20 Claymores with cattle-float bodies; the remainder will be used for general haulage.

## NATIONALIZATION LEGAL

**A**PPEALS filed by private transport operators in Uttar Pradesh, India, challenging the validity of the State nationalization of transport, have been dismissed by the Supreme Court of India. It was argued on behalf of the complainants that the Motor Vehicles (Amendment) Act, 1956, was wholly repugnant to the provisions of the Uttar Pradesh Act and that the Government of India should prevail over the State Act.

## SHORTER TOURS TO ITALY

**W**HEN the Mont Blanc road tunnel from Entreves to Chamonix is opened in 1961 it will be possible to reduce substantially the mileage of coach tours from Britain to Italy, travelling via Paris. The Paris-Turin journey will be shortened by 137 miles and the Paris-Milan route by 194 miles.

It is estimated that 24,000 coaches and buses and 15,000 goods vehicles will use the tunnel each year.



## Some Heavies as Good as Best Cars in Braking Performance

"SOME commercial vehicles with a gross laden weight of 20 tons have a braking performance comparable with that of the best cars," said Dr. H. J. H. Starks, during a paper on research on the testing and performance of vehicle brakes, presented to the Institute of Road Transport Engineers in London yesterday.

Nevertheless, not all heavy commercial vehicles gave entirely satisfactory results. Manufacturers did not pay enough attention, when designing braking systems, to details of piping, valves and the situation of items such as air reservoirs, which could transform a mediocre system into a good one.

Tests on vehicles to assess braking performance had not been sufficiently standardized, said Dr. Starks. It was not adequate to record maximum deceleration with a meter and leave it at that. Probably the most effective method was to measure the actual braking distance with detonator equipment which fired a chalk pellet on to the road at the moment the driver's foot contacted the brake pedal [the method used for many years by *The Commercial Motor* in all tests].

If it were already possible to design a braking system which could provide satisfactory retardation for a laden vehicle, it was now necessary to perfect equipment which would have differing braking characteristics as the loading on the vehicle altered and would, superlatively, provide a different braking force for each wheel as weight transference took place during the braking sequence.

With more power available for operation of the brakes and vehicle speeds increasing constantly, the risk of instability through wheel locking had assumed greater importance. Devices for preventing wheel locking were now in existence and had undergone tests.

The Dunlop Maxeret apparatus, as used for aircraft wheel brakes, had possibilities in this direction. Although it was less bulky than an American system which had been tested on commercial vehicles,

it still required further development and simplification before it could be used extensively for road vehicles.

A major problem to be overcome was the rapid loss of efficiency by some braking systems during operation. Tests conducted in 1950 on commercial vehicles in use had shown that many vehicles of over 9 tons gross weight had a maximum deceleration of less than 0.4g., which was thought to be the lowest acceptable figure.

The disc brake might be the eventual answer to this problem by virtue of the provision of automatic adjustment and the ease with which the brake pads could be replaced. Meanwhile, the exhaust brake, which could reduce the work done by the wheel brakes by 20 per cent. or more, was a useful method of prolonging the efficient life of the system.

### B.E.S.T. TO EXTEND ROUTES

PLANS to extend bus services to cover the whole of Greater Bombay have been made by Bombay Electric Supply and Transport, a municipal undertaking. Some routes that the company propose to serve are already operated by private concerns.

When additional buses arrive, tram services in congested areas will be withdrawn. It is also hoped to introduce trolleybuses on an experimental route, but it is understood that certain difficulties attach to import licensing.

### W. GERMAN PRODUCTION

DURING 1958, West Germany produced 188,410 lorries, buses and tractors, and 126,110 estate wagons. Overall production in the motor industry was just under 1½m. vehicles.

## New Transport Companies

**Chesterton Haulage Co., Ltd.** Cap. £2,000. Dirs.: Frank Condon, The Coppice, Darlaston, Stoke-on-Trent, and Victor Wild, Flat 2, Parkfields, Park Avenue, Dresden, Stoke-on-Trent. Sec.: V. Wild. Reg. office: Trentmill Road, Hanley, Stoke-on-Trent.

**Wilfred Armstrong and Sons, Ltd.** Cap. £5,000. Dirs.: Wilfred H. Armstrong, Bank Top, Prestwick, Ponteland, Newcastle upon Tyne, and Matthew Armstrong, 11a West End, Ponteland. Sec.: Eleanor Armstrong. Reg. office: Bank Top, Prestwick, Ponteland, Newcastle upon Tyne.

**John Squires (Haulage), Ltd.** Cap. £10,000. Dirs.: John Squires, William J. Squires and Mrs. Sarah E. Squires, 41 Penllwyngwyn, Llansennell, near, Llanelly. Sec.: W. J. Squires. Reg. office: 41 Penllwyngwyn, Llansennell.

**James Henderson Transport, Ltd.** Cap. £2,500. Dirs.: James Henderson and Mrs. Laurel T. Henderson, 35 Retford Road, Workop, Notts, and Jonathan Crabtree, 36 Spur Crescent, Workop. Sec.: Laurel T. Henderson. Reg. office: 35 Retford Road, Workop.

**Merle Transport, Ltd.** Cap. £100. Subs.: Joyce Swerling, 16 Edgworth House, Boundary Road, London, N.W.8, and Leila J. Schneider, 5 Edgworth House, Boundary Road, London, N.W.8. Sec.: P. S. Watt. Reg. office: 34 South Molton Street, London, W.1.

**C. S. Baker, Ltd.** Cap. £1,000. Dirs.: Charles S. Baker, and Mrs. Mary E. Baker, 1 Alma House, Ealing Road, Brentford, Middx. Sec.: Mary E. Baker. Reg. office: 48-50 Uxbridge Road, London, W.5.

**Simonstone Transport, Ltd.** Cap. £5,000. Dirs.: Cyril Hanson and Edith Hanson, Corner Croft,

George Lane, Read, Lancs. Sec.: Solly Lightbrown. Reg. office: Dene Bridge Farm, Simonstone, Lancs.

**Ashworth and Hall, Ltd.** Cap. £100. Dirs.: Sydney J. Ashworth, 60 Eton Avenue, London, N.W.3, and Ian A. R. Bingham-Hall, 34 Alverstone Road, Wembley Park, Middx. Reg. office: 34 Alverstone Road, Wembley Park.

**Hudson and Smurthwaite, Ltd.** Cap. £1,000. Dirs.: Alfred L. Hudson, 78b The Chase, London, S.W.4, and Arthur T. Smurthwaite, 40 Culmstock Road, London, S.W.11. Sec.: William J. Allen. Reg. office: 29-30 Bedford Row, London, W.C.1.

**Thompsons (Blyth), Ltd.** Cap. £100. Dirs.: A. Thompson and Mrs. Eva L. Thompson, 50 Cypress Gardens, Blyth. Sec.: A. Thompson. Reg. office: Princess Louisa Road, Blyth, Northumberland.

**Downs Transport Co., Ltd.** Cap. £1,000. Dir.: William S. Schofield, 61 Kings Road, Ashton under Lyne. Reg. office: 2 The Downs, Altrincham, Ches.

**H. G. Jenkins (Transport), Ltd.** Cap. £100. Harry G. Jenkins and Mrs. Violet L. Jenkins, 1510 High Road, London, N.20. Sec.: H. G. Jenkins. Reg. office: 77 Landsdowne Drive, London, E.8.

**Meggs Transport, Ltd.** Cap. £2,000. Dirs.: Leonard C. Meggs and Mrs. Gertrude P. E. Meggs, The Cherry Tree Inn, Woodbridge, Suffolk. Sec.: Gertrude P. E. Meggs. Reg. office: The Cherry Tree Inn, Woodbridge.

**Auto Transport and Engineering Co., Ltd.** Cap. £5,000. Dirs.: John M. C. Hutchings and Mrs. Mary C. Hutchings, Gaylands, Four Marks, Alton Hants. Sec.: J. H. C. Hutchings. Reg. office: Station Garage, Four Marks, Alton.

## Micrograms . . .

**Depot for B.R.S.:** British Road Services plan shortly to build a depot at Hove.

**New Bus Station:** Colchester Transport Department proposes soon to construct a bus station and car park at a cost of £55,270.

**Import Restriction:** Motor vehicles, chassis and bodywork are subject to import licensing which has been imposed by the Cuban Government.

**Hire-purchase Sales:** Hire-purchase debt rose by about £120m. in 1958 to a total of £604m. Commercial-vehicle business amounted to some £49m.

**More Parking Meters:** Manchester, Liverpool, Bristol, Birmingham and Sheffield have applied to the Minister of Transport for power to introduce parking meters.

**More Room Now:** The National Benzole Co., Ltd., have moved their southern division offices to larger premises at Mercury House, 100 High Street, Guildford.

**Dunlop in India:** A £1,800,000 tyre factory has been opened by the Dunlop Rubber Co., Ltd., at Ambattur, near Madras. Output will include 134,000 giant tyres a year.

**Yale Scottish Depot:** Sales, service and repair facilities are offered for operators of Yale trucks at a new depot at Wilson Place, Nerston Industrial Estate, East Kilbride, Lanark.

**Instructive Gifts:** A sectioned gearbox and rear axle are among components presented by Guy Motors, Ltd., to the Government Trade Centre in Kano, Nigeria, for instructional purposes.

**Thornycroft Distributors:** Merchiston Motor Works, Bryson Road and West Bryson Road, Edinburgh, have been appointed distributors of Thornycroft vehicles in Edinburgh and district.

**Kingsway Underpass?** London County Council are considering two schemes for the use of the old Kingsway tram tunnel as an underpass for traffic between the Embankment and Kingsway.

**Bus Indicators:** Until conclusions have been reached on proposals for the standardization of direction indicators, the Minister of Transport is not willing to make their use on buses compulsory.

**Extra Carriageway:** A second carriageway, 30 ft. wide and nearly 1½ miles long, on the East Ham-Barking by-pass, was opened last Friday. The work was completed five months ahead of schedule.

**Flasher Glare:** The Metropolitan Boroughs' Standing Joint Committee have submitted observations to the Ministry of Transport about the glare caused by brake lights and flashing direction indicators.

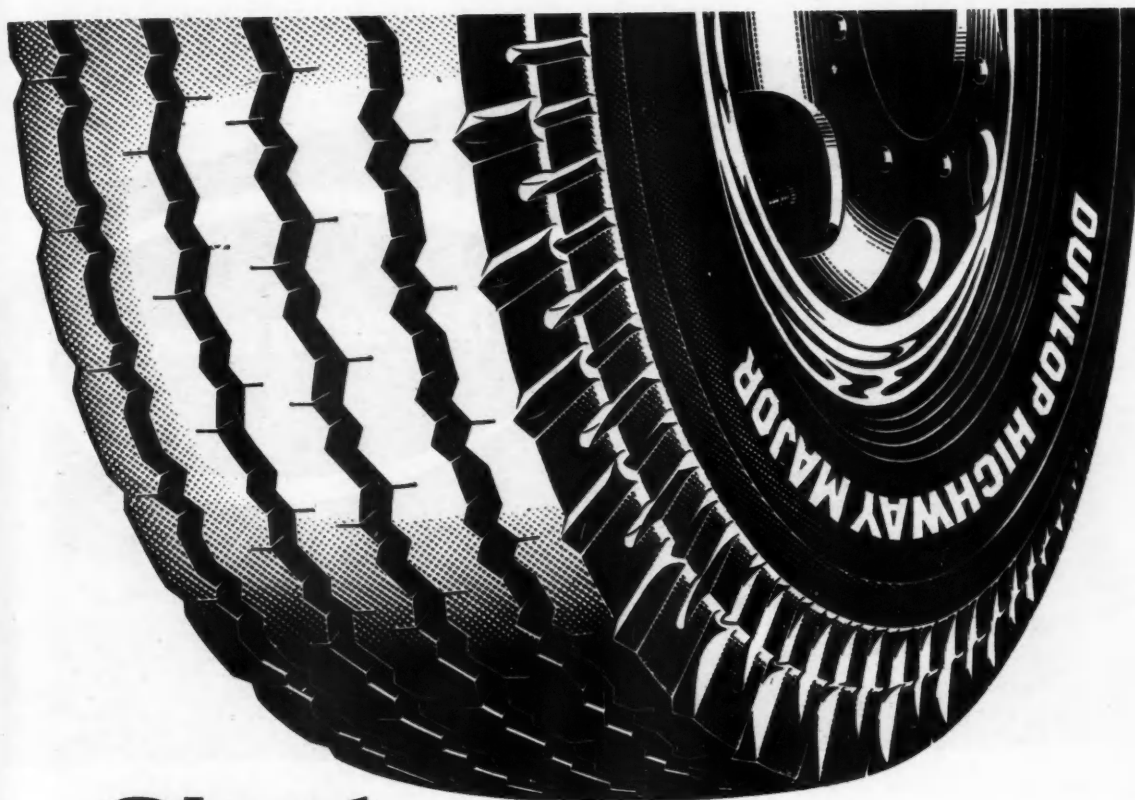
**500 Cafés:** Five hundred recommended cafés are listed in the latest edition of the "Drivers' Guide," published at 1s. by the Road Transport Catering and Accommodation Joint Committee, 146 New Bond Street, London, W.1.

**Costly Road Scheme:** London County Council are to buy land in the neighbourhood of Old Street, City Road and Great Eastern Street, at a cost of nearly £2m., for a big road-improvement scheme to be carried out in 1961-62.

**Timpson Fleet Additions:** Ten long-distance touring coaches, four of which will be based on the Continent, are to be added to the fleet of A. Timpson and Sons, Ltd., Catford. The A.E.C. Reliance coaches will have coachwork by Weymann and Harrington.

**David Brown Service:** The sales department of the David Brown Construction Equipment Co., Ltd., formerly located at Hanworth Park, Feltham, Middx., has been divided into northern and southern depots. The southern area, covering the south of England and South Wales, will continue to be operated from Feltham. The northern depot, covering North Wales, the north of England, Scotland and Northern Ireland, will be located at Moll Springs Mills, Honley, Huddersfield.





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Keep down running costs by choosing your giant tyres from the Dunlop range. There are tyres to meet every type of service condition—each one individually built to give you mileage, safety and dependability.



## DUNLOP HIGHWAY MAJOR

A first-class tyre with extra strength and tread-depth. Gives profitable *extra* mileage on goods and passenger services.

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**BUILT BETTER TO LAST LONGER!**



**USES**  
**WAKEFIELD-DICK**  
**DEUSOL CR**  
**DIESEL LUBRICANTS**



The Metal Box Company operates a fleet of over 500 vehicles. During 1957 they covered more than 7 million miles. For the diesel portion of this large fleet, Metal Box chooses Deusol CR lubricants supplied by Wakefield-Dick, specialists in lubrication for more than 50 years.

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**WAKEFIELD-DICK INDUSTRIAL OILS LIMITED**  
 A MEMBER OF THE WORLD-WIDE WAKEFIELD CASTROL ORGANISATION

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## Bus Operators Plead for Tax Relief

OPERATORS of some 50,000 buses and coaches, represented by the Municipal Passenger Transport Association, Public Transport Association, Passenger Vehicle Operators' Association and Scottish Road Passenger Transport Association, have addressed a strong appeal to the Treasury for relief from oil-fuel duty and Excise duty.

The Associations base their case for the abolition of the fuel tax on the industry's grave position, particularly with regard to country services, and on the unfairness of a tax applied only to road transport.

They state that recent increases in costs have, in the main, been met by reduced services and internal economies. The operators recognize that the competition of private motoring and the influence of television have cost them customers whom they are unlikely to win back.

Rural services, customarily subsidized by more profitable urban services, are being drastically cut, and in some cases withdrawn altogether. It is realized that the solution to the operators' problem lies not in increasing revenues but in cutting costs.

The 200 per cent. fuel duty represents £30m. to the Exchequer and up to 3d. per vehicle-mile to the industry. The Associations point out that if the tax were spread among all users of oil, the tax would be only 5½d. per gallon. The abolition of the tax, and the reduction of the Excise duty to the flat rate of £12 10s. applying to cars, would save the country bus services which are doomed to extinction and restore the urban facilities that have been reduced in recent months.

## Charged with Carrying Cattle Illegally

OPERATORS were fined, last week, in two cases concerning the improper use of B-licence vehicles for the carriage of cattle.

Cockermouth magistrates fined James Tonkin, Park View, Castle Croft, Egremont, £1, with 5s. costs, for carrying cattle for hire or reward without a carrier's licence. William Casson, trading as Dent Aerated Water Co., Cleator Moor, was fined a similar amount for aiding and abetting. An advocate's fee of £8 8s. was allowed.

Mr. Edward Fraser, a traffic examiner, said he saw Tonkin at Cockermouth Auction Mart. According to the certificate the vehicle belonged to the Dent Aerated Water Co., and Tonkin said it was on hire to him until he could buy it.

Later Casson said the vehicle was licensed for haulage in 1933, but after the war he had no time to build up his haulage business again. Tonkin had more work than he could do with one lorry, and Casson offered him the use of his, so that later it could be transferred to Tonkin, who was responsible for running it, and retained the payments.

Cross-examined by Mr. T. H. Campbell Wardlaw for the defendants Mr. Fraser agreed that the vehicle was properly

## New Lighting Cuts Accidents by 30%

THE replacement of old lighting systems by modern installations reduced road accidents after dark by as much as 30 per cent., said Mr. W. Robinson, lighting officer of the Electrical Development Association, at Cambridge last week.

Addressing the Eastern Accident Prevention Association, Mr. Robinson said that all street lighting, except that on designated trunk roads, was paid for by the local authority, with the assistance of Government loans where required. Under this arrangement it was scarcely surprising that main-road lighting was patchy and disorganized.

London's North Circular Road, for example, ran through 13 local-authority areas, whose expenditure on lighting varied from 4s. 5d. to 16s. 7d. per head. There were a dozen lighting systems in 20 miles of this arterial road.

A co-ordinated system could be introduced if street lighting were to be brought into the Improvement Grants Scheme.

### SAFE PARKING FOR LORRIES

OVERNIGHT supervised parking facilities, for which commercial vehicles are to be given priority, have been provided by Newcastle upon Tyne City Council. The parking places are at Morden Street and the junction of Northumberland Road and Ellison Place.

Charges for overnight parking are 2s. 6d. per vehicle with an unladen weight up to 2½ tons. Heavier vehicles will be charged 5s., and there is an additional charge of 2s. 6d. per trailer.

licensed for use by Dent's for the carriage of their own goods and livestock within 30 miles.

Mr. Wardlaw said that last August an arrangement was made for Tonkin to have the benefit of the licence for an unspecified period. He operated the vehicle with the company's knowledge, and was Casson's agent. He submitted that Casson was the "user," and he defined "agent" as one who "placed his principals in contractual relationships with others."

Mr. G. N. Worthington, prosecuting, said he had never heard of an agent who ran a haulage business and retained all the profits.

The magistrates found that a technical offence had been committed.

Penrith magistrates fined Fellside Transport, Ltd., Lazonby, Penrith, £5, with £3 3s. advocate's fee and 18s. costs, for carrying livestock outside the 25-mile limit imposed on their B licence.

Defending, Mr. G. W. Davidson said that Mr. Albert Kelso, owner of the business, transported the cattle under the impression that he was covered by the proviso to the 1933 Act which allowed a farmer to carry for his neighbours. He now realized he was wrong.

## "B" Grant Could Set Free "A" Capacity

BY granting the B-licence variation sought by the applicant, capacity on his special A-licence vehicles would be set free for other work, Mr. W. P. James, West Midlands Licensing Authority, said at Hanley last Friday. Without calling on the objectors, the British Transport Commission, Mr. James refused an application by Mr. G. H. Nixon, of Wolstanton, to vary the conditions of a B-licence vehicle by adding "Goods for S.M.S. Tiles, Ltd. as required."

Mr. James described the application as "very substantial." Certified figures were absent and the applicant's evidence, he added, was evasive.

Mr. Nixon said three vehicles on special A licence and one on an unlimited B licence were working mainly for S.M.S. Tiles, Ltd. There was a large demand for small loads for shop display and odd fireplaces to be carried direct to retail customers. These items did not constitute a paying load for the long-distance vehicles. The B-licence vehicle was engaged on local work.

Mr. G. C. Tinsdill, for the applicant, said small loads of fireplaces had been delayed for as long as 14 days until full loads could be made up. S.M.S. Tiles were old-established and substantial customers.

### A SUBTLE DIFFERENCE

"APPARENTLY in the eyes of the Left Wing it is not waste to have uneconomic railway facilities, even though the march of technological progress has signalled that for some purposes their day is past, but it is waste to have a flexible and efficient road haulage industry which exactly fits the needs of modern commerce and industry," Mr. J. Theobald, chairman of West London Sub-area of the Road Haulage Association, said at Hounslow last Friday.

If road transport were renationalized, he added, the nation would be deprived of an important means for raising productivity and checking inflation. Britain would be handicapped in relation to nations such as the United States and Western Germany, which were turning increasingly to road transport.

### PEGASO-VIBERTI VENTURE

INTEGRAL passenger vehicles are being built by Officine Viberti, Turin, Italy, with Spanish Pegaso engines and running units. Coaches and city buses are to be produced. The standard power unit is a Pegaso V-6 120 b.h.p. oil engine. The construction of the new vehicles follows the normal Viberti Monotral pattern, and the bodies have an overall length of 32 ft. 6 in.

### LORRY BAN AT YORK?

FOLLOWING an appeal by the Dean of York, proposals for banning heavy traffic from the vicinity of York Minster are being considered by the city council. The Dean has warned that unless six roads are closed to lorries further damage will be caused to the Minster's fabric through vibration.

## Weights Raised in Every Application

IF substantial alterations were made to unladen weight, thereby increasing carrying capacity, the question must arise as to whether the operator was using the vehicle originally specified on the licence. Mr. W. P. James, West Midland Licensing Authority, said at Hanley last Friday.

He adjourned until today an application by the Sandford Hill Haulage Co., Ltd., Longton, Staffs, for the variation of a B licence. Mr. G. C. Tinsdill, who appeared for the company, will be required today to tell Mr. James whether an increase in carrying capacity is justified.

At last Friday's hearing he said the company were seeking to replace a four-wheeled tipper of 3½ tons unladen by a Thames six-wheeled oiler weighing 4½ tons. There would, however, be little increase in carrying capacity.

Mr. L. M. Hayes, a director, agreed that on his application form he had shown an increase of 1½ tons in carrying capacity, but that was a mistake. The old vehicle was fitted with a steel body in 1957, and when it was reweighed it was found to turn the scale at 3 tons 16 cwt. This change, which had been notified to the Licensing Authority in January, 1958, had increased the safe payload capacity by about 2 tons to 8½ tons. The new vehicle would carry about 9 tons.

Mr. James pointed out that the vehicle to be deleted originally replaced a tipper of 2 tons 14 cwt. unladen. By these recurring alterations the applicants had benefited substantially and obtained a much greater carrying capacity. The implications of the increases in unladen weight would have to be considered.

### U.T.A. "NOTICE BOARD" STRIKE

NEARLY 2,000 drivers and conductors employed by the Ulster Transport Authority staged a strike last Saturday because their employers had refused to allow certain trade-union notices to be displayed at depots. They threatened that unless agreement was reached they would strike again tomorrow and the following Saturday, and would later stage a complete stoppage by members of the Amalgamated Transport and General Workers' Union.

Crews in membership of the Ulster Transport and Allied Operatives' Union remained on duty.

### NEW CABS FOR BANTAMS

THE revised forward-control cab introduced last year for Commer and Karrier Gamecock forward-control vehicles is now available on Karrier Bantam 2-3-ton chassis and 4-5-ton tractors. It has a one-piece curved windscreen and a higher roof than the previous model to increase driving comfort.

On Bantam 2-3-tonners, there is an adjustable driving seat and a two-man bench seat, whilst the tractor units have two separate bucket seats. Upholstery is in two-tone leather cloth.

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## "12-seaters Without Certificates Carry Workers to Rocket Site"

A WITNESS before the Northern Traffic Commissioners at Carlisle, last week, alleged that employees at the Rocket Research Establishment, Spadeadam, were running 12-seaters for hire or reward without certificates of fitness. They were carrying passengers to the site.

This statement was made "off the record" by Mr. W. D. Sowerby, managing director of Sowerby's Tours, Gilsland, in answer to a question by Mr. J. A. T. Hanlon, chairman.

Passenger Transport (Gilsland), Ltd., The Garage, Gilsland, Carlisle, were applying for a new service from Carlisle to the Spadeadam Establishment. It was to be operated all the year round at times requested by Rolls-Royce, Ltd. An objection by United Automobile Services, Ltd., was withdrawn without prejudice to any future application.

Mr. T. H. Campbell Wardlaw said Passenger Transport (Gilsland), Ltd., were formed 2½ years ago for the sole

purpose of operating a pool of vehicles to carry workpeople to and from the site. British Oxygen (Wimpey), Ltd., who were responsible for the construction of the site, entered into a contract with the applicants for the provision of vehicles. There were four separate parties concerned in the company—Mr. J. S. Charlton, Miss H. Palmer, Mr. W. D. Sowerby and a Mr. Kitchen, all of whom were connected with other transport businesses.

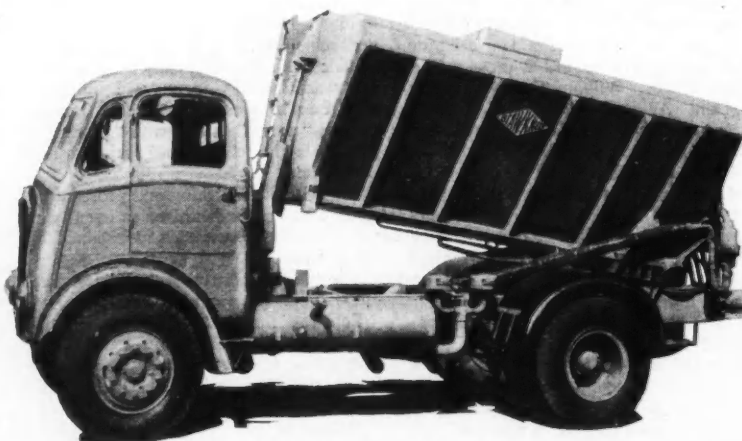
Passenger Transport (Gilsland), Ltd., worked the pooled vehicles of the four companies with whom the individual members were associated. About 41 buses operated into and out of the estate each day, carrying some 1,500 passengers.

Rolls-Royce were now occupying some of the premises. Their employees would be the future passengers, who would be required by Rolls-Royce to make contributions to the cost of transport.

In evidence, Mr. Sowerby said British Oxygen (Wimpey), Ltd., had paid a certain rate per bus per day, according to the distance covered. The longest distances were to Langholm in the north and Penrith in the south, both of which were more than 35 miles away.

After Mr. Sowerby had given his evidence on the operation of 12-seaters, Mr. Wardlaw said he (Mr. Sowerby) was a little concerned about it, because Press representatives were present.

The application was granted with a condition that no fares should be collected on the buses.



The Nagarjunasagar Dam Authority have taken delivery, through Larsen and Toubro, Ltd., Bombay, India, of a number of Thornycroft chassis with Blaw-Knox bulk-cement containers. The Auto-Airflow discharge equipment enables 20 tons an hour to be emptied up to a height of 75 ft. The container can hold 8½ tons, and a hydraulic ram has been provided so that unloading may be by gravity where there are bottom activators.

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## Rubber Suspension Used in Leyland 6 x 2 Tipper Conversions

FOUR Leyland Comet 6 x 2 tippers, equipped with Eaton-Hendrickson rubber-suspension bogies, are stated to be returning 14-16 m.p.g. on regular daily haulage. The vehicles are being operated by Burnett and Hallamshire Fuel, Ltd., 14 Wharf Street, Sheffield, and the success of these first four vehicles has resulted in a further three being ordered.

The lorries have Pilot bodies and tipping gear, and the Hendrickson bogie conversions were carried out by the Warrington Wheel Co., Ltd., Bowes Street, Warrington, who are conversion agents for the north and west of England. The Hendrickson bogies have trailing axles; the driving axles are Eaton 18500 two-speed units.

A payload of 14 tons is carried in service, and in one instance a fuel-consumption rate of 18 m.p.g. was returned. The maximum variation of chassis height between the laden and unladen conditions is stated to be 1½ in. The vehicles were supplied by Central Motors (Sheffield), Ltd., Sheffield, 1.

[A road-test report of a Leyland Comet with Eaton-Hendrickson conversion was published in *The Commercial Motor* on June 13, 1958.]



A Leyland Comet with Eaton-Hendrickson rubber-suspension bogie and Pilot tipping gear. It carries a 14-ton payload.

## Started in 1956: Now 17 Vehicles

A THREE-YEAR-OLD haulage company which now has 17 vehicles was last week authorized by Mr. S. W. Nelson, Western Licensing Authority, to replace a lorry of 4 tons unladen by a 15-tonner with a 24-ft.-long body and weighing 7½ tons unladen. The applicants were Edwards Transport (Lydbrook), Ltd., Wye Garage, Lydbrook, Glos.

Mr. Herbert Bendall, manager, said they began business in 1956 by buying vehicles with special A licences. They now had five vehicles on special A licences, four on ordinary A licences and eight on contract-A licences. Of the contract vehicles, three were operated for the Steel Company of Wales, four for Fred Watkins, Wyecliffe Quarries, Coleford, and one for Siemens Edison Swan Co., Ltd., at Lydbrook.

Mr. Nelson observed that the fleet's average earnings last year amounted to £5,000 per vehicle.

### Time and Labour Saved

Mr. Stanley Vaughan, purchasing officer of Siemens Edison Swan, said that the new vehicle would be of great assistance to them. It would enable one lorry to do the work of two, which would save time and labour. He required Edwards Transport to carry copper, lead and rubber, the main traffic being copper from Manchester.

British Railways were unsuitable for this traffic, he added, British Road Services were carrying the outward traffic, and if the application were granted the amount of work done by the railways and B.R.S. would not be affected.

When Edwards Transport said the vehicle would normally be used to carry copper, steel, metals, pallets, timber and foodstuffs within 150 miles, but generally would transport steel for the Steel Company of Wales, and goods for Siemens Edison Swan and for H. and W. Carter,

of Lydbrook, and their associated companies, B.R.S. and the railways withdrew their objections.

Mr. Nelson said there was a strong case for the additional unladen weight.

### INDEPENDENT WINS IN LONDON

MR. D. MUIR, Metropolitan Traffic Commissioner, last week granted a private operator a road service licence to operate in London Transport's territory. Banstead Coaches are to run a one-man-operated 29-seater from Banstead railway station to Woodmansterne, probably beginning on March 2.

The firm applied for permission to run the service as long ago as 1955, but London Transport refused to allow it. They changed their mind last year. The application was granted after the route had been varied to meet objections by the police.

### 18,360 NEW VEHICLES

EXCLUDING cars and motorcycles, 18,360 new vehicles were registered for the first time in November, 1958, compared with 21,488 in October. Details are given in the accompanying table.

NEW REGISTRATIONS—NOVEMBER, 1958

Type	Petrol	Oil	Electric	November	Jan.-Nov.
Hackneys .. .. .	75	204	19	298	4,723
Goods:					
Agricultural .. .. .	311	115	—	426	5,042
Showmen's .. .. .	—	—	—	—	26
Local Authorities (watering and cleansing) ..	2	10	—	12	234
Tower wagons .. .. .	1	1	1	3	91
Other goods .. .. .	10,758	3,422	151	14,331	154,945
<b>Total Goods</b>	<b>11,072</b>	<b>3,546</b>	<b>152</b>	<b>14,772</b>	<b>160,338</b>
Exempt vehicles .. .. .	245	178	15	438	9,414
Tractors .. .. .	3	31	2	36	556
Agricultural engines (£2 class) .. .. .	94	2,719	3	2,816	45,297
<b>Grand Totals</b>	<b>11,489</b>	<b>6,680</b>	<b>191</b>	<b>18,360</b>	<b>220,328</b>

**A**LTHOUGH traffic accidents in Great Britain have risen by 57,365 a year since 1951—an increase of practically 21 per cent.—the number involving Manchester Transport Department has remained almost stationary. Last year, Manchester buses had one accident for every 15,000 service miles, a figure that compares favourably with those of other large municipalities. Furthermore, boarding and alighting accidents have been cut from 2,224 in 1951 to 1,223 in 1957.

These impressive statistics, so inconsistent with the general trend, may undoubtedly be attributed to the care with which the transport department selects and trains its drivers and to the enthusiastic co-operation of employees on all matters related to accident prevention.

#### Aptitude and Ability

Under the present instruction scheme, driver trainees are treated individually rather than as a group, and may be accelerated or retarded through the course, according to their individual aptitude and ability. This policy was started 10 years ago and is evidently one of the keys to success.

An average of 500 to 600 men pass through the corporation's driving school annually—last year's total was 584—and few of them fail to attain the standard set by the Ministry of Transport test.

Before any employee can become a driver, however, he must first serve as a conductor to learn routes, traffic sense and appreciate fare-taking procedure. This conducting period also enables the department to assess the character and aptitude of a prospective driver.

Mr. J. E. J. Webb, personnel manager, considers that character is of paramount importance in drivers selected for public service vehicles. Apart from driving skill, explained Mr. Webb, essential qualities for which to look were stamina, sense of responsibility, constant vigilance, self-control and good memory.

Once recognized as potential drivers, conductors are selected for primary

training at the corporation's school. Situated on Manchester City's old football ground, this comprises class-rooms, demonstration room, cinema and a large training ground marked off with perimeter track, bends, junctions, skid pan and practically every form of hazard found on the road.

Here, under the tuition of skilled instructors, trainees are taught elementary vehicle handling and control on double-deck buses during off-duty periods. This primary training also enables the chief driving instructor, Mr. G. Walker, to decide whether a pupil is suitable to progress to more advanced training.

Remuneration at this stage is based on full-time conducting duties only; trainees are not paid for the time spent on initial instruction in the yard.

When a conductor is judged proficient in elementary handling, he is graduated to the three-stage driving course, which, according to the individual's ability, may involve anything up to 120 hours' tuition. From now on, trainees work four hours a

day on conducting duties and the remainder on driving instruction.

The first stage of the course involves 15 hours' theoretical training in the school's class-rooms and demonstration room. Here a pupil is taught the fundamental principles of road safety, the *Highway Code*, fire drill and the elementary mechanics of his vehicle. The speed with which he reacts to an emergency is also determined at this stage by means of a chronotron tester installed on a Leyland chassis.

Later in his training, a pupil is subjected to more severe practical tests designed to quicken his reactions. One such exercise takes place on the school's ground and consists simply of throwing a dummy in front of a trainee's vehicle. The stopping distance gives a fair indication of the driver's alertness.

After gaining preliminary handling experience, pupils are advanced to driving in light traffic on the road. By using carefully selected routes, trainees quickly gain adequate experience of cornering, close

## Bus Drivers Trained

### Care in the Selection and Training of Staff Improves Accident Statistics: Only 36 Failures Out of 1,614 Pupils in Three Years

(Right) Three trainees being instructed in the workings of an oil engine in the Department's lecture room. Last year, 584 men passed through the school. (Below) As the L-driver comes along, two men in a concealed position fling a dummy in front of his vehicle so that his reaction to this "emergency" can be studied. The stopping distance gives an indication of his awareness and speed of reaction.



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passing, reversing, judgment of vehicle length and width, restarting on a gradient and the normal hazards of driving.

During the early training period, pupils are frequently examined by the chief driving instructor; any failing to make satisfactory progress are immediately returned to full-time conducting duties. This has proved the most suitable method of elimination, for, as tuition costs are high, trainees showing lack of interest or aptitude cannot be allowed to complete the course.

Stage two consists of up to 36 hours' driving in heavier traffic. During this time pupils are taught the service routes on which they will ultimately be employed.

A notable feature of the double-deck tuition buses is their dual employment when on the road. While the lower deck is used for driving instruction—usually

two trainees to one instructor—the upper saloon is used as a mobile and realistic class-room for trainee conductors.

The final stage of training involves complete revision of all previous work, but is undertaken in the heaviest possible traffic by both day and night. Fifty-five hours are allocated to this stage, at the completion of which pupils are expected to have reached a high standard of proficiency.

During the past three years only 36 out of 1,614 trainees failed the Ministry of Transport test at their first attempt. Most of the initial failures were successful at the second try.

But a driver is not allowed to rest on his laurels, for the official test is followed by up to 10 hours' further instruction to bring him to what is proudly known as "Manchester's standard."

Manchester Corporation, like many other large municipalities, operate a variety of vehicles, including Leyland Tiger Cubs, double-deck Daimlers,

## By Roger Bell



*Under the surveillance of an instructor, a trainee driver takes a double-decker down a narrow lane of barrels. Road experience is later gained first in light traffic and then in heavy until a high standard of proficiency is attained.*

required for a trolleybus and is suitable for all primary training.

To explain and demonstrate the mechanics and complicated electrical circuits of a trolleybus to trainees, a special machine was built for the corporation by Metropolitan-Vickers. This consists of a mock trolleybus cab in front of which is a screen that displays in simplified form the many circuits and relays actuated by pressing the accelerator pedal.

Although the corporation prefer to

*A chronotron reaction tester is used to measure men's mental agility, being fitted to a Leyland chassis, with the control panel itself on the wall of the lecture room.*

train their own drivers, public service vehicle licence holders are occasionally employed after some months on conductor's duties. These men are given two days' training on a route on which they will then operate part-time. The remaining time is spent in learning all other routes operated from their depot.

Experienced drivers who do not hold a public service vehicle licence first serve as conductors in the normal way, and are then tested in town traffic to decide at what stage their training should begin.

A few years ago, Manchester Corporation in common with many other large

municipalities, found that some drivers were leaving the transport department immediately after their training had finished. They were merely using the corporation driving school as a cheap and easy way of obtaining a public service vehicle licence so that they could get employment with other concerns—principally coach operators in the district.

Consequently, an indemnity agreement was produced under which any driver leaving within a year of his training forfeits £15. Although a comparatively small amount, the indemnity has certainly acted as a deterrent against early leaving. Nevertheless, the annual turnover of employees is quite high—16.2 per cent. for drivers and 43 per cent. for conductors. This represents a total of some 500 new men to be trained by the department every year.

However, so long as the corporation maintain their long tradition of careful driver selection and training, the public of Manchester may rest assured that their city's buses are at all times in competent hands.

## ned from Scratch



Crossleys, Leylands and trolleybuses. Each demands an individual driving technique. For example, the double-deck buses alone account for five forms of gearbox—"crash," synchromesh, pre-selector, semi-automatic and fully automatic—each of which necessitates instruction of trainee drivers.

In general, pupils are taught to handle as many types of vehicle as possible, thus facilitating the transfer of drivers from depot to depot or service to service should the necessity arise.

The training school is not equipped with an electrified system for trolleybuses. This difficulty has been overcome, however, by using Daimler preselector buses of roughly the same size and weight, with third gear constantly engaged. Although performance is naturally impaired, the driving technique is identical to that

*Here a trainee is being taught how to use a claw jack, employed when a normal type cannot be applied.*





"WE shall have a honeymoon all right," said the operator, "but how long shall we enjoy it?" There must be few potential users of the London-Birmingham motorway who do not share his anxiety, stemming directly from the hazards inherent in the removal of speed restrictions for all classes of vehicle.

The multiplication of speed groups is thought by Mr. W. P. James, West Midland Licensing Authority, to be the most dangerous aspect of motorway travel: a large number of people will agree with him. There are those who, perhaps pessimistically, foresee the re-imposition of speed restrictions if serious multiple accidents occur within a few months of the opening of the road.

Safety, and the continued right to travel at unlimited speeds, will depend on the conduct of drivers and the suitability of their vehicles for continual high-speed running. The selection of the best tyre for the task may well be the operator's biggest difficulty.

If the initial accident rate is discouraging, it is to be hoped that the Minister of Transport will allow a reasonable proving period to elapse before taking drastic action. Hauliers who have given thought to the problems hope that the process of self-education will include the pre-planning of conservative schedules. Voluntary enforcement of moderate maximum speeds, proper regard for the condition of the vehicle and insistence on reasonable loadings are the essentials of motorway running.

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THE months that follow the opening of the new roads will provide the opportunity, for the first time, for the transport industry, the Ministry of Transport and the police jointly to appraise the problems of high-speed travel. It will be a period of challenge for all road interests and self-discipline is likely to be the most important lesson to be learned.

Taking the long view, many industrialists see the challenge as a national one. If Germany, they point out, is regarded as the most highly disciplined country in Europe, boasting the finest road network in the world, its accident rate, nevertheless, compares very unfavourably with our own. Motorway users, they say, must show that they are worthy of their new freedom in the testing time of the first few months.

#### Sympathy with Vehicle

The concept that a good driver acts in sympathy with a good vehicle is the basic safety principle underlying the work of the West Midlands Maintenance Committee and Ministry of Transport examiners. There is general support for this view from operators who intend to introduce regular services on the motorways.

Mr. G. R. Atkinson, of the West Midland Traffic Area, has emphasized the insidious effects of an increasingly serious mechanical fault on the reactions of a good driver. Michelin technicians have observed that a driver can deal adequately with a high-speed steering fault over a short distance without realizing its existence. Over a long period, however, its continuance results in nervous fatigue.

Measures to improve the safety

### *Strict Discipline, Moderate Maximum Speeds, Reasonable Loading and Adequate Brakes and Tyres are Necessary to Avoid Serious Accidents*

standards of current vehicles must take precedence initially over the design of types specifically intended for motorway use. In practice, the development of special vehicles may well depend on average standards of performance demonstrated by normal models in the first year. Severe speed restrictions could, of course, negate the value of producing vehicles with advanced features and effectively put the brake on design progress in this country.

So far as the technique of driving on the motorway is concerned, clearly, overtaking is the manoeuvre most fraught with danger. Timing when pulling out to overtake a slower vehicle and the method of signalling used should be studied by all road users, including operators' associations, British Road Services, the Ministry and the police. The enforcement of driving regulations and the compulsory use of signalling lights might well be placed high on the agenda for joint consultation.

When reviewing safety factors it would be appropriate to concentrate on goods vehicles rather than buses and coaches. In spite of the higher speed capabilities of public service vehicles, they normally operate well within their rated capacity, and the safety margin for brakes, tyres and other stressed parts is correspondingly

increased compared with the typical goods vehicle carrying its full load.

An old-established operator in the Midlands believes that a 20 per cent. overload on the gross weight is a common feature of trunk vehicles. If overloading of this order is allowed to continue on the motorways, a forecast of the problems associated with continuous high speed becomes impossible.

It is, however, likely that vigorous police action will reduce the number of flagrant breaches of gross-weight regulations. It is to be hoped that hauliers will co-operate in observance of the law.

They stand to gain more than any other group of road user from speed

de-restriction and they can demonstrate the economic advantages of a modern transport service run on modern roads. Solid support of enforcement measures by such bodies as the Road Haulage Association would bring lasting credit to the industry and free enterprise.

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JUST how fast is it expected that commercial vehicles will operate on the motorways? In the case of heavier machines in sound mechanical condition and capable of 50 m.p.h. or higher speeds, a continuous rate of 40-45 m.p.h. is regarded by most operators as the upper limit, laden or unladen. Trouble is expected at higher speeds, the precise nature of which depends on chassis characteristics, load and so on.

#### 40 m.p.h. the Limit

It is, perhaps, significant that a manufacturer of double-deckers views with some alarm the possibility of such vehicles being driven at speeds higher than 40 m.p.h. In this case, the trouble foreseen results from increased front-axle loadings caused by weight transfer under full braking.

The adequacy of typical braking systems for motorway use is a highly controversial subject. It is widely held by both vehicle and brake manufacturers that the reduced frequency of brake application on these highways will ensure freedom from fade when a "crash" stop has to be made. Nevertheless, comments by members of the Automobile Division of the Institution of Mechanical Engineers (*The Commercial Motor*, January 30) have made it clear that many technicians regard improved braking standards as essential.

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braking

It is sobering to recall a recent multiple accident on an American highway in which 30 vehicles were involved. No one suffered serious injury, but if one of the vehicles to join the later stages of the *mêlée* had been a fully laden "heavy" with below-average brakes, the story might have been very different.

Brakes of private cars compared with goods vehicles of over 5-ton capacity are said to enjoy a difference in efficiency of upwards of 25 per cent. Test figures for stopping distances are normally taken with a load giving a low centre of gravity. Raising the centre of gravity several feet adds substantially to the load transferred to the front wheels during heavy braking.

Not only is the overall efficiency reduced, but the stability of the vehicle is adversely affected if the brake system and steering are designed for a lesser weight transfer.

A statement made by Ferodo, Ltd., that "No brake facing—certainly no Ferodo brake facing—will fade as a result of one emergency stop," is reassuring. The company pointed out, however, that traffic conditions on the motorways might increase the severity of heat-spotting. This condition results from the localized metallurgical change caused by a very high rate of heating and cooling in a limited area.

## Vibration Causes Fade

An unusual cause of fade was described by Transport Brakes, Ltd. They told me that high-frequency vibrations in parts of the brake assemblies could reduce efficiency, and emphasized that the restriction of heat dissipation by the small clearance between wheel rims and drums could foster fade tendencies.

They also mentioned weight transfer and brake-torque reaction as potentially dangerous factors at high speed. It is claimed that the effect of these phenomena could be particularly unfavourable in the case of vehicles with twin-axle bogie suspension at the rear, because of the uneven loading and braking produced by wind-up of the springs.

Transport Brakes made a strong plea for close liaison between the makers of all brake assembly parts and the manufacturers of chassis and tyres.

A characteristic referred to by Small and Parkes, Ltd., is the speed sensitivity of braking systems, which tends to reduce retardation efficiency at higher speeds. The company observed that it could be tolerated so long as the driver had adequate experience of it. They advocated that drivers should practise high-speed braking on a deserted stretch of road before venturing on the motorway.

Technicians of the company told me that brake testing at high speed was a "must," but that this facility did not exist at the Motor Industry Research Association's proving ground at Lindley, where space was too limited to allow vehicles to reach their maximum speed on the level. They

Asks P. A. C. Brockington,  
A.M.I.Mech.E.

also strongly recommended a standard method of brake testing.

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SOME of the problems associated with high loads have already been mentioned. Another concerns the handling of vehicles in a high cross-wind, when stability will inevitably be impaired if a critical speed is exceeded.

High vans with a hanging load will be the most difficult to control with precision. If maintaining control taxes the mental, and possibly physical, resources of the driver to the limit, he will be unable to handle his vehicle safely for long periods.

It would be entirely unfair to discriminate against high-load vehicles and high vans without concrete proof of their instability under common high-speed conditions. Nevertheless, an immediate investigation of high-load and windage problems is an urgent necessity.

One might be tempted to think that no high-speed problem remains unsolved by operators on the Continent or in America. That may well be so, but the lessons learned by overseas manufacturers and hauliers are not immediately applicable to Britain. The relatively high ratio of laden to unladen weight, and the increased tyre loadings, that are common in this country, indicate that problems have to be studied from a different angle.

My observation of haulage opera-

tions in Belgium, Holland and Germany, and reports from British exporters to the Continent, indicate that Continental long-distance vehicles are maintained to a higher standard than they are in this country. This is the result partly of official enforcement in most countries, and partly of the realistic appreciation by operators of the value of mechanically sound running gear in the interests of safety and reliability.

Even small defects in brakes and steering gear can affect stability. Small and Parkes, Ltd., emphasized the related importance of the two systems. Their technicians reminded me that the amount of kinetic energy in the form of heat output that is dissipated by the brakes when applied hard at 60 m.p.h. was more than double that generated in a 40-m.p.h. "crash" stop.

## Effects on Steering

Differences in brake torque may have a marked effect on the steering when braking from high speed, although they may not be apparent at lower speeds. Correctly balanced braking is an essential safety factor.

Commenting on traffic conditions in America, a Michelin engineer referred to the development in that country of water-cooled multi-disc brakes to provide adequate stopping power for heavier commercial vehicles and trailer outfits. He also pointed out that highway tests in France, Germany and Britain indicated that break-up of the road surface was usually related to maximum axle loads and not to speed.

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IN the view of vehicle manufacturers and operators alike, the greatest single problem with which motorway users will have to contend is tyres. Heat build-up following a period of sustained high speed is clearly the chief hazard. It is one which is by no means unknown to the builder of fast saloon cars.

## Wind Resistance

Tests with experimental vehicles have shown that the power absorbed by the tyres at 100 m.p.h. sometimes equals the power absorbed by wind resistance. This normally represents a large proportion of the total resistance at higher speeds, because it increases as the square of the speed. At 100 m.p.h. it is therefore 100 times greater than the resistance at 10 m.p.h.

There is evidence that up to a critical speed in a given ambient temperature (and at higher speeds over shorter distances), premature deterioration of the carcase, including wear, will be reduced on the motorways.

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This is because fewer brake applications can be expected, and because the tread scuffing that occurs in high-speed cornering will be largely absent.

Assessment of this critical speed and the time element is best arrived at after practical tests. Heat generation is a product of the speed/load factor. If the vehicle is not overloaded an increase in speed will cause the onset of critical heat build-up more frequently than a proportionate increase in weight carried.

A senior member of the Michelin company pointed out that the metallic construction of their X-type tyre aided heat dissipation. The reduction in wall thickness for a given rating reduces the amount of heat generated.

### Hot-spots in Tyres

The company have found that excessive heat generation creates hot-spots that can lead to local separation of the structural elements. A heat burst may not follow immediately, or even for an appreciable time. The damage may be revealed, however, after a heavy impact with the kerb.

The Michelin representative also said that Continental makers offered tyres for high-speed running that contained less material than equivalent British types. They generated less heat under critical conditions at the cost of some reduction in mileage life.

Apart from the avoidance of high maximum temperatures, rapid heat dissipation is also advantageous in that it reduces the temperature gradient throughout the tyre.

Michelin's claims are supported by the test staff of a leading British oil-engine manufacturer, who have been engaged in sustained high-speed tests with 7-tonners on the M.I.R.A. proving ground for the past two years. From 10 a.m. until 6 p.m., five days a week, the vehicles circulate, with a change of driver at appropriate intervals. Vehicles are at rest for very short periods, so that reduction in tyre temperatures is insignificant.

When the tests began, a speed of 45 m.p.h. was maintained. This was later increased to an average of 50 m.p.h., with running speeds of 55-60 m.p.h. The camber of the lower track used for the tests was found to increase heat generation; during the summer, with high ambient temperatures, a tyre was apt to burst every four hours.

On changing to the Michelin X tyre, the burst period was dramatically extended to nine months and the life between replacements from 10,000-12,000 miles to 20,000 miles. During these tests, a fairly high incidence of rim-cracking and stud fracture was noted.

The theory that a lightweight tyre is less susceptible to damage caused by high speed was confirmed by a number of tread separation failures in the case of tyres with a high ply-rating in which the tread was completely detached from the carcass by centrifugal force.

In anticipation of "significant effects" which running on the motorways will have on tyre performance, Firestone have been conducting a research project which will help customers in the selection of the most suitable tyre for the speeds and loads envisaged.

### Nylon Resists Heat

The value of nylon as a heat-resisting carcass-cord material was specially mentioned by Goodyear. They stated that the established types of tyre produced by the company were designed for high-speed operations. They added, however, that users would be responsible for the condition of the tyres employed and that poor-quality or worn covers would not be suitable.

Discussion with operators and manufacturers has convinced me that at no time has there been such universal willingness in all sections of the industry to co-operate with the authorities in exploiting a national asset.

## 1958 Exports Earn over £89m.

EXPORTS OF 7,666 commercial vehicles in December brought the total for 1958 to 121,685, valued at £89,656,923. This was a poorer performance than in 1957, when 130,794 vehicles were sold overseas and earned £96,961,051. Shipments in 1957 may have, however, been inflated by numbers of vehicles which would have been exported in the latter months of 1956 but for the Suez crisis and its attendant shipping difficulties.

Totals of commercial vehicles sent to leading markets last year, with 1957 figures in parentheses, are as follows:

Australia 14,910 (15,950); South Africa 10,936 (13,300); Ghana and Nigeria 7,190 (7,212); New Zealand 5,670 (8,142); Denmark 5,132 (4,692).

In the first nine months of last year, British imports by the European Common Market countries were: Belgium, 1,319; France, 17; Holland 1,584; Italy, 9; Western Germany, 29; Luxembourg, 9; total, 2,967, which compares with a sum of 19,052 for the whole of Europe.

Compared with 290,122 vehicles made in 1957, the 1958 figure of 312,856 indicates a higher demand from the home

NEW COMMERCIAL VEHICLE EXPORTS—DECEMBER, 1958

Type	December		January—December	
	No.	Value £	No.	Value £
Goods vehicles, complete and chassis, assembled and unassembled ..	6,585	4,587,894	105,208	65,912,176
Motorbuses and trolleybuses, complete and chassis ..	406	770,019	5,775	10,710,404
Road haulage tractors ..	32	97,849	541	1,572,910
Other descriptions, complete and chassis ..	10	229,407	1,007	2,843,195
Dumpers and dump trucks ..	—	445,682	—	3,454,385
Industrial trucks ..	—	197,609	—	2,182,895
Trailers ..	633	157,906	9,154	2,980,958
Totals	7,666	6,486,366	121,685	89,656,923

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PRODUCTION—DECEMBER

	Dec.	Jan.—Dec.
Goods vehicles, tractors and special types		
Under 15 cwt.	14,826	166,027
15 cwt.—3 tons	6,697	69,157
3—6 tons	3,779	37,549
Over 6 tons	3,345	30,257
Total	28,647	303,010
Passenger vehicles		
Motorbuses, single-deck	770	7,191
double-deck	214	2,439
Trolleybuses	5	216
Total	989	9,846
Grand Total	29,636	312,856
Weekly Average	5,927	5,903

market. Last year's production is detailed in tabulated form on this page, together with export statistics.

Production in 1957 comprised 278,755 goods vehicles, road-haulage tractors and special types, 1,869 battery-electrics (no longer separately classified), and 9,498 passenger vehicles. Of the goods vehicles, etc., 142,148 were of up to 15 cwt. carrying capacity, 61,355 15-cwt.-3 tons, 51,234 3-6 tons, and 24,018 over 6 tons. These are all below the 1958 figures, except in the 3-6-ton category, in which 37,569 vehicles were produced last year—a substantial drop that may reflect the effects of purchase tax.

## New Equipment

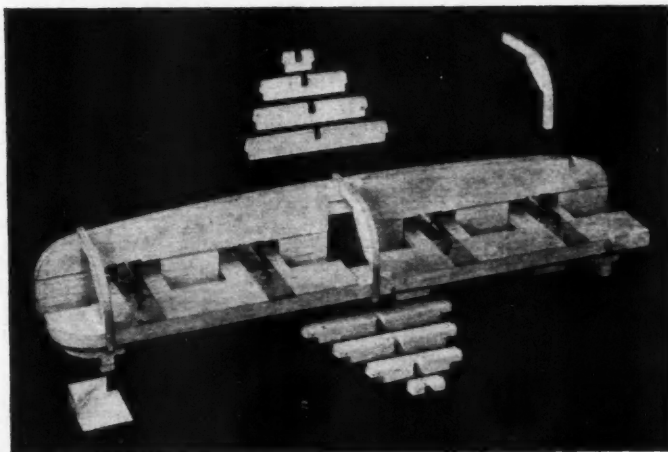
### Edge Filters

**D**ESIGNED primarily for lubricating and fuel oils, distillates, paints and greases, three new filters have been produced by Auto-Klean Strainers, Ltd., Hounslow, Middx. They embody the principle of edge filtration, the elements consisting of metal washers precisely spaced apart by distance pieces on a central spindle.

The size of the interstices between the washers is governed by the application of the units, and for petrol may be 0.003 in. and derv 0.001 in. On each side of the element are two rods on which there are blades which penetrate into the spaces between the washers. These blades remove accumulated dirt when the spindle is turned by a knob on the top of the unit.

The filters vary in size and capacity. The smallest, the type 05C, can filter 40 gal. of petrol an hour under gravity feed. The type 10GA is capable of passing 40 g.p.h. of derv under gravity feed, and the 15JA 120-200 g.p.h. according to the type of element fitted.

*Capable of being set up in a few minutes, this roof jig is the invention of Mr. L. A. Temple.*



means of a lever. When this is actuated, the wheel is in balance and the operator may read the spirit level. The lever is then returned to secure the wheel so that weights may be attached.

### Clockwork Cutting

**S**UITABLE for use with the Sifbronze Demon cutting blowpipe, the S.I.F. Colibri cutting machine operated by a clockwork motor is being marketed by Suffolk Iron Foundry (1920), Ltd., Stowmarket. The machine has a heavy Garrard spring motor housed in a steel box. When wound up, a length of 11 ft. 6 in. can be cut at a time.

The drive on the sheet metal is by a toothed wheel on the left of the housing, the wheel being protected from the heat emitted by the blowpipe. Cutting speed can be regulated for thicknesses of sheet between 1/8-3 in. and braking is actuated by a knob. On the

right of the housing is a small jockey wheel, which can serve as a guide when straight lines have to be accurately cut along smooth angle bar.

When this kind of work is done, a blowpipe holder is fitted at the rear and is provided with a second guide wheel to ride on the bar. For cutting rings and flanges an adjustable square rule with centring pins is provided.

### Master Jig

**A** THREE-DIMENSIONAL master roof and cab jig which enables setting-up to be done by unskilled labour has been invented by Mr. L. A. Temple, 30 East Churchfield Road, London, W.3, and is covered by a provisional patent. The jig can be set up in a few minutes and operates on a precision-sliding principle in three directions, calibrated blended radius inserts being used.

### Lighter Skid

**A** BARREL skid made of light alloy and stated to be only half the weight of comparable steel products has been produced by Messrs. Powell and Co., Burry Port, Carmarthenshire. The runners have wood battens along the tops of the metal sections, and the hooks are of steel. The battens can easily be replaced when worn. The lengths in which the skid is made are 8 ft., 10 ft. and 12 ft.

### Resistant Gaskets

**A** GASKET material produced by the Armstrong Cork Co., Ltd., Aldwych, London, W.C.2, composed of cork particles enclosed in a special binder is said to be highly resistant to the passage of fluids or gases.

Oil penetration of a gasket made of the material, which is called Uniphase cork, is stated to be only one-third that of a cork or synthetic-rubber gasket, and one-fifth that of a conventional cork composition gasket. It is capable of withstanding flange loads as high as 4,000 p.s.i.



*The Repco wheel balancer is of the spirit-level type.*

This may have interstices of either 0.0015 in. or 0.003 in.

Although not primarily designed for water separation, the filters have good capability in this respect.

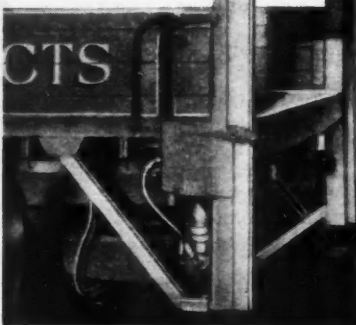
### Higher Loading

**A**N improvement has been made by Edbro-B. and E. Tippers, Ltd., Quebec Street, Bolton, Lancs, to the tailboard loader which they introduced at the Commercial Motor Show last year. This hydraulically operated 15-cwt. device has been developed for use on vehicles with double loading decks, and can raise goods to a height 3 ft. above the basic platform.

### Spirit-level Balancer

**A** STATIC wheel balancer of the type incorporating a circular spirit level is being marketed by Repco, Ltd., 59 St. James's Street, London, S.W.1. The cone can be fitted with an adapter plate of any size suitable for the wheel and is raised by

*The Edbro-B. and E. 15-cwt. tailboard loader is now offered in such form that goods can be elevated not only to platform height but also 3 ft. above.*



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Jan.—Dec.

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**F**OG, ice and snow made conditions particularly difficult when testing the Dodge forward-control 7-ton tipper, both on and off the road, but despite these adversities the tests showed that this is one of the most rugged vehicles of its type at present manufactured in quantity in Great Britain. Not that the weather unduly affected the occupants of the cab, for a most efficient heating system successfully counteracted the temperatures below freezing point that prevailed during the three-day test.

The additional margin of strength built into the chassis is such that the manufacturers themselves are prepared to accept an overload of 20 per cent., subject to the provision of suitable tyres. Like any other manufacturer, Dodge Bros. (Britain), Ltd., are fully aware that all their chassis are liable to be overloaded and the 7-tonner virtually becomes a 14-ton gross chassis and with a specification to match.

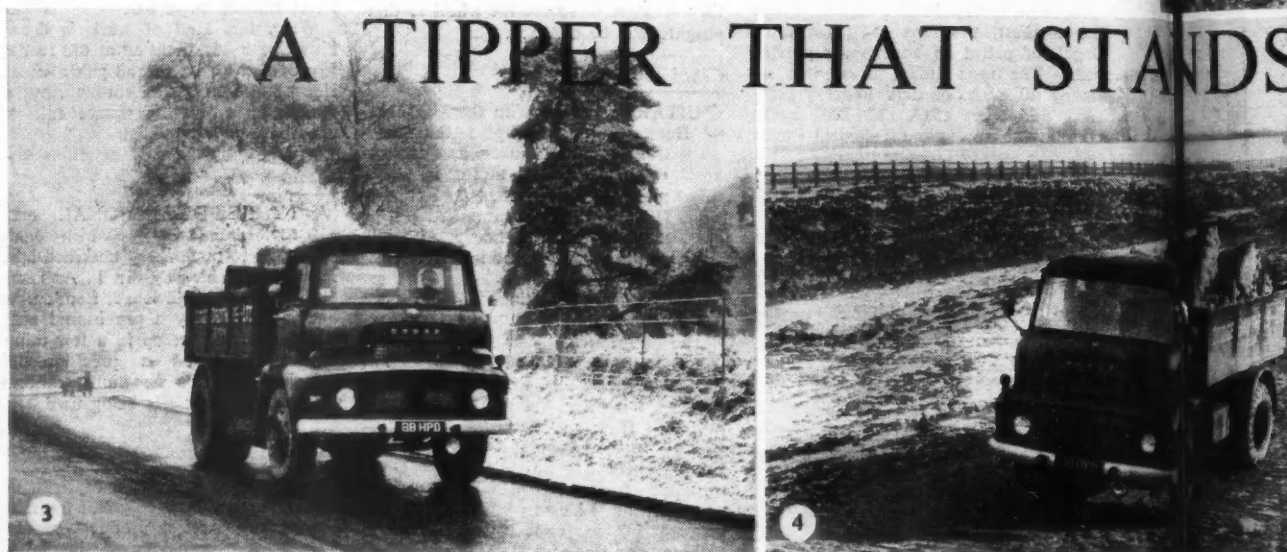
Some idea of the arduous conditions under which a vehicle of this type is expected to work was gained from a morning spent on the site of the London-Yorkshire motorway, where heavy clay is being excavated to form a cutting on the new road. Few vehicles could be expected to operate

two of which are alligator-jawed boxed top-hat sections, are riveted in position to make the frame exceptionally rigid, whilst spring-hanger brackets and so forth are bolted.

A choice of two power units is offered in the chassis. One is the Leyland O.350 5.76-litre 105 b.h.p. oil engine (as tested) and the other the Perkins R6 Mk.2 5.56-litre unit, which develops 104 b.h.p. With either engine a five-speed constant-mesh gearbox with direct-drive top gear is specified.

The standard single-speed spiral-bevel rear axle has ratios of either 6.16 or 6.66 to 1, but two Eaton two-speed axles are available. These are the 16800, which has ratios of 5.62 and 7.81 to 1, or the 18500, which is offered with ratios of 5.571 and 7.599 to 1 or 6.5 and 8.866 to 1. The 18500 axle, which was fitted to the test vehicle, is recommended when overloading is expected.

*Dodge 7-tonner Put Through Gruelling  
Three-day Test: Bad Conditions and  
Overloading have Little Effect: Leyland  
Oil Engine Provides Ample Power*



on this kind of job for more than a few weeks without trouble, but if any could, I would lay my money on the Dodge, which has the chassis frame and suspension to haul concentrated loads weighing at least 9 tons over uneven surfaces and up steep gradients.

Although it is heavier than other 7-ton tipper chassis in a similar price range, this is not detrimental in terms of running economy. Even when carrying a payload of nearly 8½ tons, the vehicle returned 12.6 m.p.g. over an undulating route at an average speed of 27.6 m.p.h., which is a favourable figure with a gross weight of well over 13 tons.

Safety has not been neglected either, for the braking system of the test vehicle was air-hydraulic, which is an alternative to the standard vacuum-hydraulic system. Unfortunately it could not be tested to the full because of dangerous road conditions, but it is undoubtedly powerful enough to cope with any practicable overload, whilst the use of a conventional brake pedal ensures fully proportional braking at all times.

The frame has ⅝-in.-thick side members, with a maximum depth of 10 in., and 3-in. flanges. Five cross-members,



(6) Apart from sticking tailboard catches, there was no difficulty in the load of gravel used for the normal road section of the tests. This picture clearly shows the twin rams of the Edbro-B. and E. W3 body tipping gear.

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By John F. Moon, A.M.I.R.T.E.



# STANDS OVERLOADING



(1) Care is necessary when tipping with a load of wet clay, as recklessness can result in the body being wrenched off its mountings. (2) The Dodge tipper was loaded by one of Sheppard's Ruston-Bucyrus 22-RB drag-lines, equipped with a  $\frac{3}{4}$ -cu.-yd. bucket. Each bucket-load weighed about  $1\frac{1}{2}$  tons. (3) The tipper is seen in wintry conditions near Sunningdale during the "overload" fuel-consumption test. (4) Carrying some 9 tons of heavy wet clay, the tipper leaves the excavation site on the London-Yorkshire motorway en route for the tip. The ground was reasonably level, but slippery. (5) The last bucketful of a load is dropped on to the Dodge tipper. Some difficulty was experienced backing into this position when unladen because of the light rear-axle loading, unsuitable tyres and slippery ground.

Dual-rate semi-elliptic springs are employed at both axles, the front springs being 50 in. long and the rear 48 in. long. Helper springs can be fitted at the rear axle for overload and stability purposes, and telescopic dampers are available for the front axle. Such dampers are also offered for the rear axle, except, as in the case of the test vehicle, when an 18500 two-speed axle with electric shift is fitted. The electric shift is employed when air-hydraulic brakes are specified.

The front axle is rated for a loading of 4 tons and the high-efficiency cam-and-lever steering gear has a ratio of

20 to 1. Hydrosteer power assistance for steering is standard and is particularly useful on a forward-control tipper, the front-axle loading of which is naturally higher than that of a normal-control design. Power steering is an alternative to the employment of low-ratio steering gear, which necessitates more turns of the wheel from lock to lock.

An all-steel cab is standard on the Dodge forward-control range and is flexibly mounted on the chassis frame at four points. Features include a wide one-piece curved wind-screen, rear quarter lights, double-skinned rear panel and lined roof. Smiths heating equipment is available. It consists of fan units at each side of the cab, which can be used to warm the interior with either fresh or recirculated air, or to draw in fresh cool air. Its efficiency, and that of the accompanying four-duct demisting system, was both marked and welcome during the test.

A standard Dodge 6-cu.-yd. timber tipping body with metal-lined floor was fitted to the test vehicle. The normal tipping gear provided is the Edbro-B. and E. W3 underbody type, which has two telescopic rams side by side to give a high measure of lateral stability when tipping.

## On the Road

FOR the first day's testing the Dodge had been loaded with 7 tons 2 cwt. of washed gravel, which, complete with myself and Dodge driver in the cab, brought the gross running weight to 11 tons 17 cwt. The winter was at its worst, fog limiting visibility, and ice and snow making the roads treacherous, but it was possible to conduct a 21-mile fuel-consumption test between Egham and Camberley.

This is an undulating route with heavy traffic and, despite the bad weather, we were able to average 27.1 m.p.h. The fuel-consumption rate for this out-and-return run was 13.1 m.p.g., and where possible the tipper was driven at 33-34 m.p.h. in top gear, high axle ratio.

Because of the slippery roads, brake tests were out of the question, but some acceleration figures were taken which showed that through the gears they were better when the high axle ratio was engaged, although the low axle ratio is advisable for maximum performance when pulling away in direct drive from 10 m.p.h. During the direct-drive tests, the engine and transmission were smooth at all speeds between 10-30 m.p.h.

Weather conditions were little better on the second day of the test, but another fuel-consumption run was made over the same route with concrete blocks totalling 1 ton 6½ cwt. dumped on top of the ballast to provide an overload approximate to that tolerated by the manufacturers. Traffic was lighter, with the result that the course was covered at a slightly higher average speed.

The resulting fuel figure of 12.6 m.p.g. is good and the fact that it was little heavier than the return obtained with the correct payload helps to indicate that the overload had remarkably little effect on performance. Indeed, it became really apparent only on hills by virtue of slightly reduced top-gear performance.

### Brake Tests Tried

After this test the overload blocks were removed and, because road conditions looked better, an attempt was made to obtain braking figures along the Great Chertsey Road. The appearance of the roadway was deceptive, however, because although it looked dry it was still frost-laden, and a full-pressure stop from 20 m.p.h. resulted in a dangerous left-hand tail slide, all wheels locking as soon as the brakes were fully applied.

Nevertheless, a measurement was taken—32 ft.—and the Tapley meter showed that the maximum retardation had been 67 per cent. With a dry road the tipper would probably have stopped in under 20 ft. from 20 m.p.h. and well under 50 ft. from 30 m.p.h.

Carrying on along the Chertsey Road, the lorry was run into a gravel pit at Sunbury, where the load was discharged. The vehicle was then taken back to the Camberley-Egham course, where an unladen test was conducted which produced a figure of 20.6 m.p.g. at 29.5 m.p.h. average speed. Under normal conditions the overall average is likely to have been about 16 m.p.g., neglecting fuel used at each end of a run in shunting and tipping. Even assuming the worst terminal conditions the overall fuel figure should rarely drop below 14 m.p.g.

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## Working on Site

FOR off-the-road tests it had been arranged with Mr. George Mapp, transport manager of F. and R. Cawley, Ltd., 62 Wingate Road, Luton, that the vehicle should be worked in with some of their own and sub-contractors' lorries on section B1 of the London-Yorkshire motorway at Salford, close to overbridge structure No. 40 where a minor road crosses the motorway. This site is being worked by George Sheppard (Hatfield), Ltd., Hatfield, Herts, and 1m. cu. yd. of clay has been shifted by Sheppard's in the past nine months. At one stage 60,000 cu. yd. of clay was excavated and carted away

(Continued on page 85)

### ROAD TEST No. 651/M110—DODGE FORWARD-CONTROL 7-TON TIPPER (LEYLAND OIL ENGINE)



FIRING ORDER 1-5-3-6-2-4  
COMPRESSION RATIO 16:1  
VALVE CLEARANCE 0.020"

MODEL: Dodge 3144BY 9-ft. 8½-in.-wheelbase forward-control 7-ton chassis with Leyland oil engine, standard all-steel cab, and 6-cu.-yd. tipping body with Edbro-B and E W3 underbody tipping gear.

#### WEIGHTS:

	Tons	cwt.	qr.
Unladen (kerb weight) ..	4	11	2
Payload ..	7	2	0
Driver, observer, etc. ..	3	2	
	11	17	0

#### DISTRIBUTION

Front axle ..	3	19	1
Rear axle ..	7	17	3

ENGINE: Leyland O.350 Mk. III six-cylindered direct-injection oil engine; bore 100.6 mm. (3.96 in.); stroke 120.7 mm. (4.75 in.); piston-

sweep volume 5.76 litres (351 cu. in.); maximum net output 105 b.h.p. at 2,400 r.p.m.; R.A.C. rating 37.6 h.p.; maximum net torque 255 lb.-ft. at 1,400 r.p.m.

TRANSMISSION: Through 12-in.-diameter single-dry-plate clutch to five-speed constant-mesh gearbox, thence by single propeller shaft to the Eaton 18500 two-speed fully floating hypoid-bevel rear axle.

GEAR RATIOS: 7.08, 3.78, 2.24, 1.47, and 1 to 1 forward; reverse 7.01 to 1; rear-axle ratios 5.571 and 7.599 to 1.

BRAKES: Clayton Dewandre-Girling air-hydraulic system, with two-leading-shoe units at all wheels. Single-pull hand brake linked mechanically to rear wheels only. Diameter of drums, front, 16 in., rear, 15.25 in.; width of facings, front, 3 in., rear, 5 in.; total frictional area 480 sq. in., that is 40.5 sq. in. per ton gross weight as tested.

FRAME: Pressed-steel channel section, with five cross-members riveted in position. Spring hanger brackets bolted to frame.

STEERING: High-efficiency cam and lever, with Hydrotec hydraulic servo.

SUSPENSION: Dual-rate semi-elliptic springs with helper springs at rear axle and telescopic dampers at front axle.

ELECTRICAL: 24v. compensated-voltage-control system with 67-amp.-hr. battery.

FUEL CONSUMPTION: (a) 7.1-ton payload, 13.1 m.p.g. at 27.1 m.p.h. average speed; (b) 8.425-ton payload, 12.6 m.p.g. at 27.6 m.p.h. average speed; (c) unladen, 20.5 m.p.g. at 29.5 m.p.h. average speed, that is 155 gross ton-m.p.g. as tested (a) and 166 gross ton-m.p.g. as tested (b), giving time-load-mileage factors of 4,200 (a) and 4,582 (b).

TANK CAPACITY: 26 gal., range approximately 340 miles.

ACCELERATION: Through gears (high axle ratio), 0-20 m.p.h., 17.8 sec.; 0-30 m.p.h., 39.25 sec.; direct drive (low axle ratio), 10-20 m.p.h., 18.5 sec.; 10-30 m.p.h., 39.5 sec.

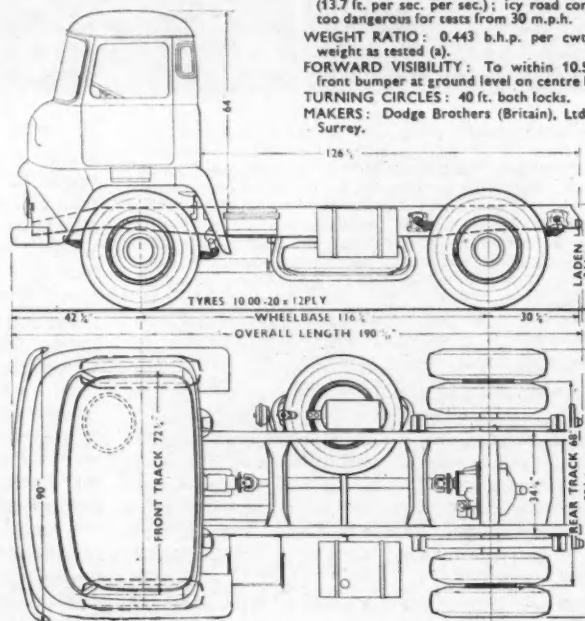
BRAKING: (icy asphalt): From 20 m.p.h., 32 ft. (13.7 ft. per sec. per sec.); icy road conditions too dangerous for tests from 30 m.p.h.

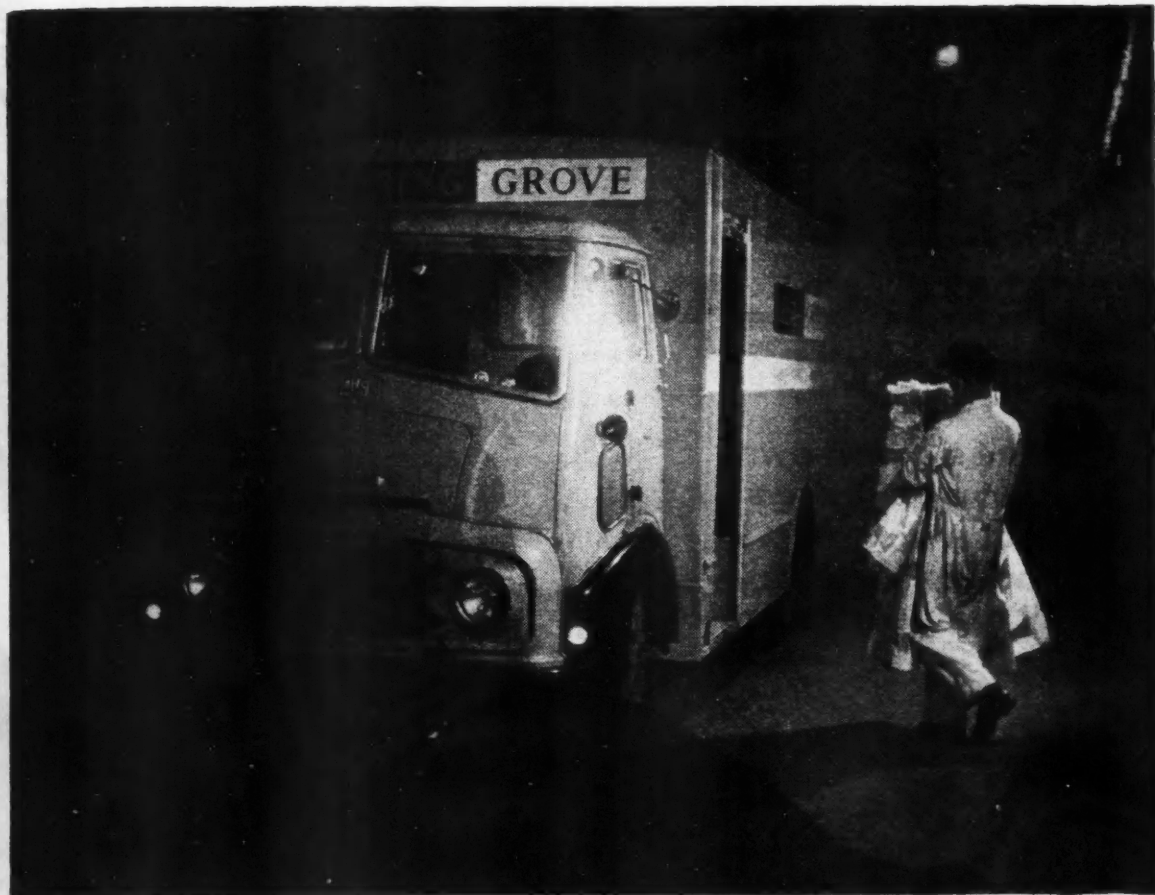
WEIGHT RATIO: 0.443 b.h.p. per cwt. gross weight as tested (a).

FORWARD VISIBILITY: To within 10.5 ft. of front bumper at ground level on centre line.

TURNING CIRCLES: 40 ft. both locks.

MAKERS: Dodge Brothers (Britain), Ltd., Ken Surrey.





## Fog blankets London — but these Austins get through

LONDON S.W.19. Evening, winter. Fog oozes through the streets, choking and blinding. Nosing through this thick blanket, an Austin 4 tonner moves steadily, bright pink in the darkness.

The A41. Two hours before dawn. Ahead Birmingham casts an angry glow. Along the road flurries of snow cover the ice patches. Warm in the cab, a driver steers a bright pink Austin 5 tonner through the night.

**All day long, all year round** Every hour of every day, every day of every year, these Austins are on the job. They work for Spring Grove Laundries Ltd. And Spring Grove have important contracts that can't wait on the weather. Contracts with hospitals, industries, power stations, airports and hotels.

Spring Grove of Isleworth—one of Britain's largest group of commercial laundries—supply linen to 54 major hospitals in and around London. Every day 12 Austin 5 tonners are on this job alone. Through the night Austin 3 and 5 tonners take the road north from

Hertford with loads for Birmingham's and Manchester's industries.

**Thousands of customers** Austins serve 60 power stations from Southampton to the Wash, deliver Rolomatic towels for B.E.A. cloakrooms at London Airport, supply over 7,000 customers in hotels and factories who use the Spring Grove linen hire service.

Spring Grove also keep the man in the street well-dressed. They have a dry cleaning service operating from Staines to Plaistow. The 4 ton Austin diesels on this service are on collection and delivery work from 8 a.m. to 2 a.m., cover over 1000 miles a week.

**The trucks for the job** Reliable transport is essential for this job. Spring Grove could not take chances. They put their transport in the hands of specialists—Blox Services Ltd., of Morden. Mr. R. P. C. Block, Managing Director of this transport hire firm, recommended Austins. Spring Grove now have 32 on contract—3 A40s, the rest 3, 4 or 5 ton f.c. diesels.

Mr. Block, with over 70 Austins in

his fleet, and 30 years' experience of vehicles of every kind, says, "Of the popular makes we have found Austins the most robust. They compare very favourably for economy of running and maintenance—and they're thoroughly reliable. Also Austins have the range to suit all trades. I've just ordered three new 3-tonners and I shall go on using Austins."

The Austin vehicles used by Blox Services Ltd., are supplied by L. F. Dove (C.V.) Ltd., Croydon.

The all-purpose Austin commercial range includes vans from  $\frac{1}{2}$  to 2 tons, trucks from 3-7 tons, plus 5 ton tippers and 15-ton gross train weight prime movers. All available as chassis/scuttle/cab units. All warranted for 12 months. All backed by B.M.C. Service—Britain's best service and parts organisation.

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**extra grip -  
extra safety**  
all the year round!



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OWN ROAD-TESTED TYRE

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in 10 weeks, and all the haulage has been sub-contracted to Cawley's.

The Cawley fleet totals 17 vehicles, including five Dodge semi-forward-control 6-ton tippers and three Dodge semi-forward-control 7-ton tippers, all with oil engines. The transport side of Cawley's was started eight years ago with five tippers.

The journey from Luton to Salford was made over icy roads, with an ambient temperature of 28°F. Despite the relatively low rear-axle loading—a feature sometimes held against forward-control tippers—only once or twice was rear-end sliding noticed, although the vehicle was being cruised at 40 m.p.h. for the most part. The approach to the excavation site from the road was made down a smooth gradient of about 1 in 15 and, although the ground was frozen hard, the wintry sun made the top surface slippery. Even walking was difficult.

Two Ruston Bucyrus 22-RB draglines with  $\frac{3}{4}$ -cu.-yd. buckets, together with a Caterpillar bulldozer, are normally employed on loading operations on this section, although one of the 22-RBs was temporarily out of action while we were on the site. The clay being loaded had a high water and ice content and each bucket load must have weighed in the region of 1½ tons.

Taking our turn in the queue, the Dodge was backed up towards the excavator and a certain amount of wheelspin was experienced. Once in position, six bucket-loads were emptied into the body, and the tipper was driven from the site without much difficulty and across to the tip some four miles away.

### Precautions Against Sticking

To prevent the wet clay from sticking when tipped, the bottom of the body had been swilled out with derv. The wisdom of this precaution was seen at the tipping site, because the body had to be elevated to at least 45° before the load started to move. Once it did move, however, the weight of it behind the pivot point brought the body up with a bang, extending the rams to their full extent and imposing a great strain on the body, its underframe, tipping gear and the chassis.

This type of load is one of the most damaging that can be carried. Bodies have often been ripped from the chassis and tipping gears wrecked because of the impact of the body when suddenly tipped as the load moved, and, as witnessed on the site, a little carelessness can even result in the vehicle standing up on its rear wheels.

The Dodge was then returned to the site for a second load and this time I drove. Backing into position under the dragline was not as easy as it looked. Only a narrow pathway had been smoothed out by the bulldozer and, not realizing how much space there was on the near side, I drove too far to the right with the result that I got on to the rough stuff and rear-wheel traction was lost.

A certain amount of see-sawing and manual labour was necessary to get clear, although the wheelspin experienced might not have occurred had the tyre pressures been lower. Low rear-axle loading when empty is, however, one of the factors generally aimed against a forward-control design for tippers.

Six bucket-loads were then emptied into the body, giving a payload of about 9 tons, despite which the vehicle was easy

to drive out on to the road. The steering power assistance made light work of pulling the front wheels out of the ruts and so forth. Between the site and the tip the Dodge was well able to keep up with other traffic despite the large overload and, although the engine was obviously working hard, there appeared to be plenty of power in hand.

The only difficulty experienced in tipping was in freeing the locking cam bar which secures the bottom of the tailboard. The pressure of the clay inside the body forced the tailboard against the bar and made it difficult to release the securing chain. Normally, pins are put in the two lower hinge brackets to secure the tailboard, but as these had already been found difficult to remove on previous occasions they had been left out for this run.

### Handling Characteristics

TAKEN generally, the Dodge 7-ton tipper is a pleasant vehicle to drive, although the short gear lever is set back rather a long way and calls for an awkward stretch when changing gear, particularly when using second and third ratios. Engine-cowl insulation is most effective, thus relieving the driver of noise fatigue.

The power steering works well and, although its setting is such that the wheels can be moved when the vehicle is stationary, the amount of "feel" when travelling on the road at normal speeds is not unduly low. In the straight-ahead position there is a slight tendency to wander which demands constant correction at the wheel and can at first be disturbing. Although not tested to the full, the brakes felt dependable and the use of a long-travel pedal is much preferable to the treadle type often encountered with air-hydraulic systems.

### Good Suspension

Both laden and unladen, the suspension gave a good ride and when running over rough surfaces without a load the absence of cab and body rattles was noteworthy, suggesting that the dual-rate suspension should give marked benefits in respect of increased chassis life. The seats are comfortable, the driver's seat being adjustable fore and aft and vertically, although I would prefer a little more support for the small of the back.

All the instruments are centrally grouped and marked in white characters on a black background. I was particularly impressed by the side and headlamp switch, which is of the finger-tip "flap" type mounted on a stalk protruding from the right of the steering column. It makes it easy to give light signals to other drivers and avoids the long stretch so often encountered on commercial vehicles.

The basic Dodge 7-ton tipper chassis, with Leyland oil engine and the standard cab, costs £1,718 and in Great Britain £377 7s. 8d. purchase tax is payable. The 6-cu.-yd. tipping body and gear add a further £220 to the basic price and other extras fitted to the test vehicle included the Eaton 18500 two-speed axle with electric shift (£172 10s.); 10.00-20-in. (12-ply) tyres (£41 17s. 11d.); front dampers (£7 16s. 3d.); rear helper springs (£10); air-hydraulic brakes (£67 10s.); cab heating and demisting gear (£20 10s.); and flashing direction indicators (£10).

## Operated Illegally for Years, Objector Alleges

THE applicants had been systematically operating illegally to Margate for many years, Mr. H. Backhouse told the North Western Traffic Commissioners during the concluding stage of an application by Pleasureways (1955), Ltd., Oldham, for a new seven-day tour to Margate.

He was objecting for W. Robinson and Sons (Great Harwood), Ltd., Happiway Tours (Manchester), Ltd., Stanley Spencer Tours (Manchester), Ltd., and Smiths Tours (Wigan), Ltd.

He claimed that illegal working by Pleasureways had been so blatant that when Mr. A. C. Bond, general manager

of Happiways, had applied early last year to run to Margate, he believed that Pleasureways had been operating under licence. All the members of the public who had given evidence on behalf of the applicants had travelled on illegal tours with Pleasureways to either Margate or Torquay, and in law were accomplices. Existing operators must be protected.

Traffic on the Cromer tour, which the applicants proposed to abandon if the Margate tour were authorized, had dropped from more than 800 passengers a year to 244.

Mr. R. C. Oswald, objecting for British Railways, said he could not ignore the

irregular operation that had taken place. Witnesses had given an impression of loyalty to the applicants rather than one of public need.

Mr. J. Booth, for the applicants, said they had been punished for their illegal activities and their application should now be treated on its merits. There was a substantial demand for the Margate tour. Present facilities were insufficient. Members of the public who had given evidence had all said they did not wish to travel with any company other than Pleasureways.

Decision was reserved. The early part of the hearing was reported last week.

## Bird's Eye View

SOCIALIST M.P.s would probably rather not be told that the drivers employed by Hart's Transport think so well of their boss, Mr. Reginald J. Hart, and his wife, that they made presentations to them at the firm's annual dinner.

Mr. and Mrs. Jack Rawlings, of Andover, are also proud of the esteem of their drivers, who entertain them annually to dinner. On the last occasion their drivers combined with those of another local haulier, and the proprietors of the two businesses were the guests of their joint employees.

Although much larger, Simms Motor Units, Ltd., also obviously have the common touch. Since 1954 their labour turnover has been reduced from 29 per cent. to 17 per cent. Happy relationships of this kind between employers and employees deserve more publicity than they receive.

## Progress

AFTER 30 years the Post Office have heard of the use of oil engines in road vehicles. More than that, they are actually trying them out. Careful tests have revealed that oilers are more economical than petrol-engined vans, but insufficient experience has been accumulated by the G.P.O. to make possible a comparison of maintenance costs.

Meanwhile, I look forward to hearing that bullock carts have been abandoned on Manchester to London postal deliveries.

## Selling Transport

MR. E. R. L. FITZPAYNE, general manager of Glasgow Transport Department, has gone to town with his latest annual report. The glazed cover carries a full-colour reproduction of a street scene in Glasgow, and the square-backed 85-page book contains some first-class half-tone pictures, as well as graphs. I wonder how much it cost to produce?

## Gentlemen on Wheels—1

I CANNOT recall having seen any of the London Brick Company's drivers in the Lorry Driver of the Year Competition, yet they are among the best in Britain. I never fail to be impressed by their consideration for other road users. They should be entered in the competition, if only as an example to others.

## Gentlemen on Wheels—2

THEY would find strong rivals in courtesy in Tate and Lyle's drivers, who do so well in the competition every year. Tate and Lyle have had a delightful letter from an elderly man at

# Happy Ships

Marlborough, who cycles every day on the Bath road in fear of death at the hands of what he describes as "monstrously bad" drivers of heavy vehicles.

"I would like to put on record, before I am killed by one of the others, that I have never seen the slightest deviation by your drivers from the highest possible standards of courtesy, skill and safety," he writes.

## Back-room Boy

WHO remembers the Sainsbury bus? It was a one-off job built at London Transport's Chiswick works and operated from Alperton on routes 18 and 83 for four years until the middle of 1954. It was novel in its day and had been designed to simplify maintenance. All parts of the body could be quickly detached through the use of bolts or patent fasteners.

The man who designed it in his spare time and then helped to build it—65-year-old Arthur William Sainsbury, foreman in the Chiswick experimental shop—retired last Friday after more than 47 years' service. As a back-room boy, he had worked on all types of London bus from the B to the Routemaster.

## All Done by Mirrors

I THOUGHT I could report anyone until I listened to a talk on the future of the motor industry by Mr. Reay Geddes, Dunlop's managing director and son of Sir Eric of the Axe. I now admit defeat.

Apart from a condemnation of purchase tax on commercial-vehicle chassis as indefensible—which isn't news, anyway—I can recall no positive statement by him. But I was charmed by Mr. Geddes' astringent and sometimes cynical wit, his mastery of innuendo and the throw-away, and his subtle, slightly apologetic delivery. When one can spend an evening with such a polished conversationalist, the future of the motor industry hardly seems to matter.

## The Last Straw

RELIGION has now become an influence in local politics affecting transport in Eastbourne. It has been seriously suggested that in the municipal elections next May, Roman Catholics in Eastbourne should vote only for candidates who are in favour of the town council's paying the whole of the bus fares of children attending a new Catholic school at Bexhill.

There are enough transport troubles in Britain without importing Continental prejudices.

# "Serious View" of Charges Taken by Commissioners

A SERIES of summonses against Silver Star Motor Services, Ltd., Porton, near Salisbury, was heard by Salisbury County Magistrates last week. The company were accused of contravening the conditions of licences granted for the carriage of Servicemen on week-end leaves from Salisbury Plain to the Midlands and the north.

Mr. G. Leopold Lush, prosecuting, said the Western Traffic Commissioners took a serious view of the matter, particularly as they had gone to some trouble to work out the licence conditions.

Silver Star pleaded not guilty to picking up passengers at Middle Wallop for journeys to Birmingham, Manchester and Newcastle upon Tyne without getting agreement from Wilts and Dorset Motor Services, Ltd. They admitted using a bus with the exits and gangways not kept

clear and carrying too many passengers on the vehicle.

Mr. Lush said that when the licence was issued certain conditions were laid down. One was that Silver Star should not pick up at Little Wallop except by arrangement with Wilts and Dorset, and another was that people should not be picked up there within two hours of a Wilts and Dorset bus calling.

Mr. S. R. Wickham, a Ministry of Transport examiner, explained the licence conditions and said the company were allowed to run a feeder service from Middle Wallop to Tidworth, connecting with the main service, but this must not be done to defeat the conditions of the licence.

After hearing evidence from soldiers, Mr. P. E. G. Mather, for Silver Star, submitted that the only case to answer

was one concerning a Serviceman who was carried to Manchester. The magistrates agreed and fined the company £15, dismissing three similar summonses.

On the overloading charge, Mr. Lush said 49 men were found in a 41-seater, and they were blocking the exit and gangway. The company were fined a total of £10 on the two summonses. Three other summonses—two in respect of unauthorized picking-up and one concerning the operation of three coaches instead of two on a journey—were adjourned because prosecution witnesses were absent.

The company were ordered to pay £3 3s. advocate's fee and £3 14s. 2d. costs. One of their drivers was fined a total of £6 10s. for aiding and abetting in the overloading case and failing to display his driver's badge conspicuously.

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(Above) This Commer 10-ton tractor, with Scammell semi-trailer, was supplied to Thos. Allen, Ltd., by Rootes, Ltd., and is on contract hire to Carlsberg Distributors, Ltd. It is seen here with a full load at Ipswich docks. The crates of bottled lager are imported from Copenhagen for distribution to depots in East Anglia, London and the south-western counties.



(Above) S. M. T. Sales and Service Co., Ltd., Edinburgh, supplied 14 Bedford 15-cwt. vans with special bodywork to Pantrypride, Ltd. Framing is of hardwood and the interior panelling of hardboard, whilst the plywood floor is above the level of the wheel arches. The rear door is hinged at the top to form a serving canopy, and racking is provided for 36 trays. There is a 2-gal. hot-water washing unit by the side of the driver. The paintwork is in turquoise blue, white and light grey.



These Thames (above) and Morris (left) vans represent two grocers' differing approaches to their distribution problems, one having chosen a rigid vehicle and the other an "artic." J. H. Jennings and Son, Ltd., built the bodywork of the Trader, supplied to Stranaghan and Nixon, Ltd., Birkenhead, by the Birkenhead Automobile Co. At the rear are two doors and a step which also serves as a bumper. There are shelves for small goods along the sides. The roof is of translucent plastics. The Morris was supplied to Charles Douglas and Co., Ltd., London, S.W.19, with two semi-trailers with Duramin light-alloy bodywork. Each semi-trailer is worked on alternate days, loading one day and delivering the next within a 20-mile radius.





**Opinions and Queries**

# Why That Stunned Silence?

**R**EFERRING to the article headed "Vehicle Stability Essential on Motorways" in your issue dated January 30, your comments in the same publication seemed to express incredulity regarding the suggestion from Mr. G. Grime, of the Road Research Laboratory, that four-wheeled light vehicles would give better stability if braked on three wheels. You also said that nobody had challenged Mr. Grime or inquired whether he had tried out this theory in practice.

I would like to draw your attention to a paper presented to the Automobile Division of the Institution of Mechanical Engineers in 1954 by Mr. H. J. H. Starks, Ph.D., and Mr. R. D. Lister, B.Sc.(Eng.), A.M.I.Mech.E., both of the Road Research Laboratory. It was entitled "Experimental Investigations on the Braking Performance of Motor Vehicles." It was stated in this that tests were carried out on a car in which (a) both rear wheels only were locked; (b) both front wheels only were locked. They were made on a level concrete surface having a uniform crossfall of about 1 in 40.

Graphs of the results showed that in the first case the angular deviation of the vehicle was about 180 degrees at just over 30 m.p.h., whereas in the second case, where the front wheels only were locked, the deviation was not more than 10 degrees at the same speed.

When all four wheels were locked, those at the front first, the car proceeded in a straight path. On the other hand, if the rear wheels were locked more than 0.5 sec. before those at the front, there was some deviation, its direction being governed by the crossfall of the surface, but if locking of one of the rear wheels was delayed, then no side movement occurred until this wheel was held.

Further tests with only three wheels locking showed that if there was no braking on one rear wheel the vehicle travelled sensibly straight when braking from speeds up to about 50 m.p.h. With no braking on one front wheel, however, the deviation was similar to that obtained when both rear wheels were locked.

Havant, Hants.

J. O. STARLING,  
M.S.A.E., G.I.Mech.E.

## Early Origin of Demountable Bodies

**I** FIND a considerable amount of interest in my weekly perusal of *The Commercial Motor*, which covers the whole field of road transport in a most thorough manner. Looking through some of my cuttings from it covering 50 years, I see no features which have been lost, but many new ones which seem worthwhile.

From my own angle, your test reports have always been very good, whilst your extension of these to include servicing items is a most commendable feature and I am sure that you have heard quite a lot of "excuses" regarding some of them.

Your correspondence columns provide plenty of opportunity for readers to express their views. In this connection Mr. Lort-Williams, who wrote in your issue dated January 30, would have to go back more than 60 years before he could claim truthfully to find the origin of the "demountable-body scheme." The old Lancashire and Yorkshire Railway, long before 1900, used detachable bodies or "flats," as they were called, in order to save waiting and handling time in works, warehouses and goods yards. At a warehouse a loaded flat could be slung onto

a horsed lorry, taken to a goods yard and transferred in a few minutes to a railway goods wagon ready for dispatch.

It was a common sight, even in a small goods yard, to see a crane carrying a large steel "X" at the end of the jib. This was held at its centre by the crane hook and from each end of the four arms hung a chain terminating in a hook. Two loops or rings on each side of the flat allowed the whole to be slung for transfer.

Before 1910 a Leyland catalogue showed one of their chassis equipped with a few cross-bearers shaped to accommodate a standardized "flat." Such a machine was owned by John Berry and Sons, Ltd., of Accrington, and the assembly could be seen leaving early and daily for a Manchester warehouse with a "flat" load of cloth. During the late afternoon the lorry would return with another flat loaded with skips of weft, probably collected in the Oldham area. This could no doubt be considered as an early type of palletizing applied to motor vehicles.

• Even before this another chassis with a special type of "body" to carry rollers for tinplate was operated by Howard and Bullough, Ltd., of Accrington. In this case no platform was employed, but use made of cross-bearers divided horizontally and with holes lined with felt to accommodate rollers of various diameter.

Blackpool.

TRANSPORT ENGINEER.

## A Reader's Ideal Chassis

**W**ITH the exception of the Tempo, which is not of British design, almost all the light commercial vehicles in production in Britain at the moment suffer from one great drawback—high floors. Specialist bodybuilders who convert delivery vans into light passenger vehicles and caravans, sometimes increase the interior headroom by fitting extra high roofs. This method, however, naturally increases the overall height, raises the centre of gravity and usually gives a somewhat ridiculous appearance.

If chassis designers would put the horse before the cart, as did Citroën, the floor height could be dropped from about 26 in. to 14 in. with no loss of clearance.

When there are so many successful designs produced on the Continent which incorporate low floors, fully independent suspension, front-wheel drive and many other features which are not now unconventional but positively good, why do our British makers not do the same?

What has happened to the spirit of non-conformity which produced, say, the Jowett vehicle with a flat-twin engine, the B.S.A. van with front-wheel drive, the early Trojan with underfloor engine which could almost be driven up a wall, the Turner milk float with a two-stroke oil engine, and the Gilford low-level bus?

Here is what I would like to see for delivery work, carrying personnel, as a caravan, or even as a private car by those who have the foresight to realize that one day they may want to carry in it a wardrobe or a sack of cement: A vehicle with an overall length of 13 ft., width 6 ft., overall height 6 ft. 3 in., wheelbase 7 ft. 6 in., track 5 ft., independent suspension all round and a floor height of 14 in. I would also favour front-wheel drive, two-cylindrical horizontally opposed air-cooled oil engine of about 2-litre capacity under the front seats, aluminium framing with glass-fibre panelling, direct gear change, and rack-and-pinion steering.

Reading, Berks.

I. THOMPSON.





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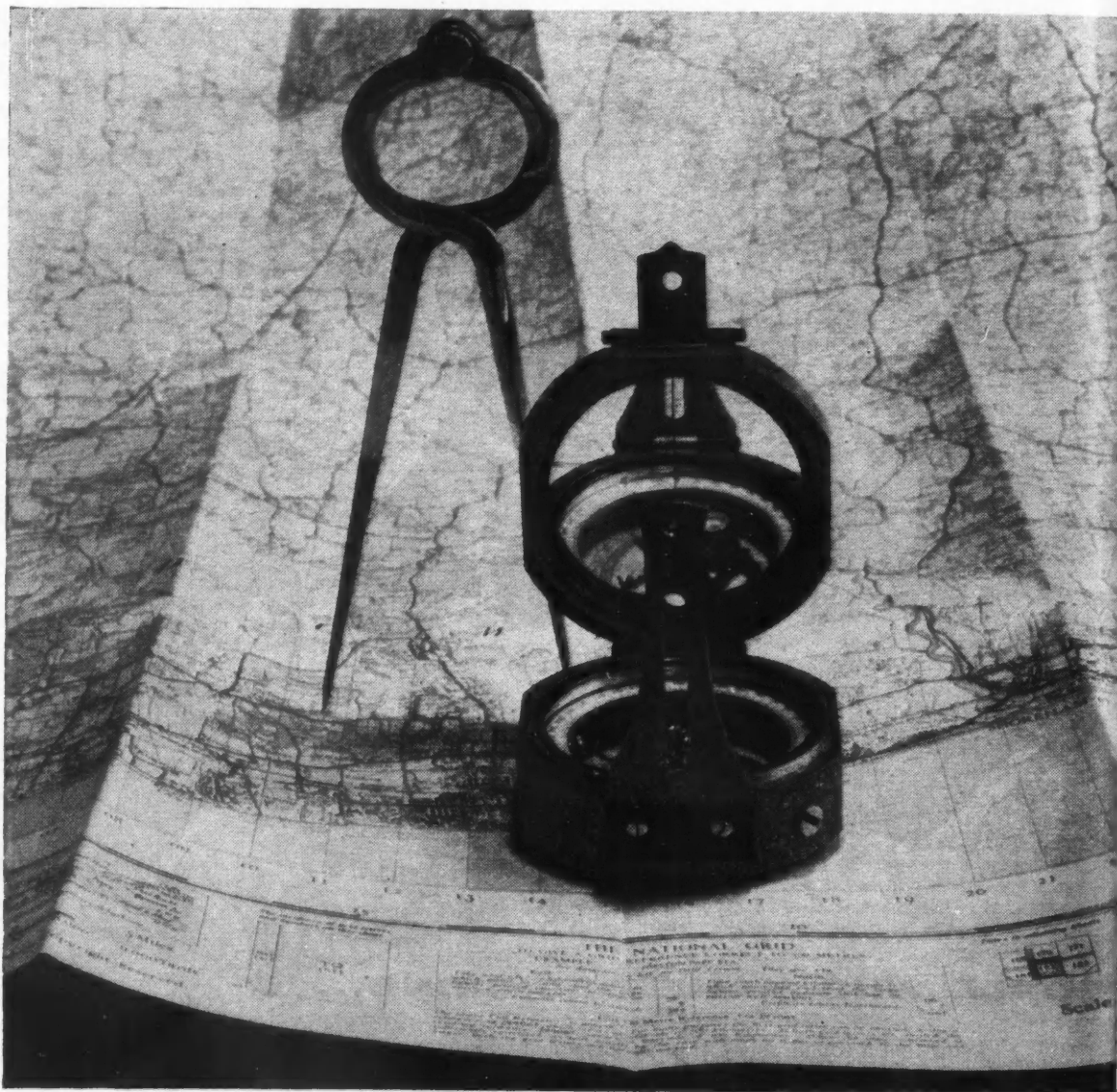
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**Political Commentary****By JANUS**

## Two Voices

**D**EVOLUTION seems to be taken seriously by the regions of British Railways. To judge from recent utterances, the men in charge at area level are not only acting independently but are even thinking differently from each other. The haulier is puzzled to know what should be his reaction when he reads one day that the London Midland Region intend to go all out to win back freight traffic that they believe rightfully belongs to rail, and the next day that in the Eastern Region there is talk of the British Transport Commission co-operating with private operators to secure traffic now carried under C licences.

There is much to make hauliers uneasy in the plans announced by Mr. David Blee, general manager of the London Midland Region, which carries a third of all British Railways freight traffic, bringing in an annual revenue of about £110m. The emphasis is on quick results. Mr. Blee has expressed the belief that it is better to hurry now than to risk further loss of traffic while long-term plans are being made down to the last detail.

Overnight express train freight services are already in operation. There are to be reductions from 170 to a maximum of 48 in the number of terminals for sundries traffic, and from 111 to 46 in the number of marshalling yards. An extension is planned of a door-to-door service that is already being achieved by direct delivery on railway motor vehicles from railheads, thus eliminating the delay caused by the sorting of wagons for onward transit to local stations.

When the plans, which are estimated to cost £25m. spread over three or four years, begin to take effect, the haulier will find more than one cause for worry and for complaint. There is a direct challenge to his long-distance activities. Next month, for example, the London Midland Region are to start a regular freight train service, five days a week, between London and Glasgow, and are offering to take a 4-ton container for £16.

### Public Support

A service of this kind, if it is run efficiently, must represent serious competition to the haulier, and a possible threat to the tenure of his licence. The railways would be able to object to its renewal in stronger terms than previously, and they would have a good deal of public support. The idea has never been allowed to remain dormant that, for historical reasons that carry great weight in a country rich in tradition, the railways have a prescriptive right to long-distance traffic. If the idea takes root in the traffic courts or before the Transport Tribunal, there would be great difficulty in eradicating it.

The medium-distance haulier may, to some extent, find the licensing system to his advantage. In most traffic areas, he has been diligent, or ought to have been diligent, in seeing that the licences granted for railway-owned vehicles did not allow them to be operated beyond a fairly short distance from the local station. For many of their vehicles the London Midland will shortly require a bigger radius and a greater tonnage. Hauliers who can do the work should object vigorously and in strength.

Even so, there are well-grounded fears that the new railway plans will work to the disadvantage of hauliers, whatever their range of operation. For the first time the

road operator is beginning to appreciate the effect of the generous treatment of the railways by the Government. The expenditure of £1,500m. on a modernization scheme is going on behind the financial protection afforded by a further £400m. that enables the Commission to show an apparently clean balance sheet even for a year like 1948, when in fact they incurred a loss of £85m. No haulier can run his business on this basis. He must make a profit or perish. Modernization of his fleet is at his own expense, and although he has the roads for his use, such as they are, he pays for them in effect many times over in taxation.

Against this background the desire of the London Midland Region for "quick results" has a sinister appearance. The aim may be not merely to arrest the decline in railway goods traffic, but to eliminate competition while the circumstances are most favourable. The public are already resigned to seeing the £1,500m. loan ultimately translated into a subsidy, and they will feel the same about the £400m., once it has been spent. The railways have an unrepeatable opportunity to cut rates without having to fear the fate that inevitably lies in wait for the haulier who consistently operates below his true costs.

### Stroke of Irony

It is ironical that the more work a haulier does—and therefore the more fuel tax he is likely to pay—the greater is his contribution to the national revenue surplus from which is financed the modernization of the competitors determined to put him out of business. A further stroke of irony is that, although the Government loan is for railway equipment and vehicles, it frees money for the bigger and better railway road vehicles that may also become a menace to the haulier.

If the haulier takes a gloomy view of developments in the London Midland Region, he may be puzzled by the somewhat different outlook of Mr. G. F. Fiennes, line traffic manager (Great Northern) in the Eastern Region. Expressing his own and not necessarily official opinion, he recently restated the Commission's four-fold task in dealing with the railways. The system must be brought up to date, and operated punctually, conveniently, comfortably and cheaply, and rail transport must be sold effectively to the user. But the aim was also "to contract the system to an economic size."

Hauliers may wonder whether Mr. Fiennes' is a lone voice. They are aware that complaints of startling and even insensate rate-cutting by the railways seem to be most frequent in the Eastern Region, where some operators are saying they must go out of business if there is no improvement. On the other hand, the possibility of co-operation has been suggested from time to time by other representatives of the Commission and of their services.

At least the opportunity should be created for joint discussion. The Socialists aim to make the Commission solvent by brute force, by suppressing hauliers and ancillary users alike. Mr. Fiennes offers a better solution. If by means of co-operation the professional carriers can provide so good a service that the trader no longer requires his own vehicles, everybody should be pleased.





### Planning for Profit

## "Little and Often" Puts Up Costs

*The Parkside Garage Ltd., Coventry, produce this dumper, based on a reinforced Morris 7-ton chassis with Martin-Harper four-wheel drive. The Turriff Construction Co., Ltd., helped in the vehicle's development. The tipping angle is 70 degrees.*

**L**AST week I discussed some of the factors which affect road transport charges. Basically, these are the 10 items of vehicle operating costs: licences, wages, rent and rates, insurance, interest, fuel, lubricants, tyres, maintenance and depreciation. The first five are standing costs and are incurred whether the vehicle operates or not. The remainder are running costs varying in total with the mileage run.

When more than one vehicle is operated there will also be establishment or overhead costs resulting from running a business as distinct from vehicles. All items which cannot be directly attributed to particular vehicles would come under this heading. After recording and totalling, it will be necessary to allocate them to individual vehicles according to the circumstances peculiar to each operator.

If vehicles are operated for hire or reward an addition to standing, running and overhead costs will have to be made if some of the contingencies inseparable from running a business are to be met.

Distribution costs, and charges are, however, affected by external factors as well as by the operator's own efficiency. Variation in supply and demand, terminal facilities and trade customs, for example, all exert an influence on overall costs and charges.

Changing trends in customers' demands or distribution services also have their effect on transport economy. Post-war financial policies have, in many instances, compelled trade and industry to reduce stocks to a minimum and then demand replacements to be delivered not only in small quantities, but at more frequent intervals and at a fixed time.

Because this trend has been gradual, its full significance may not always have been appreciated. Its continuance, however, may so alter the pattern of distribution that the traditional siting of depots or factories may be affected where other

circumstances—availability of labour, for example—do not conflict.

It is instructive to examine this trend in terms of cost as applied to the example quoted last week of a company engaged in basic food production. In 1948, 60 per cent. of their weekly output was ordered by customers for delivery any day the following week. Ten years later 70 per cent. of their customers demanded delivery of smaller quantities on a specific day, and often at a stated time. On many occasions only 24 hours' notice would be given.

It has been customary for several basic industries engaged in processing raw materials to site mills or factories at the source—the mine, quarry or dock. In this particular instance, agricultural produce was collected from both surrounding and more distant areas during a main harvest period of approximately three months. Every 4 tons of raw material was processed down to 3 tons of finished product and then sent to places between 50-200 miles away.

When other factors were also favourable, it was obviously

*(Continued on page 91)*

### *Demand for More Frequent Dispatches in Smaller Quantities Affects the Collection of Raw Material*



*Neilson's Ice Cream Co., Ltd., Uxbridge, Middx, operate this Commer SC P6(V) refrigerated van. The company have 44 Commer and 40 Trojan vehicles all with Perkins power units.*



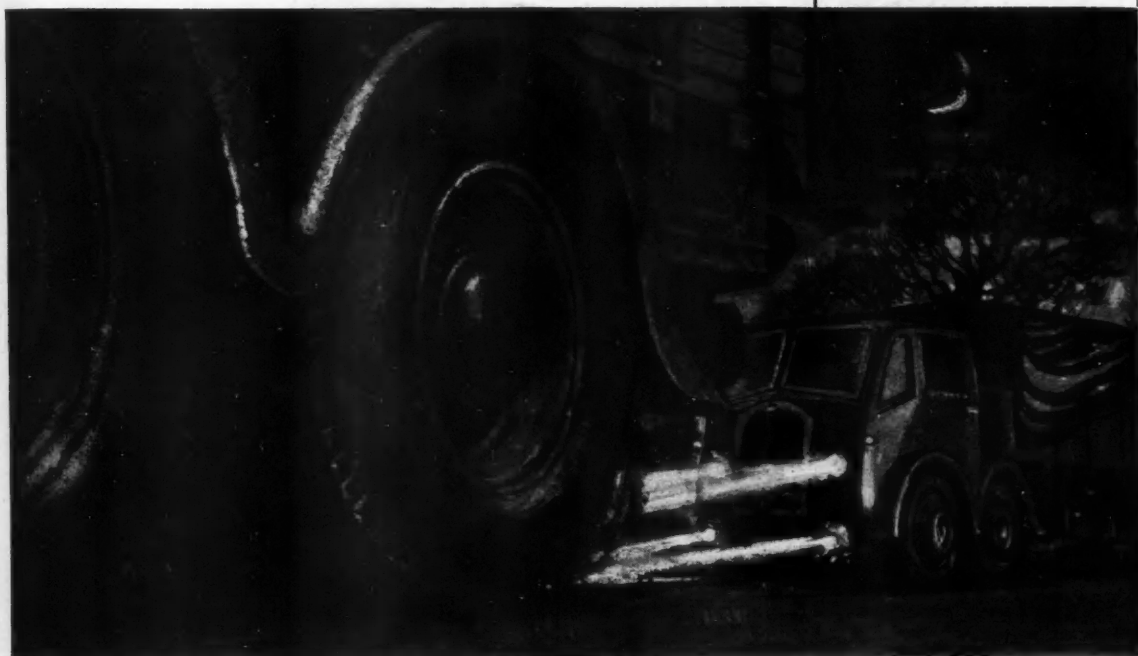
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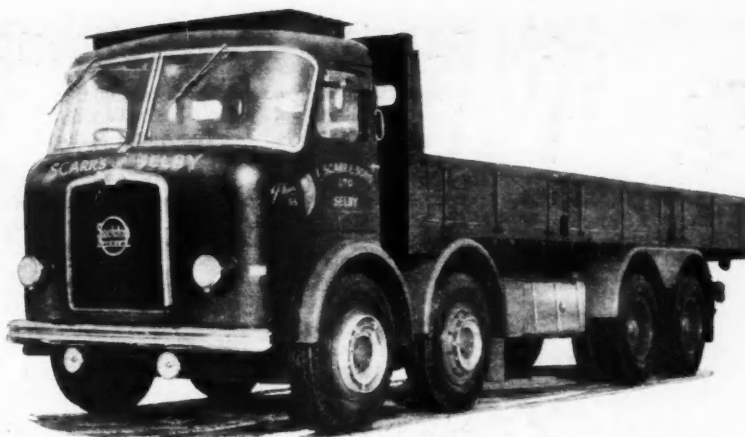
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Messrs. Scarr's, of Selby, have been supplied with this Seddon eight-wheeler, which has a Gardner 6LW engine and a David Brown five-speed main gearbox with a two-speed auxiliary epicyclic unit. Both rear axles are driven, and all wheels are braked by air pressure. Steering is power assisted. The tyres are 9.00-20 12-ply. Plastics parts were used in the cab construction. The body is 24 ft. long with sides 1 ft. 6 in. high.



economic to conduct the first processing of raw materials as near as possible to the source, thereby saving up to 25 per cent. on ultimate delivery costs. As shown, however, distribution is now being affected by changing economic trends and so reducing, if not eliminating, the original economic advantage of the present sites.

Collection of raw materials necessitates average hauls of 50 miles and loads ranging from 7 tons to 16 tons. To estimate the cost of collecting 1,000 tons and delivering it to the factory I will assume that, on balance, 500 tons is collected by 7-tonners and the balance by maximum-load eight-wheelers (oil-engined, of course).

The smaller vehicle will be estimated to weigh 3 tons 4 cwt. unladen and annual licence duty will amount to £38 15s., or 15s. 6d. per week. As with the four other standing costs, licence duty is calculated on the basis of a 50-week year to allow for two non-revenue-earning weeks. Wages are reckoned at 186s. 3d. per week, based on R.H.(64) Grade 1 rates with appropriate allowances for insurance contributions and holidays with pay.

Rent and rates are assessed at 11s. per week and vehicle insurance at 16s. per vehicle, as applicable to ancillary operation in medium-risk areas. Interest adds a further 21s. per week, making the total weekly standing costs £12 9s. 9d.

Assuming that oil fuel is purchased in bulk at 3s. 10d. per gallon, fuel cost will be reckoned at 3.07d. per mile and lubricants at 0.25d. Tyre costs amount to 1.76d. per mile, assuming an average mileage per set of 30,000. Maintenance is estimated at 2.13d. per mile and depreciation at 3.15d., where vehicle mileage life is 125,000. Total running costs are, therefore, 10.36d. per mile.

### Total Operating Cost

The addition of standing and running costs would give a total operating cost per mile of 15.36d. or 14.11d. at 600 or 800 miles per week respectively.

Turning to the maximum-load rigid eight-wheeler, licence duty will amount to 48s. per week and wages to £9 19s. 10d., again allowing for insurance contributions and two weeks' holiday with pay. Rent and rates are slightly increased to 13s. per week. Because of the increased initial cost and carrying capacity, vehicle insurance costs £2 4s. per week and interest £2 18s. 8d., making the total weekly standing cost £18 3s. 6d.

Assuming that fuel is bought at the same price, the cost will now be 5.11d. a mile as a result of the lower consumption figure of 9 m.p.g. Lubricants are reckoned at 0.28d. per mile and tyres at 5.12d. Where the weekly mileage is 600, maintenance costs will amount to 2.62d. per mile, whilst depreciation adds 3.57d., making the total running cost 16.70d. per mile. If the weekly mileage is 800, the aggregate running cost will be slightly reduced to 16.46d.

Total operating costs per mile for the eight-wheeled oiler will amount to 23.97d. at 600 miles a week and 22.33d. per mile at 800 miles a week.

Ten years ago the company were able to make all their outward deliveries on one or other of these two types of vehicle. Because of the demand for smaller quantities to be delivered at more frequent intervals it has now been necessary to include 5-ton oilers in the fleet.

The five weekly standing costs of this size of vehicle would be: licence, 14s.; wages, 179s.; rent and rates, 10s. 6d.; insurance, 13s. 7d., and interest, 18s. 3d.; total, £11 15s. 4d. Corresponding running costs per mile at 600 miles per week or above would be: Fuel, 2.56d.; lubricants, 0.24d.; tyres,

1.41d.; maintenance, 1.82d., and depreciation, 2.30d.; total, 8.33d. Total operating cost per mile is 13.04d. at 600 miles per week and 11.86d. at 800 miles per week.

When 500 of a total of 1,000 tons of raw materials were collected by the 7-tonners, approximately 71 trips were necessary. With an average lead of 50 miles, 7,100 miles would be operated. The operating cost for this class of vehicle has been shown to be 15.36d. a mile at 600 miles per week. The cost of collecting 500 tons by 7-tonners would thus be £454 8s.

Similarly, the remaining 500 tons would be collected in 33 trips (3,300 miles) by the 15-tonners at a total cost of £329 12s. The 1,000 tons would thus be cleared for £784, excluding, of course, the benefit accruing from any traffic carried on the outward journeys.

Turning now to the delivery of the finished product, the 375 tons to be hauled by the 7-tonners would involve 53 trips. As the radius of distribution ranges from 50 to 200 miles, the average return mileage will be 250 and the total mileage covered 13,250.

It would be reasonable to assume that these vehicles would average a higher weekly mileage—say, 800. At the appropriate cost per mile of 14.11d., the total cost would be £779. Similarly, delivering the balance by 15-tonners in 25 trips would involve a mileage of 6,250 at a cost of £581 10s. Total delivery costs for the 750 tons would be £1,360 10s.

Now, however, the customer demands smaller quantities more frequently. Assuming delivery of the 750 tons were equally divided between the 5-, 7- and 15-tonners, the number of trips would be 53, 36 and 17 respectively, and the corresponding cost £617 14s., £529 2s. and £395 8s. The total of £1,542 4s. shows an increase of £181 14s. compared with the cost of delivery in larger quantities, equivalent to the addition of 4s. 10d. per ton.

In practice this additional cost may be increased still further by the influence of factors inseparable from day-to-day traffic office work. With a mileage radius of 50, all collections would normally be made within the day and the following day's arrangements could then be carefully allocated to achieve maximum efficiency. Deliveries, however, necessitate an over-night stop so that on average only half the fleet would be available for loading on any one day. Some under-capacity loading must therefore inevitably arise and moreover increase as the range of vehicles widens.—S.B.

### UP-TO-DATE COSTING INFORMATION

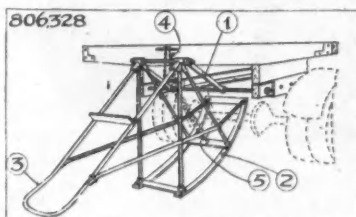
Every operator should have a copy of the new edition of "The Commercial Motor" Tables of Operating Costs." Completely revised and based upon prevailing prices, figures relating to goods and passenger vehicles of all sizes and with different power units enable providers of transport to measure the economy of their own methods, whilst users can judge, with the aid of this publication, the standard of services they may be offered.

The Tables may be purchased from booksellers at 3s. or 3s. 6d. by post from Temple Press Ltd., Bowling Green Lane, London, E.C.1.

## Apparatus for Loading Sacks

**P**ATENT No. 806,328 is concerned with the loading of sacks. It describes apparatus for loading such items by employing movement of the vehicle to provide the power for lifting. (P. Dunne, Ginnetts Park, Summerhill, Co. Meath, Eire.)

The unit illustrated is mounted on the



rear end of the chassis frame by links (1) pivoted at both ends to allow it to rock. It comprises an arcuate portion (2) which is in contact with the ground. Rigidly attached to this member is a projecting cradle (3) upon which the sacks are placed.

With a sack in position, a short forward movement of the vehicle turns the curved member and lifts the sack up and forward to deposit it on the floor.

A short chain (4) is provided to hold the loader clear of the ground when travelling. This operation is assisted by a tension spring (5) which lifts the complete unit when there is no load on the cradle.

### MOVABLE FILAMENT FOR HEADLAMP DIPPING

**A** HEADLAMP dipping device forms the subject of patent No. 806,474 which describes a bulb with a moving filament, the movement being created magnetically. (Joseph Lucas (Industries), Ltd., Great King Street, Birmingham 19.)

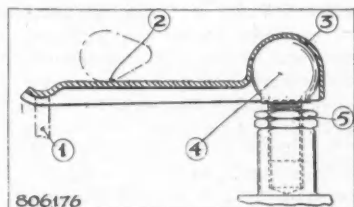
The filament is mounted on a pair of springy leads which normally hold it concentrically to produce a horizontal beam.

Moulded into the cap is a strip of iron and the filament support carries a small iron piece also. If the strip is magnetized, it will attract the small piece and so pull the filament away from the central position, causing the beam to dip.

The iron strip can be magnetized by an external winding around the lamp holder, energized from the battery through the dip-switch.

### SIMPLIFIED ROCKER GEAR

**V**ALVE rockers are usually made from machined forgings. A scheme for making them as a simple pressing which



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requires no further machining is the subject of patent No. 806,176. (N. Momtchiloff and The Austin Motor Co., Ltd., both of Longbridge Works, Birmingham.)

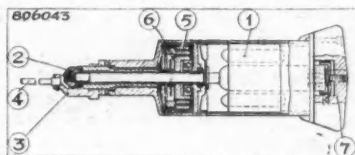
The drawing clearly illustrates the proposed form, which can be pressed easily from sheet steel. It works a valve (1) at one end, and receives the thrust of the cam at the point 2. Its other end is formed into a hemispherical socket (3) which seats upon a ball-ended fulcrum (4).

As the rocker is too thin to receive tapped holes, adjustment is performed by altering the height of the fulcrum. To allow this, the ball stud is screwed and fitted with lock-nuts (5). The wearing surfaces may be case-hardened or faced with Stellite.

### A VALVE-SEAT GRINDER

**A** MACHINE for re-grinding valve seats in situ forms the subject of patent No. 806,043. The grinding wheel, as it revolves, is also moved around the seat so that it cuts only on line contact. (I. Yule, 145 Yarningale Road, King's Heath, Birmingham, 14.)

It consists primarily of an electric motor (1) driving a spindle to which the grinding wheel (2) is attached. The wheel



is enclosed by a sleeve having a conical end (3) which is located by the valve seat. Additional location is obtained from a pilot pin (4) which enters the valve guide.

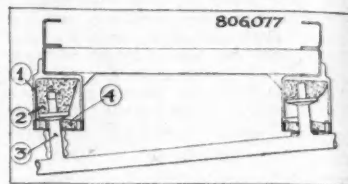
The grinding wheel projects through a cut-away portion of the sleeve to reach the seating. In operation, the wheel runs at 10,000 r.p.m. and the sleeve at about 20 r.p.m. A reduction gear for driving the sleeve comprises a 100 to 1 epicyclic gear (5) and a 5 to 1 fluid gear located in the space (6). The reduction gear forms the subject of an additional patent numbered 806,044.

Depth of cut is regulated by lowering the motor spindle by a few thousandths of an inch. This is controlled by a knurled knob (7) which pushes on the motor thrust-race.

### RUBBER SUSPENSION

**A** SUSPENSION system claimed to give satisfactory riding qualities over a wide load range is shown in patent No. 806,077. The resilient medium is stated to be "a resilient plastic of high porosity," but the term rubber will be used for brevity. (V. Poglioli, 50 Via Albertazzi, Bologna, Italy.)

Attached to the frame are rectangular metal pots (1) containing blocks of rubber (2). Into these are fitted flanged studs (3) projecting from the axle. The studs are not bonded to the rubber and in the



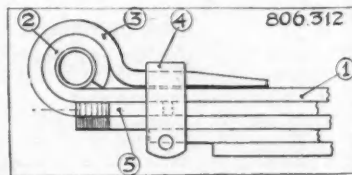
unladen condition, as shown on the left, there is an air space at the top of the bore.

Rectangular collars (4) are provided in the mouths of the pots and these offer resistance to sideways movement. The scheme is said to be quite suitable for replacing conventional leaf-spring suspension.

### SPRINGS FOR TRAILERS

**S**PRING assemblies for heavy vehicles such as trailers are covered in patent No. 806,312 which describes a method of constructing the eyes of a laminated spring with three full-length leaves at the top. (J. Brockhouse and Co., Ltd., Victoria Works, Hill Top, West Bromwich.)

The drawing shows one end of such a spring. The uppermost leaf (1) is turned over to form an eye (2) to hold a bush. Surrounding the eye is a clip (3) which is pulled downwards by the leaf-retaining clamp (4). The lower end (5) of the clip is V-shaped in plan and extends into a corresponding V-notch cut in the end of the second leaf. The notch also has a short parallel portion to locate the second leaf should it move slightly out of the V portion. The arrangement is said to reduce the tendency for the first leaf to rock on the second one under downward load.



### FORWARD-CONTROL CAB

**P**ATENT No. 807,201 comes from Vauxhall Motors, Ltd., Luton, and discloses improvements in forward-control cabs. The upper part is removable as a complete shell from the lower portion, the dividing line being at or about the waist level of the cab.

### LIGHTER STEERING

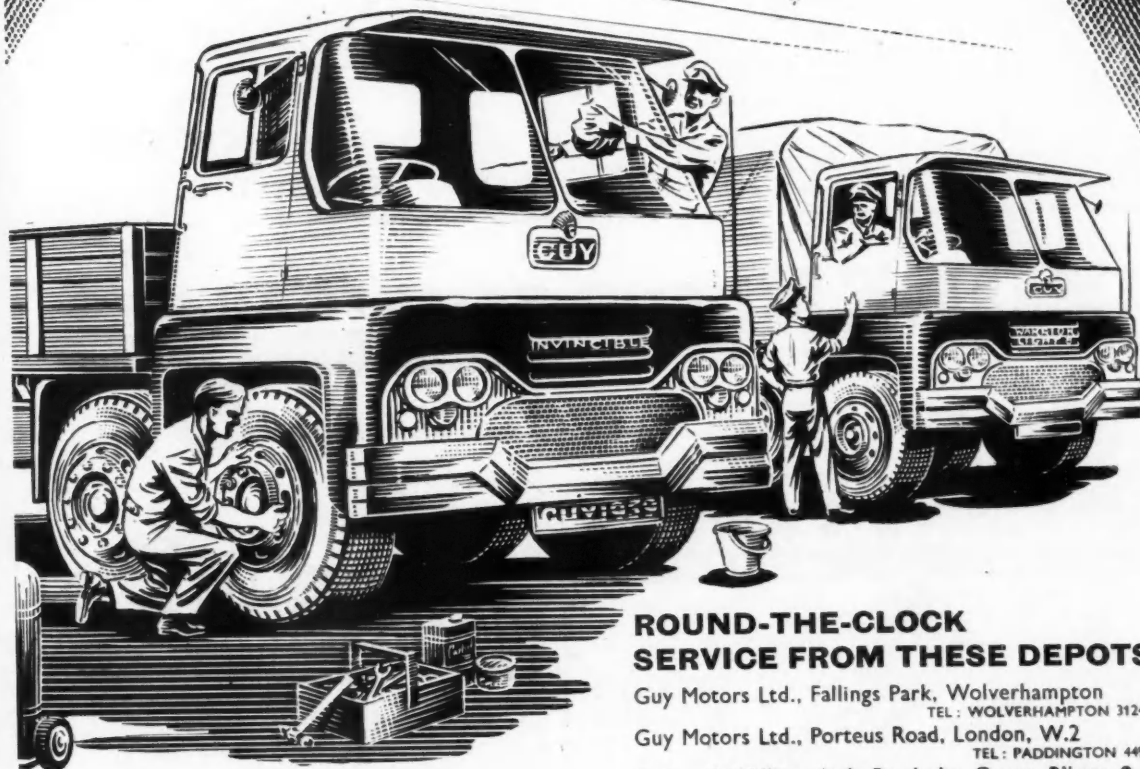
**S**UITABLE for use in steering mechanisms and similar applications, a worm-and-nut design described in patent No. 806,811 incorporates ball-bearings between the moving members. (George Kent, Ltd., 199 High Holborn, London, W.C.1.) It is said to give a smooth action without back-lash.

The thread on the nut is formed by a helical spring, the coils of which act as a bearing race. The balls tend to force the coils apart, but excessive movement is prevented by the assembly housing.



# DAY & NIGHT SERVICE

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Powdertight, watertight

All welded aluminium alloy

No bolts or rivets  
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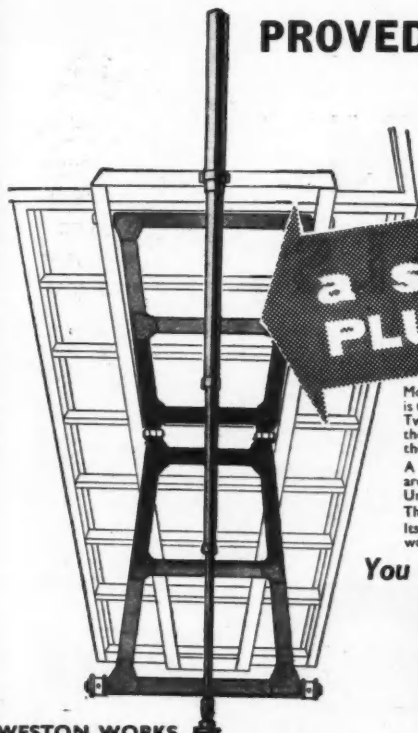
In addition to supplying up-to-the-minute details of new models, every issue of "The Motor" offers you a choice of thousands of used cars of every type and price.

**The Motor**  
Every Wednesday 1/-

if it's **DODGE**  
ENGLISH OR CANADIAN  
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**PROVED IN PRACTICE . . . the most efficient  
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Most efficient long wheel base, tipping combination, a single ram, plus stabilizer. What is the advantage of a single ram tipping gear when fitted to a long wheel base truck? Twin rams will control side movements of the body but since the feed from the pump to the ram is from a common source any inequality in weight distribution may result in the lighter loaded ram extending faster than the other.

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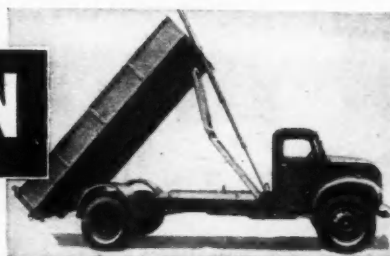
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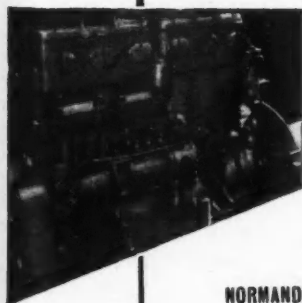
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Normand Limited are accredited Gardner, Albion, Leyland, Scammell and Perkins repairers, specialise in the reconditioning of all types of diesel engine and provide an over-the-counter exchange engine service in Gardner, Leyland and AEC engines. All engines are thoroughly bench-tested at our Park Royal works and carry our guarantee.



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RANGE**

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## AEROSTART STARTS

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FOR  
OVER  
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THE CAR MART SERVICE AND SALES DEPOT AT WELSH HARP

- IMMEDIATE DELIVERY OF MOST MODELS 5 cwt. to 10 tons
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# THE COMMERCIAL MOTOR CLASSIFIED ADVERTISEMENTS

**PRESS DAY.** Classified advertisements must normally be received at Head Office by **FIRST POST TUESDAY.** Last minute additions and deletions are accepted by telephone from trade advertisers up to noon. Advertisements received too late for a particular issue are automatically inserted in the succeeding one unless instructions to the contrary are received.

**RATES:** 10d. per word (Minimum 12 words 10/-). Each paragraph charged separately and name and address must be paid for. Semi-displayed setting 67/6 per single column inch. Centred lines 20/-. Series discounts of 5% for 13, 10% for 26 and 15% for 52 consecutive insertions allowed to trade advertisers.

**TERMS:** Strictly net and prepayable. Monthly accounts for settlement by the end of the month following insertion are allowed to trade advertisers if satisfactory references are provided.

**REMITTANCES:** Cheques and postal orders should be crossed and made payable to **TEMPLE PRESS LIMITED** and instructions sent addressed to the Manager, "The Commercial Motor," Bowling Green Lane, London, E.C.1.

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**HEAD OFFICES:** Bowling Green Lane, London, E.C.1, England. Telephone: Terminus 3636. Telegrams: "Pressimus London Telex." Telex: 23839.

**BRANCH OFFICES:**  
Bayliss House, Hurst St., Birmingham, 5. Telephone: Midland 6616.  
50, Hertford St., Coventry. Telephone: Coventry 62464.  
1, Brazennose St., Manchester. Telephone: Deansgate 6114-8.  
12, Renfield St., Glasgow. Telephone: Glasgow Central 1413.

## GOODS VEHICLES FOR SALE AND WANTED

### USED GOODS VEHICLES

**A.E.C.**  
**6X6** Unregistered ex-W.D. diesel/petrol chassis and cabs, also 2,500-gal. refuellers; c/w winches if required. Genuine delivery mileage only. M.O.S. rebuilds. Cuney and Stewart, Ltd., Alfreton, Derbyshire. Phone, Leabrooks 477. zzz-644

**A.E.C.** Matadors, 4 x 4, with diesel engines, power winches. Arnott's Auto Spares, Guthrie St., Dundee. 799-7102

**A.E.C.** 6 x 6 tankers and chassis and cab only.  
**APPLY** Swarder (Motors), Lane End, High Wycombe, Bucks. Lane End 234. 799-7141

**1937** 8-wheeler A.E.C.; also 1937 4-wheeler A.E.C., both in good running order. £325 the two. Adam Erwin and Co., 15 King St., Maidstone 5522. 799-7171

**A.E.C.** Ex-W.D. 6 by 4 7.7 engine, fitted drop-side body and complete with 15-ft. ex-W.D. drop-side trailer on 050 by 13 tyres. Both are in excellent condition. £275 the pair. Justice, Langley Mill 3182, home 3625. 799-113

**50** A.E.C. Matador diesel lorries, 4-wheel-drive, complete with air brakes, winches and wooden bodies. In little used condition, as received direct from the War Office. £575 each.

**MESSRS. D. ARNOLD,** Feltham Rd., Ashford, Middlesex. (Five minutes from London Airport.) Phone, Ashford 3349. 800-50

**1952** A.E.C. 8-wheeler, 9.6 engine, double-drive, air brakes, 24-ft. body, in very good order throughout, choice of two.

**1949** A.E.C. 4-wheeler, 20-ft. body, 7.7 engine, good order. £550.

**1949** A.E.C. 6-wheeler, 22-ft. body, 9.6 engine, in good running order. £750.

**USH GREEN MOTORS,** Langley, Hitchin, Herts. 799-244

**1936** A.E.C. diesel 32-seater coach, £150; 1948 A.E.C. diesel 8-wheeler, single drive, 24-ft. platform body, £360, engine faulty; also, body only, 24-ft. platform, T. and F. Motors, Ltd., 2A Poole Park, N.4. Archway 4582. 799-353

### A.E.C. Wanted

**WANTED** A.E.C. 4-wheelers, Monarchs or Mandators, any wheelbase, with or without licence, from 1946 on. Box CM9814, care of "The Commercial Motor."

**A.E.C.** 4- or 6-wheeler in clean condition, 949-51, flat or tipper. Dodd, Dromara, Co. Down. 799-85027

### ALBION

**LATE 1955** ALBION Chieftain artic., one 24-ft. trailer and two 17-ft. trailers, in excellent condition. £1,100, or would exchange for 4- or 6-wheeler. Roger Rees Transport, Swansea 7383. 799-125

**1950** ALBION FT3 6-ton truck, good condition. £140. East 1132. 799-370

### Albion Wanted

**1956** Onwards, ALBION Chieftains wanted, long wheelbase, with or without bodies, must be clean. Adv 5096. 799-281

### ATKINSON

**RYLAND GARAGE, LTD.,** Midland distributors, for prompt delivery all Atkinson models. New and used 8-wheelers available.

**RYLAND GARAGE, LTD.,** Ryland St., Birmingham, 1, 16, Edgbaston 4501-5, Grams, "Diesel." 799-130

**1947** ATKINSON 6-wheeler, sound condition, £625. Acorn 6911. 799-321

**1948** ATKINSON 8-wheeler, 6LW engine, double drive, nearly new cab, in excellent order throughout, any trial terms and exchanges. £750.

**USH GREEN MOTORS,** Langley, Hitchin, Herts. R Stevenage 175. 799-248

### Used Goods Vehicles (contd.)

**THE NIGHTINGALE ENG. CO., LTD.**

ATKINSON VEHICLES.

LONDON SALES AND SERVICE STATION.

**FAVOURABLE DELIVERIES**

ALL MODELS.

PART-EXCHANGES AND H.P. ARRANGED.  
NEW AND USED 8-WHEELERS ALWAYS IN STOCK.

**THE NIGHTINGALE ENG. CO., LTD.**

BALHAM, S.W.12. BAT 2193 (five lines).

zzz-0628

**PRICE'S (EARL SHILTON), LTD.**

THE MAIN ATKINSON DISTRIBUTORS.

COMPREHENSIVE spares always available.

**PRICE'S (EARL SHILTON), LTD.**

"THE ATKINSON PEOPLE."

NEW STREET,

EARL SHILTON,

NEAR LEICESTER.

Phone 3321-2-3.

zzz-917

**ATKINSON.** 1953 4-wheeler, 4LW, mileage 40,000, in excellent order, well tired, £800; choice of two. 4 Carruthers St., Liverpool, 3. Central 2047. 799-x5041

### AUSTIN

**4 X 2, 4 X 4, 6 X 4,** unregistered ex-W.D. trucks, good selection. Cuney and Stewart, Ltd., Alfreton, Derbyshire. Phone, Leabrooks 477. zzz-829

**MARSTON MOTOR CO., LTD.**

SEVEN SISTERS ROAD,

TOTTENHAM, N.15.

PHONE, STAMFORD HILL 8000.

**SERVICE AND SPARES.**

SPECIALISTS IN ALL TYPES OF BODYWORK.

zzz-699

**1956** AUSTIN A40 van, first-class condition throughout with heater, £330. Scottorn, Ltd., Kingston Rd., New Malden. Malden 3633. zzz-906

### CLASSIFIED ADVERTISEMENTS

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### Used Goods Vehicles (contd.)

**CAR MART, LTD.**

SIX MONTHS' GUARANTEE

WHERE STATED.

**10% DEPOSIT.**

**1948** AUSTIN 2-3-ton boxvan, £125.

**1957** AUSTIN A152 Omnivan, 6,000 miles. £475.

**1957** AUSTIN B.M.C. diesel prime mover (Scammell Brookhouse coupling), guaranteed, £975.

**1950** AUSTIN 3-ton Luton van, approx. 550 cu. ft., £245.

**1951** AUSTIN A70 gown van, £195.

**1955** AUSTIN 3-ton B.M.C. diesel boxvan, £595.

**1958** AUSTIN 13-seater Omnicoach, 6,000 miles, guaranteed, £575.

**1958** AUSTIN A35 van, guaranteed, £375.

**1949** AUSTIN 2-3-ton drop-side truck, £95.

**THE CAR MART, LTD.**

WELSH HARP,

EDGWARE ROAD, N.W.9.

Hendon 6590.

799-51

### SAUNDERS FOR YOUR AUSTIN.

**AUSTIN 152** 15-cwt. chassis, fitted large capacity Luton body, ready for colour.

**AUSTIN LD2** chassis, petrol, fitted Luton body, 550 c.c., ready for painting, lettering.

**AUSTIN LD2** chassis, petrol, fitted Luton body, 650 c.c., nearing completion.

**H.A. SAUNDERS, LTD.**

836-842 HIGH ROAD,

FINCHLEY, LONDON, N.12.

Hillside 5272, ex. 25.

799-160

**1957** AUSTIN prime mover, diesel engine Brockhouse-Scammell interchangeable coupling, low mileage, one careful owner, excellent tyre equipment all round, 40-gal. fuel tank.

**J. J. WRIGHT AND SONS, LTD.,** Dereham, Norfolk. 799-8

**1955** 5-ton petrol long-wheelbase boxvan.

**1952** 2-ton petrol long-wheelbase boxvan.

**ARLINGTON MOTOR CO.,** High Rd., Ponders End, Enfield, Middx. Phone, Howard 1266. 799-269

**1957** AUSTIN A55 pick-up, green, good condition throughout, £430.

**1956** AUSTIN A40 van, green, low mileage, good tyres, £340.

**1955** AUSTIN A40 van, green, good condition throughout, £325.

**1955** AUSTIN 1-ton petrol van, grey, one owner, good condition, £360.

**1948** AUSTIN 25-cwt. 3-way van, blue, choice of three, £120.

**1946** AUSTIN 2-3-ton drop-side truck, tyres and general condition very good, £150.

**MARSHALLS (CAMBRIDGE), LTD.,** Airport Garage, Newmarket Rd. Cambridge 56291. 799-401

**WRAY PARK GARAGES** offer three 5-ton long-wheelbase drop-sided trucks, one N.C., two F.C., ready immediate use, £550 each.

**1955** June, 2-3-ton Series 3 truck, mint condition, ready for use, £425.

**1950** AUSTIN 2-3-ton N.C. D.S.T., good tyres, clean condition, ready immediate use, £175. Phone, Reigate 2263. 799-109

### Used Goods Vehicles (contd.)

**1954** 5-ton AUSTIN articulator, in good condition, £435.  
**COOMBS COMMERCIAL (GUILDFORD), LTD.**,  
Portsmouth Rd., Guildford, Surrey. Phone, Guildford  
62907.

**AUSTIN** 152 15-cwt. van, September, 1957, 7,000 miles  
only, £400. Pearl Street Garage, 17 Pearl St. Sheffield  
29139.

**AUSTIN B.M.C.** diesel van, late 1955, 850 cu. ft., as  
new, 2600. Walkers Filling Station, Ecclesfield, nr.  
Sheffield. Phone, Ecclesfield 3667.

**AUSTIN** 25-cwt., very good condition throughout, £110.  
Cavendish Motors, Cavendish Rd., N.W.6. Willesden  
0046-8.

**1956 AUSTIN** 30-cwt. diesel van, blue, one owner,  
unwritten, exceptional condition, £525. Frank  
Watson (Croydon), Ltd., Thornton Rd., Thornton Heath  
4321.

#### Austin Wanted

**W.E.M. MOTORS (WIMBLEDON).** Cherrywood  
4322, 4568. Will always pay a little more  
for your Austin, whether a truck, a tipper, a tractor, or  
a Luton van. W.E.M. Motors (Wimbledon). Cherrywood  
4322, 4568.

**AUSTINS** wanted, trucks, tippers, vans, etc. G.T.C.  
(A) Commercial, Ltd., 65 Rennets Wood Rd., Eltham,  
S.E.9. Eltham 8253.

### BEDFORD

**LAWSON PIGOTT MOTORS, LTD.**

OFFICIAL BEDFORD MAIN DEALERS.

**BEDFORDS ARE OUR BUSINESS.**

**NEW BEDFORD LUTON VANS!!**

FURNITURE REMOVERS' IDEAL VEHICLE!!

**BRAND-NEW** and unregistered Luton van on  
BEDFORD 4-ton chassis, 4-cylinder diesel powered,  
1,000-cu.-ft. body with excellent low loading, first-class  
body, immediate delivery.

**FULL** specification on application.

**NEW BEDFORD** chassis, excellent range ex stock for  
early delivery.

**WE** have a wide range of good used BEDFORDS  
from 10-cwt. vans to 7-ton tippers, all good, sound  
value.

**PHONE, BARNET 1061.**

**YOU** can be sure of a good deal.

**PART-EXCHANGE** your old vehicles and get up to  
date with a BEDFORD from our new or used stock.  
Why not take advantage of today's attractive hire-  
purchase facilities?

**SALES** staff in attendance until 5 p.m. Saturdays.

**LAWSON PIGOTT MOTORS, LTD.**

186 EAST BARNET ROAD,

NEW BARNET.

Phone, Barnet 1061, 1066.

320 KING STREET, HAMMERSMITH,

LONDON, W.6.

Phone, Riverside 4111.

799-348

**E. J. BAKER AND CO. (DORKING), LTD.**

**BEDFORD**

**MAIN DEALERS.**

**INQUIRIES** will be welcomed for used commercial  
vehicles of all types from our large and comprehensive  
stock, including the following:—

**BEDFORD** 1951 5-ton metal-body tipper, in good  
condition, good tyres.

**BEDFORD** Grosvenor Grafton, one owner, low mileage,  
two-colour finish, in all a most excellent vehicle.  
See also articulated vehicles and trailers.

**HIRE-PURCHASE**, part-exchanges welcomed. See also  
under New Goods Vehicles—Bedford.

**273 LONDON ROAD,**

STAINES.

Staines 4211 (five lines).

**1955** BEDFORD CA van, excellent condition, small  
mileage, £275.

**1954** BEDFORD 7-ton long-wheelbase twin-ram  
tipper, double-drop-side wooden body, Mark II  
R6 engine, repainted, excellent condition, £785.

**1949** BEDFORD 30-cwt. Spurling van, good con-  
dition, £125.

**1948** BEDFORD-SCAMMELL tractor unit, P6  
engine, very good condition, £195.

**WE** welcome inquiries for early delivery all types of  
new BEDFORD commercial vehicles, articulated  
vehicles and trailers.

**55-61 LONDON STREET,**

CHELTSEY.

Chertsey 2391.

799-463

**COACHES** and COMPONENTS. BEDFORD Main  
Dealers.

**1959** BEDFORD 5-ton diesel chassis-cab with 1,650-  
c.c. Luton body, brand new.

**1958** BEDFORD 6-ton diesel chassis-cab,  
unregistered.

**1957** BEDFORD 7-ton diesel, 1,650-c.c. Luton,  
immaculate.

**1957** December, BEDFORD 10-cwt. van.

**1956** BEDFORD 4-ton petrol, fixed-side truck,  
immaculate.

**THE** new BEDFORD van and Workabus, etc., from  
stock.

**92-94 STAMFORD HILL, N.16.** Stamford Hill 8444.

799-399

**R38**

### Used Goods Vehicles (contd.)

**ARLINGTON MOTOR CO., LTD.**

OUR REPUTATION IS YOUR GUARANTEE.

SELECTION OF OUR RANGE OF

**USED BEDFORDS.**

COMPREHENSIVE RANGE OF USED VANS AND

PERSONNEL CARRIERS ALWAYS AVAILABLE.

**1958** 300-cu.-in. diesel 7-ton long-wheelbase forward-

control, 2-speed axle, 17-ft. 6-in. platform body,

55-gal. fuel tank.

**1956** 3-ton petrol long-wheelbase truck.

**1955** 7-ton long-wheelbase petrol drop-side truck.

**1954** Petrol 7-ton with 16-ft. platform body.

**1954** 7-ton long-wheelbase 16-ft. platform body with

Leyland Comet reconditioned engine.

**1955** 7-ton long-wheelbase with Comet engine, 16-ft.

platform body.

**1953** Petrol 3-ton long-wheelbase boxvan.

**1953** Petrol 7-ton short-wheelbase tipper.

**MAY WE**

**QUOTE YOU**

**FOR YOUR**

**NEW BEDFORD?**

COMPREHENSIVE RANGE OF VEHICLES IN STOCK.

HIGH ROAD, PONDERS END, ENFIELD, MIDDXX.

Phone, Howard 1266.

ALSO AT

25-27 VAUXHALL BRIDGE ROAD, S.W.1.

Victoria 6033.

CORNARD ROAD, SUDBURY, SUFFOLK.

Phone 2301.

DUMBALLS ROAD, CARDIFF.

Phone 30641.

SALES DEPARTMENT OPEN UNTIL 5 P.M.

SATURDAY.

799-268

**HAMILTON MOTORS (LONDON), LTD.**

THE MAIN BEDFORD DEALERS.

**WE** always have a comprehensive stock of the first-  
class "Quality Tested" and other BEDFORDS for  
your inspection.

**MAKE** sure you inspect these before you make your  
purchase. A sample of our stock is as follows:—

**NEW BEDFORD** 6-ton short-wheelbase normal-control

chassis-cab, diesel.

**NEW BEDFORD** forward-control truck, Leyland Comet

engine.

**NEW BEDFORD** 7-ton long-wheelbase chassis-cab,

petrol.

**1950** BEDFORD 3-ton boxvan, £225.

**1950** BEDFORD 5-ton tipper, £225.

**1950** BEDFORD 3-ton van, diesel, colour, grey-blue,

£475.

**1954** BEDFORD 2-3-ton platform, £80.

**1946** BEDFORD 7-ton tipper, £745.

**1956** BEDFORD 10-12-cwt. van, grey, £395.

**1957** BEDFORD 5-ton truck, £775.

**1956** BEDFORD 5-ton long-wheelbase truck, £645.

**1954** BEDFORD Dormobile, 2-tone, £375.

**1949** BEDFORD 8-ton tractor unit, £155.

**1956** Grosvenor Grafton, maroon, ivory, £425.

**1956** BEDFORD Utilabike, colour red, £425.

**SEE** the new BEDFORD at our showrooms.

**HIRE-PURCHASE** facilities, insurance and taxation can

be arranged while you wait.

**PART-EXCHANGES** welcomed.

**DEMONSTRATIONS** gladly carried out under no

obligation.

**OUR** showrooms are open until 6 p.m. daily and 5 p.m.

Saturdays.

**HAMILTON MOTORS (LONDON), LTD.**

466-490 EDGWARE ROAD,

LONDON, W.2.

AMBASSADOR 7211-5. PADDINGTON 0022-8.

799-314

**1954** BEDFORD 4-ton P6 diesel drop-sided truck,

C-licence operated only, really excellent

condition throughout, ready for immediate use, £425.

**OAKTHORPE MOTOR CO.**, North Circular Rd.,

Palmer Green, N.13. Palmer Green 0446 and 1023.

799-382

**CHARLES WENSLEY AND SONS, LTD.**, 68 Ings Rd.,

Wakefield, BEDFORD main dealers. Early delivery

of all models. Specialists in all types of coachbuilding,

including refrigerated and insulated bodywork. Hire-

purchase available and part-exchanges. Phone 2294-5,

3371.

**4 X 2.** 4 x 4, unregistered ex-W.D., low mileage,

M.O.S., rebuilt Q.L.s, c/w winches if required,

also very excellent selection of G.V. trucks with V.G. cabs,

Cuney and Stewart, Ltd., Alfreton, Derbyshire. Phone,

Leabrooks 477.

**1952** Big BEDFORD 7-tonner, petrol, £175. Erwin,

15 King St., Maidstone 55922.

799-7172

**1955** Short-wheelbase steel-bodied tipper for quick

sale. Hamblins Garage, Rectory Rd., Rushden

3211.

**5-TON** tippers, petrol or diesel.

**JEFFREYS COMMERCIAL MOTORS,** Neath Rd.,

Swansea. Phone 7288.

799-142

### Used Goods Vehicles (contd.)

**CARMO OF LONDON.**

THE LONDON CENTRE

**FOR NEW AND USED BEDFORDS.**

A LARGE SELECTION OF REASONABLY PRICED

USED BEDFORDS, INCLUDING:—

**1956** And 1957 BEDFORD 10-12-cwt. vans, large

selection in good condition at reasonable

prices.

**1949** BEDFORD 2-ton 800-cu.-ft. Luton van, good

condition, £285.

**1949** BEDFORD 2-ton long-wheelbase boxvan, £150.

**1953** BEDFORD 30-cwt. van, bargain, £345.

**1955** BEDFORD 25-cwt. van, £545.

**1951** (First registered) BEDFORD 5-ton long-wheel-

base platform, £95.

**1952** BEDFORD 5-ton diesel Luton van, £850.

**NEW BEDFORDS**

FOR IMMEDIATE DELIVERY.

**VANS, CONVERSIONS**

**AND**

**LIGHT COMMERCIALS**

UP TO 30 CWT.

LATEST LOW-DEPOSIT CREDIT TERMS.

BEDFORD EXCHANGE RECONDITIONED ENGINES

FROM STOCK.

DEMONSTRATIONS ARRANGED ON ANY VEHICLE

OR OUR REPRESENTATIVE WILL CALL.

WRITE, PHONE OR VISIT

**THE BEDFORD CENTRE,**

LEIGHTON ROAD,

LONDON, N.W.5.

Gulliver 5555.

799-474

**SHUKER'S OF SHEFFIELD, LTD.**

OFFICIAL BEDFORD dealers since 1932. Recom-  
mended for new or good used Bedford, all models;  
sent anywhere in Britain.

**TIPPERS** a speciality, petrol or diesel. Why not send  
your inquiries? A straight deal and real value  
guaranteed.

**56-60 BROAD ST.**, Sheffield, 2. Phone 20311 (four

lines), or 23280 after 6.30 p.m.

zzz-775

**1956** CA-type open dairy truck, Osborne conversion,  
suitable for dairymen or greengrocer, nice clean  
motor, very good condition.

**1954** 7-ton U-shaped-body tipper, fitted with R6

engine, reconditioned gearbox and differential

assembly, mechanically sound, body in fair condition.

**MAYFAIR GARAGE (TAMWORTH), LTD.**

COLESHILL ROAD, FAZELEY,

TAMWORTH, STAFFS.

Phone, Tamworth 1396-7.

799-40

**TOM BYATT, LTD.**

FOR

**NEW OR SELECTED USED**

**BEDFORDS.**

SEVERAL USED BEDFORD TIPPERS AND

DROP-SIDES REQUIRED IN PART-EXCHANGE

PHONE, CALL, WRITE.

**TOM BYATT, LTD.**

FENTON.

STOKE-ON-TRENT.

TERMS. EXCHANGES.

Phone, Stoke-on-Trent 48581 (six lines).

799-171

**BEDFORD.** 1956, petrol, 22-ft. platform, 6 tonner, 5-  
type coach chassis, unladen weight 3 tons. With  
goodwill and work. Howard 2211.

**1955** 7-ton BEDFORD R6 diesel, 17-ft. platform,

900 by 20 tyres, £475.

**W**EST TOWN DIESELS, Broad St. Garage, Dewsbury.

Phone 3505.

**1954** S-type BEDFORD 7-cubic-yard Weston tipper

(petrol), on good tyres, body rebuilt to new

standard, exchange gearbox, good vehicle, ready for ser-

vice, £345. W.E.M. Motors (Wimbledon), 506-508 Kingston

Rd., London, S.W.20. Phone, Cherrywood 4322, 4568.

799-139

## Used Goods Vehicles (contd.)

### COUNTY OAK SERVICE STATION, LTD.

VAUXHALL-BEDFORD MAIN DEALERS,  
LONDON ROAD, CRAWLEY, SUSSEX.  
Phone, Crawley 25475-6-7.

- B**EDFORD 1958 CA van, low mileage. £460.  
**B**EDFORD 1958 Workabus, 5,000 miles. £475.  
**B**EDFORD 1953 CA van, £195.  
**B**EDFORD 1947 long-wheelbase truck, £125.  
**B**EDFORD 1946 2-ton truck, very clean. £100.  
**A**USTIN 1947 2-ton meat van, £175.  
**A**USTIN 1952 ET6 5-ton meat van, £200.  
**A**USTIN 1949 3-way van, £95.  
**B**EDFORD 1947 5-ton pantechnicon, £275.

- 1958** BEDFORD 5-ton platform truck, petrol, £25 by 20 tyres C licence owner, retired, 12,000 miles. £775.  
**1954** BEDFORD 5-ton long-wheelbase diesel tipper, 8-yd. metal drop-side body, Anthony hoist, £25 by 20 tyres, C licence owner, retired, £495.  
**PART-EXCHANGES**, hire-purchase terms, on request.  
**GLOSTER GARAGE, LTD.**, 117 Whitehorse Rd., West Croydon. Phone, Thornton Heath 5224, 7500.  
**FOUR** 1952 5-type BEDFORDS, 10-cu.-yd. steel bodies, contract ended. Greenwich 0474.  
**THREE** short-wheelbase steel-body drop-side tippers, 1947-49, £125; 1949 long-wheelbase Anthony hoist, £135. Edgware 2572.

### E. J. BAKER AND CO. (DORKING), LTD.

BIRCHETT ROAD, ALDERSHOT.  
Phone, Aldershot 777.

- B**EDFORD, 1954, R6 7-ton long-wheelbase, twin ram, wood body tipper, tyres as new. £785.

### CITY MOTORS, Botley Rd., Oxford 48021.

- NEW** BEDFORD 4-ton long-wheelbase diesel lorry, £1,182 6s. 7d.  
**NEW** BEDFORD 3-ton long-wheelbase diesel, Techoast d.a. tipper on Michelin X tyres, £1,287 10s. 4d.  
**NEW** BEDFORD 7-ton normal-control Techoast 7-cu.-yd. d.a. tipper, 900 by 20 tyres, £1,816 2s. 7d.  
**1955** BEDFORD 7-ton long-wheelbase, petrol, platform, £485.  
**1955** BEDFORD 7-ton U-bodied petrol tipper, £560.  
**1955** BEDFORD 7-ton short-wheelbase d.s. tipper, completely rebuilt, R6 peripat engine, new chassis frame with flitch plates, £875.

### CITY MOTORS, Botley Rd., Oxford. Phone 48021.

- W**RAY PARK GARAGES offer 1956 BEDFORD 7-ton short-wheelbase diesel, all-steel U body tipper, new tyres throughout, £675.  
**1957** BEDFORD 10-cwt. van, one owner, 9,000 miles, well maintained, £425. Phone, Reigate 2263.

### CAPITAL MOTOR CO., LTD.

BEDFORD MAIN DEALERS.  
SCAMMELL DISTRIBUTORS.

**NEW** BEDFORD vans and conversions, early delivery.

- NEW** BEDFORD 7-ton forward-control chassis-cab, 300 diesel engine.  
**NEW** BEDFORD 3-ton normal-control chassis-cab, diesel engine.  
**1952** BEDFORD 5-ton long platform, excellent condition, £240.  
**1957** BEDFORD Utilibike, blue-grey, excellent condition, low mileage, £475.  
**BEDFORD** C.A. vans, 1954, choice of several, from £175.  
**1953** BEDFORD 5-ton truck, in excellent condition, £230.  
**1957** BEDFORD C.A. van, in primer, good mechanical condition, £380.  
**1958** BEDFORD C.A. van, ford blue, low mileage, £415.  
**REMINGTON ST.**, City Rd., N.1. (Near Angel). Clerkenwell 7456.

- 1952** 7-ton BEDFORD long-wheelbase truck with rack, good condition, tyres nearly new, £350.  
**COOMBS COMMERCIAL (GUILDFORD), LTD.**, Portsmouth Rd., Guildford, Surrey. Phone, Guildford 62907.

**COOMBS**, main Ford dealers.

- 1950** BEDFORD 30-cwt. normal-control van in black, K model, a very reliable vehicle, good tyre equipment, wonderfully maintained, £150.  
**COOMBS SERVICE STATION, LTD.**, By-pass Rd., Guildford. Phone 62962-3-4.

**1952**, October, BEDFORD Luton van, extended chassis, approximately 1,400 cu. ft., one C licence owner only, £285.

**C**OPPEL'S MOTORS, 92 Broughton Lane, Salford. Phone, Blackfriars 7764.

- 1948** Long-wheelbase BEDFORD tipper, new steel body, very good condition all round, £150 o.n.o. Miller Motors, 11 Prescott Place, Clapham, S.W.4. Macaulay 2264.

- 1948** Long-wheelbase BEDFORD tipper, just had reconditioned engine fitted, ready for work, £150. Miller Motors, 11 Prescott Place, Clapham, S.W.4. Macaulay 2264.

- 1952** BEDFORD long-wheelbase drop-sided truck, immaculate condition, £195. Miller Motors, 11 Prescott Place, Clapham, S.W.4. Macaulay 2264.

- 1953** Long-wheelbase BEDFORD tipper, perfect condition, Hypoid axle, synchro gearbox, £275. Miller Motors, 11 Prescott Place, Clapham, S.W.4. Macaulay 2264.

## Used Goods Vehicles (contd.)

**1955** BEDFORD long-wheelbase A-model diesel tipper, perfect condition, £495 o.n.o. Miller Motors, 11 Prescott Place, Clapham, S.W.4. Macaulay 2264.

### Bedford Wanted

**A** BEDFORD for sale? Phone, Hamilton Motors, 466-490 Edgware Rd., London, W.2. Phone, Paddington 0022 (12 lines). Immediate settlement and best prices. zzz-915

**B**EDFORD. All types wanted.

**B**EDFORD. All types wanted.

**C**HANDLERS MOTORS, LTD., 71 Greenwich South St., London, S.E.10.

### GRE 2033-4.

zzz-821

**W.E.M. MOTORS (WIMBLEDON).** Cherrywood 4322, 4568, will be the best buyers of your Bedford in singles or fleets. W.E.M. Motors (Wimbledon), Cherrywood 4322, 4568.

**B**EDFORDS wanted! Bedford wanted! We want Bedford's! Trucks, tipper, vans, Lutons etc. G.T.C. (Commercial), Ltd., 65 Renneis Wood Rd., Eltham, S.E.9. Eltham 8253.

### B.M.C.

**1957** B.M.C. articulated unit with Scammell coupling and 10-ton trailer, choice of two.

**1956** B.M.C. diesel 7-ton long-wheelbase drop-sided truck, 18-ft. drop-side body, choice of two, terms and exchanges, £650.  
**RUSH GREEN MOTORS**, Langley, Hitchin, Herts. Stevenage 175.

### CHEVROLET

**CHEVROLET** 4 x 4 long-wheelbase, fitted Perkins P6 diesel, £165. L. Thompson, 57 Lower Forbury Rd., Portsmouth 24117.

### COMMER

**COMMER** 25-cwt. van, 1939, complete running condition, reasonable offer. Romford Auto Station, Ltd., Main Rd., Romford, Essex. 43323.

**UNREGISTERED** 7-ton diesel drop-sider, as new, with full guarantee, £175 below list at £1,775.

**HARTWELL MOTORS**, Bournemouth, main dealers and parts stockists for Commer and Karrier. Phone, Winton 1779-9.

**COMMER** OX 5-6-ton 1950 underfloor-engineered 17-ft. platform truck, used only by chemists from new, never carried over 3 tons, bargain, £195. W.E.M. Motors (Wimbledon), 506-508 Kingston Rd., London, S.W.20. Phone, Cherrywood 4322, 4568.

**1946** COMMER van, Perkins P4 engine, £195. Acorn 6911.

### E. J. BAKER AND CO. (DORKING), LTD.

BIRCHETT ROAD, ALDERSHOT.  
Phone, Aldershot 777.

**COMMER**, 1955, 25-cwt. hand tipper, wood drop-side body, 9 ft. 6 in. long, good tyres, first-class condition throughout, £350.

**COMMER**, 1949, 7-ton forward control, long-wheelbase, drop-side truck, well tyre, front axle damaged, £120.

799-333

**1956** COMMER TS3 diesel 7-ton long-wheelbase truck, 18-ft. drop-sided body, choice of two, £K50 each.

**1956** COMMER TS3 diesel 7-ton tipper, excellent order, £K50.

**1954** COMMER artic. unit, diesel, 2-speed axle, Scammell coupling, good running order, £325.

**RUSH GREEN MOTORS**, Langley, Hitchin, Herts. Stevenage 175.

**1952** 5-ton COMMER articulator, in good condition, £295.

**COOMBS COMMERCIAL (GUILDFORD), LTD.**, Portsmouth Rd., Guildford, Surrey. Phone, Guildford 62907.

### Commer Wanted

**COMMERS** for sale???

**W.E.M. (WIMBLEDON)** can pay a little more for your COMMER Lutons, tipper and trucks, 1950 to 1958. W.E.M. Motors (Wimbledon), 506-508 Kingston Rd., London, S.W.20. Phone, Cherrywood 4322, 4568, 1480.

**COMMERS** wanted, trucks, tipper, vans, etc. G.T.C. (Commercial), Ltd., 65 Renneis Wood Rd., Eltham, S.E.9. Eltham 8253.

**DENNIS**

**DENNIS** Max 8-ton diesel trucks, unregistered ex-M.O.S., excellent condition, spare wheel, batteries, etc., £420 each.

**L. VASS, LTD.**, Amphilil, Bedford. Amphilil 3255.

**MAX**, fitted with DENNIS engine, overdrive gearbox, 20-ft. platform bodies, 9'00" by 20' tyres, choice of six, all in very good condition, ready to go to work.

### MAYFAIR GARAGE (TAMWORTH), LTD.

COLESHILL ROAD, FAZELEY, TAMWORTH, STAFFS.  
Phone, Tamworth 1396-7.

**1946** DENNIS Max, diesel, 18-ft. drop-side, perfect runner, late cab, bargain, £150. Weightman, Latham St., Poplar, London, E.14.

### DODGE

**1956** DODGE 7-ton tipper, R6 engine, Eaton 2-speed axle, good condition, £750.

**1957** DODGE 7-ton long-wheelbase tipper, R6 engine, excellent condition, £1,250.

**Cox's MOTORS (HILL TOP), LTD.**, 127 Hill Top, West Bromwich, Phone, Wednesbury 0470, 1047.

799-91

## Used Goods Vehicles (contd.)

**DODGE**, 1951, 6-yd. 6-ton tipper, £225. J. G. West, Thame, Oxon. 800-7179

**1956** DODGE 106/AP6 6-ton long-wheelbase drop-side truck, 2-speed axle, shock absorbers.

**GEO. H. KENDRICK, LTD.**, Carriers Green, West Bromwich 0778.

**1952** DODGE Kew 103 AP6 tractor and 22-ft. double drop-side trailer, quick-release fifth wheel and landing wheels, 825 by 20 tyres, clean vehicle, H.P. available, £225. 9 Newthorpe Common, Eastwood, Notts. Phone, Lanley Mill 2623. Evenings 3000.

**1954** DODGE 7-tonner, Perkins P6, 17-ft. alloy platform in really good condition, £500. Transpore, Repairs, Liverpool. Central 9421.

**DODGE** 1950 125 7-ton chassis-cab, 5-speed box, new crown wheel and pinion fitted, engine reconditioned, block cracked, 750 by 20 Michelin metallics as new, £55. Barnes, 14 Crown Colony, Holbeach, Lincs. Phone, Holbeach 3024.

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### Used Goods Vehicles (contd.)

**FODEN** 8-wheeler, 1948, reconditioned Gardner engine, oil boost brakes, platform body, good 40 by 8 tyres, £1,250.  
**FODEN** 4-wheeler, 1955, 9-tonner, fitted with Foden 2-stroke engine, nearly new 9.00 by 20 tyres, £1,650.  
**BARNARDS** of Stowmarket, Suffolk. Phone 621 (five lines). 799-201

### BIRMINGHAM COMMERCIAL OFFER:—

**1952 FODEN** 6-wheeler, S.A. boxvan body with 6LW, new 8.20 tyres all round, one C licence owner, serviced by makers from new.  
**1952 FODEN** 6-wheel tipper, new 18-ft. 6-in. body and gear, reconditioned engine 6LW, chassis rebuilt, tyres 900 by 20, as new all round.

TERMS AND EXCHANGES.  
**560 COVENTRY ROAD.**  
BIRMINGHAM, 10.  
Phone, Victoria 0437-8. 799-447

**1951 FODEN** FG model 6-wheeler, 22-ft. platform body in excellent running order, 1950. Terms and exchanges.  
**1951 FODEN** 8-wheeler, FG type, double drive, Gardner 6LW engine, one owner since new, well maintained by very large concern, £1,150, choice of four. Terms and exchanges.  
**RUSH GREEN MOTORS**, Langley, Hitchin, Herts. Stevenage 175. 799-261

### FORD THAMES AND FORDSON

**4 X 4 WOLFE**, ex-W.D., delivery mileage only, new and rebuilt trucks, also Canadams, Cundey and Stewart, Ltd., Alfreton, Derbyshire. Phone, Leabrooks 477. 799-447

**£1,400** 7-ton Thames Trader, 160-in. wheelbase, 6-in. truck body, ex-demonstration model, low mileage, first registered October, 1958, and in new condition, has the following factory-fitted equipment: heater, flashers, rear shock absorbers, H.D. frame, twin passenger seats, cost new £1,590. Dispatch Motors, Waterloo 4959. 799-902

**4D** Diesel long-wheelbase van, aluminium body with wheel, 6595; photo, exchanges. Lawton-Goodman, 135 Crickwell Broadway, N.W.2. Gladstone 2226. 799-56

**1958** 3,780 miles only, Thames 30-cwt. 2-ton drop-side truck, 4-cylinder petrol engine, hand-operated tipping gear, front gantry, flashing indicators, painted in cream, very suitable small builder, £715. **R. P. MOTORS LTD.**, Central Garage, High St., Redhill, Surrey. Phone, Redhill 3933-4-5. 799-11

**THAMES** 1962 5-ton long-wheelbase truck, P6 engine, 14-ft. drop-side steel body, Anthony lift, £260.  
**THAMES** 1953 4-ton long-wheelbase tipper, all-steel 6-cu.-yd. body, Edgware garage, £490.  
**THAMES** 1956 2-ton truck, 4D, £390. Godfrey Davis, Ltd., Neasden Lane, N.W.10. Dol 8000. 800-7188

**1954 FORD** Sussex P6 diesel double-drive 6-wheeler, twin rear tyres, all-metal double-sided body, used on "C" chassis, very suitable small builder, £490, only, cost nearly £2,000 when new, in sound and clean condition, maintained to a high standard, £495. **W.E.M. Motors** (Wimbledon), 506-508 Kings on Rd., London, S.W.20. Phone, Cherrywood 4322, 4568. 799-242

### GORDON KING MOTORS, LTD.

FORD AND THAMES DEALERS.

**TRADER** 4D diesel 4-ton chassis, fitted with 1,250-cu.-ft. low-loading Luton body, new and unregistered ex-works, £1,330.

**NEW** Thames 4-cylinder luxury 41-seater coach, demonstrator available shortly. Please ask for details.  
**TRADER** 6D diesel 7-ton chassis, fitted with 6-yd. drop-side steel-bodied tipper, Anthony hoist, 900 by 20 12-ply, flashing indicators, finished in red.  
**IMMEDIATE** delivery, list price £1,661.

**1947 FORDSON** 5-ton, fitted with 1,300-cu.-ft. all-aluminium Luton body, good tyres, etc., £75.  
**HIRE-PURCHASE** deposit now from 10%.

**ALL** Thames models in stock for early delivery; any body built to your requirements.  
**WE** are now accepting orders for the new low-frame Trader models. Please ask for details.

**MITCHAM LANE**, S.W.16. Streatham 3133-4. 799-346

**1949 FORD** 2-ton van, V8, good tyres, £50. 82 Bridge Rd., Maidenhead. 799-XA5036

**1955 FORD** 4D platform lorry, £395. Acorn 6911. 799-325

**1950 FORD** P6 diesel 5-yd. drop-side tipper, £250.

**1951 FORD** P6 diesel 5-ton long-wheelbase truck, £265.

**1952 FORD** P6 diesel 6-wheeler, £195.

**1953 FORD** 4D diesel 2-ton van, £325.

**1952 FORD** P4 diesel 2-ton van, £275.

**EDGWARE 2572.** 799-293

**1955** 4D 5-ton long-wheelbase, steel-body, drop-side tipper, low mileage, £465. Edgware 2572. 799-297

**1957 FORD** 4D diesel artic. with 20-ft. tanker trailer, in very good running order, £700.

**1956 FORD** 4D diesel long-wheelbase truck, £450.

**1955 FORD** 4D diesel Luton van, good running order, £375.

**1952 FORD** 4D diesel long-wheelbase truck, clean and in good order, £250.

**1952 FORD** diesel P6 5-yd. tipper, steel body, excellent running order, £275.

**RUSH GREEN MOTORS**, Langley, Hitchin, Herts. Stevenage 175. 799-245

**FORD** 5-cwt. vans, choice of two, both are fitted with extra and both in excellent condition, equal to new.

**J. BAKER AND CO. (DORKING)**, LTD., 273 E. London Rd., Staines. Staines 4211. 799-465

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### Used Goods Vehicles (contd.)

#### CHASESIDE MOTOR CO., LTD.

GREAT CAMBRIDGE ROAD,  
ENFIELD, MIDDLESEX.  
FORD MAIN DEALERS.  
Phone, Enfield 3456.

**1956 FORD** Thames 4D 30-cwt. truck, painted green, good tyres, well maintained and in first-class condition throughout, one owner, A1 guarantee, £445.

**1957 FORD** 5-ton 4D Trader, 33,000 miles, owned and maintained by this company, full A1 guarantee, £785.

**1958 FORD** Thames 15-cwt. van, 12,000 miles, faultless condition, cellulose finish in ivory, full A1 guarantee, £490. 799-207

**1956 FORD** 7-cwt. van, black and silver, no writing, mileage, £290.

**1955 FORD** 10-cwt. van, nearly new engine, £180.

**COOMBS COMMERCIAL (GUILDFORD)**, LTD., Portsmouth Rd., Guildford, Surrey. Phone, Guildford 62907. 799-487

**1958 FORD** SQUIRE, in showroom condition, low mileage, £265.

**1955 FORD** Thames 5-cwt., in exceptionally good condition throughout, £275.

**1953 FORD** 5-cwt., in very good condition throughout, £150. Cavendish Motors, Cavendish Rd., N.W.6. Willesden 0046-8. 799-363

#### FRANK G. GATES, LTD.

MAIN FORD DEALERS,  
GATES CORNER, E.18.  
Wan 6633.

**1958** Trader diesel articulated unit and trailer, fifth-wheel coupling, 24-ft. trailer, £1,650.

**1958** Thames 15-cwt. van, works painted, choice of two from £450.

**1953 FORD** 10-cwt. Utilicon, fold-flat seats, £245.

**1955 FORD** 30-cwt. van, £425.

**1950 FORD** 10-cwt. all-steel Luton body, very good condition, £190.

**1950 FORD** 2-ton 4D Luton, 750 cu. ft., £675. 799-394

**1955**, Late, Ford 2-ton van, excellent condition, one owner, £435. Aerodrome Autos, Sun 0071. 799-277

#### Ford Thames and Fordson Wanted

**WANTED**. FORDS, 4D vans and Perkins, all capacities. Chanders Motors, Ltd., 71 Greenwich South St., London, S.E.10. Gre 2033-4. 799-302

**W.E.M. MOTORS** (WIMBLEDON). Cherrywood 4322, 4568. A FORD for sale? Let us give you our price before deciding to sell. W.E.M. Motors (Wimbledon). Cherrywood 4322, 4568. 799-232

**WANTED**, FORD 4D, low mileage, 1957 onwards. Church Road Motors, Ltd., Hadeleigh, Essex. Phone, Hadeleigh 57271. 799-452

#### G.M.C.

##### G.M.C. Wanted

**G.M.C.** 6 x 6 wanted, also spares. Robert Dixon, Ltd., Luton Aerodrome, near Ware. Herts. Stanstead Abbots 301. 800-7151

#### GUY

**GUY** Vixen Luton van, large capacity, 1947, P4 diesel engine unit, £390. Sidney Seward, Ltd., Redcliff Way, Bristol 1. Phone 25713. 800-X5044

#### Guy Wanted

**GUY** Vixen Luton van, 1,000-1,200 cu. ft., low loading, walk-in tailboard. Particulars to 82 Bridge Rd., Maidenhead. 799-XB5036

#### JENSEN

**1948** 6-ton 23-ft. flat, good tyres, extremely good condition, £425. Edgware 2572. 799-296

**1953 JENSEN**, long wheelbase, 4-speed gearbox, Servo brakes, mechanically good, tyres all good, 23-ft. platform, £300.

**ATKINSON AND GRIFFIN, LTD.**, Kirkland Garage, A. Kendal. Phone, Kendal 1732. 799-502

#### JEN-TUG

**JEN-TUG** with two trailers, unregistered, practically unused, ideal internal works, price £275.

**APPLY**: Sworder (Motors), Lane End, High Wycombe, Bucks. Lane End 234. 799-7140

#### JOWETT

**1953** Bradford van, £85.

**COOMBS COMMERCIAL (GUILDFORD)**, LTD., Portsmouth Rd., Guildford, Surrey. Phone, Guildford 62907. 799-492

#### KARRIER

**4 X 4** unregistered low-mileage ex-W.D. trucks, c/w winches if required, Cundey and Stewart, Ltd., Alfreton, Derbyshire. Phone, Leabrooks 477. 799-447

#### LAND ROVER

**1954** LAND ROVER, good condition, £375.

**1955** LAND ROVER, excellent condition, many extras, £430.

**1951** LAND ROVER, new engine fitted, £285.

**COOMBS COMMERCIAL (GUILDFORD)**, LTD., Portsmouth Rd., Guildford, Surrey. Phone, Guildford 62907. 799-488

### Used Goods Vehicles (contd.)

#### LEYLAND

**LEYLAND** Lyng diesel gulley emptier, £375. H. W. Thomas and Son, Maidenhead 568. 799-X4514

**1953** LEYLAND Comet diesel, twin-ram tipper, £1,000.

**1952** LEYLAND Comet diesel, twin-ram tipper, £1,000.

**1952** LEYLAND Comet diesel, twin-ram tipper, £950.

**1950** LEYLAND Comet diesel, twin-ram tipper, £850.

**T. SHINGLER**, The Poplars, Liverpool Rd., Bickerstaff, near Ormskirk. Skelmersdale 220. 801-7147

**LEYLAND** Comet flat truck, late 1949, recently conditioned engine. Kirk's Motors, Longdons Rd., Lincoln. Phone 1947. 799-X4754

**1956**, October, LEYLAND Comet long-wheelbase twin-ram tipper, high-sided coal body, exceptional condition, £1,500.

**COX'S MOTORS (HILL TOP)**, LTD., 127 Hill Top, West Bromwich. Phone, Wednesbury 0470 and 1047. 799-90

**HIPPO** tipper, ex Ministry, October, 1957, registered, new gear and large alloy body fitted, suitable coal, etc., £950.

**RYLAND GARAGE, LTD.**, Ryland St., Birmingham, 16. Edgbaston 4501-5. Grams, "Diesel". 799-128

**LEYLAND** S/H 8-wheeler, fitted with new all-steel body and rear.

**LEYLAND** Octopus 8-wheel flat-platform body, 1954.

**JEFFREYS COMMERCIAL MOTORS**, Neath Rd., Swansea. Phone 7288. 799-141

**LEYLAND** 6-wheeler, E39 engine: 4-wheeler, E50 engine, £125 each. Phone, Dominion 5671 and Ripleyway 5026. 799-342

**1950** LEYLAND Comet, 7-ton short-wheelbase diesel tipper, Pilot twin-ram underfloor gear, standard 7-cu.-yd. timber body, extended to 8, completely new floor, tailgate, corner posts, reconditioned gearbox, new fuel pump, injectors, brake linings, hubs, £575. H.P. facilities, Sward, George Inn, Newport Pagnell, Bucks. 799-XA5043

**1950** LEYLAND Hippo 6-wheeler, 600 engine, double-drive, 24-ft. body, very good running order, £850.

**1950** LEYLAND Beaver 4-wheeler, 600 engine, good runner, £650.

**1950** LEYLAND 8-wheeler, 600 engine, double-drive, 24-ft. platform body, in good running order, £950.

**RUSH GREEN MOTORS**, Langley, Hitchin, Herts. Stevenage 175. 799-262

**LEYLAND** Hippo, ex-Ministry, unregistered, 36 by 8 twin rear, low mileage, £600.

**F. A. DOLMAN LTD.**, 186 Carlton Avenue, Southend-on-Sea. Phone 43262. 799-480

**J. CAMPBELL PARK, LTD.**, offer:—

**1954** LEYLAND 8-wheeler, double drive, very good condition, excellent tyres, three available now.

**J. Gateacre**, Liverpool. Phone, Gateacre 1331. 799-424

#### MACK

**TWO** 6 x 6 MACK diesels, spare diesel engine, numerous spares including front and back axles, wheels and tyres, £1,000 the lot. Kerby Motors, New Southend Arterial Rd., Hornchurch, Hornchurch. 799-274

#### MAUDSLAY

**THREE** unregistered MAUDSLAY 8-ton diesel trucks, fitted Gardner 4LW engines, excellent condition, £375 each. L. W. Vass, Ltd., Amptill, Bedford. 799-870

**TWIN-STEER**, 1948, fitted A.E.C. 7.7 engine, 20-ft. platform body, in very good condition

**MAYFAIR GARAGE (TAMWORTH), LTD.**

**COLESHILL ROAD, FAZELEY, TAMWORTH, STAFFS.**

Phone, Tamworth 1396-7. 799-42

**1947** Mogul 4-cylinder Gardner diesel lorry, £155, good 36 by 8 tyres, and one C-licence user, any trial.

**MEADWAY COMMERCIALS**, Bordesley Green Rd., Birmingham, 9. Victoria 4933. 799-23

**MORRIS AND MORRIS-COMMERCIAL**

**4 X 4** unregistered ex-W.D. M.O.S. rebuilds, also compressor trucks, winches optional. Cundey and Stewart, Ltd., Alfreton, Derbyshire. Phone, Leabrooks 477. 799-447

**MORRIS** 1955 5-cwt. van, first-class condition throughout, with heater, £325. Below.

**1955** Morris 10-cwt. J-type van, very clean, £250. Scott's, Ltd., Kingston Rd., New Malden. Malden 3633. 799-907

**1955**, October, MORRIS FC diesel, Balco extension, 20-ft. drop-side body, excellent condition, £675.

**COX'S MOTORS (HILL TOP)**, LTD., 127 Hill Top, West Bromwich. Phone, Wednesbury 0470 and 1047. 799-90

#### PALMERSTON OF KINGSTON

**MORRIS AND MORRIS-COMMERCIAL RETAIL DEALERS.**

**RELIABLE** used vehicles in stock.

**PALMERSTON COMMERCIAL MOTORS, LTD.**, 75-7 Penryn Rd., Kingston 5618. 799-144

**1954** MORRIS 10 LD1 boxvan, £250. Wray Park Garages. Phone, Reigate 2263. 799-112

### Used Goods Vehicles (contd.)

#### 1955 MORRIS

condition, reconditioned, £375. H. W. Thomas and Son, Maidenhead 568. 799-X4514

**SCAMMELL** 6 x 4 unit, in first-class condition, £1,000. Lane 175. 799-261

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**SCAMMELL** 6 x 4 unit, in first-class condition, £1,000. Lane 175. 799-261

**SCAM**



## Used Goods Vehicles (contd.)

**1955 MORRIS 10-cwt. J-type van.** excellent condition, resprayed £300. **COMBES COMMERCIAL (GUILDFORD), LTD.,** Portsmouth Rd., Guildford, Surrey. Phone, Guildford 6297.

### SCAMMELL

**SCAMMELL 6 x 4, ex-W.D.,** with winch, 30-ton tractor unit, in first-class order, tyres as new. **PELX SWORDER (Motors),** Lane End, High Wycombe, Bucks. Lane End 234. 799-7142

**SCAMMELL Scarab, September, 1951, 6-ton tractor** in excellent condition, used by ourselves since new. Licence, genuine mileage 26,000, complete with two 164 drop-side and one 18-ft. 6-in. platform semi-trailers. **CLARK (SHEFFIELD), LTD.,** Penistone Old Rd., Sheffield. Phone 20171. 800-7181

**1945 SCAMMELL 40-ton winch truck tractor, 6LW** Gardner, good condition throughout, £450. 9 Northorpe Common, Eastwood, Notts. Langley Mill 203; evenings 3000. 799-152

**1945 SCAMMELL 24-ft. platform, 8-wheeler, 6LW** engine, in good condition throughout, £450. **LEPPERS GARAGE (NANTWICH), LTD.,** Birmingham Rd., Nantwich, Cheshire. Phone 5704. 799-313

**1951 SCAMMELL 8-wheeler, 40 by 8 tyres all round,** Gardner 6LW engine, 6-speed gearbox, air makes, in very nice condition, excellent runners, choice of oil, £50 each. 799-257

**1950 SCAMMELL tractor unit, 6LW engine, 6-speed** box, air brakes, one owner since new, exceptionally clean and in good condition; also a number of other Scammell tractors. **RUSH GREEN MOTORS, Langley, Hitchin, Herts.** Stevenage 175. 799-257

**TWO 1946 25-ton SCAMMELL tractors, 6LW Gardner,** fully floating axle, on 40 by 8 twin tyres, £650 each. **KIRBY MOTORS, New Southend Arterial Rd., Hornchurch, Essex.** 42776. 799-373

**RIGID 8 SCAMMELL 2,000-cu.-ft. Luton van, 6LW** engine, high-speed axle, good tyres, any trial. Phone, Adv 5096. 799-280

### SEDDON

**SEDDON 1949 Mark 5L Perkins P6, 6-7-ton drop-side** body, one C-lit licence, price £350. **LEPPERS GARAGE (NANTWICH), LTD.,** Birmingham Rd., Nantwich, Cheshire. Phone 5591. 799-7154

**1951 SEDDON 5L 7-tonner, alloy drop-side body,** 16 ft., ex-C licence, good tyres and condition, choice of two, reasonable price. 799-262

**1951 SEDDON 7L diesel 3-tonner, 14-ft. flat,** ex-brevity licence. 799-262

**1954 SEDDON 5L 7-tonner, 16-ft. drop-side,** repaired, ready for work, good tyres. **PART-EXCHANGES or terms. New SEDDONS** supplied. 799-480

**RYLAND GARAGE, LTD., Ryland St., Birmingham,** 16, Edgbaston 4501-5. Grams, "Diesel." 799-126

**1951, Fitted with reconditioned P6 engine, 20-ft.** platform body, very good condition. 799-424

**Fitted with P6 engine, 17-ft. 6-in. platform body,** good very good condition. 799-424

**1953, Articulated body, fitted with P6 engine, 22-ft.** platform semi-trailer, the whole unit is in very good condition. 799-374

### MAYFAIR GARAGE (TAMWORTH), LTD.,

COLESHILL ROAD, FAZELEY,

TAMWORTH, STAFFS.

Phone Tamworth 1396-7. 799-43

**1949 SEDDON Mk. 5, P6, 17-ft. platform, well** shod, and a good runner. £300. Cottee and Edwards, Ltd., Nottingham. Phone 52213. 799-73

**1951 SEDDON Mk. 4 bus chassis and cab, suitable** for 20-ft. body, £400. Cottee and Edwards, Ltd., Nottingham. Phone 52213. 799-74

**1955 SEDDON 5L 7-ton long-wheelbase drop-side,** 2-speed axle, Continental cab. 799-143

**GEO. H. KENDRICK, LTD., Carters Green, West** Bromwich 0778. 799-143

**HILLS. SEDDON diesel 6-7-ton long-wheelbase tipper, 14 ft.** S by 7 ft. with 3 ft. sides. Pilot twin vertical gear, first registered 1948. 799-73

**SEDDON diesel Mk. 15 7-tonner platform lorry, 18 ft.** S by 7 ft. 4 in., one owner, first registered August, 1957. 799-74

**HILLS operate a round-the-clock 24-hour service for** SEDDON parts despatched at any hour by post or passenger train. 799-60

**HILLS GARAGES (MANCHESTER), LTD., Port St.,** Manchester. 1. Phone, Central 4311. Grams, Hills, Manchester. 799-60

**1954 SEDDON 6-7-ton boxvan, aluminium body** and cab, one owner, 90% tyres all round, excellent condition throughout. £645. Aerodrome Autos, Sun 0071. 799-278

### STANDARD

**1957 6-cwt. van, exceptionally clean, plain dark blue,** Brew Bros., Ltd. Fre 3333. 799-319

**1956 STANDARD 7-cwt. van, one owner, low** mileage. 799-464

**E. BAKER AND CO. (DORKING), LTD., 273** London Rd., Staines. Staines 4211. 799-464

### THORNYCROFT

**4 X 4 unregistered ex-W.D. trucks, very good selection,** Cundy and Stewart, Ltd., Alfreton, Derbyshire. Phone, Leabrooks 477. 799-551

**1954 THORNYCROFT 8-wheeler double-drive, air** brakes, 24-ft. platform body, one owner since new, very clean and in good running order, £1,250, any trial. Terms and exchanges. 799-260

**RUSH GREEN MOTORS, Langley, Hitchin, Herts.** Stevenage 175. 799-260

### Thornycroft Wanted

**THORNYCROFT Amazon 6 x 4 chassis, any condition,** Price and particulars to H. Sellers (Leeds), Ltd., 79 Clarence Rd., Leeds, 10. Phone 34291. 799-7104

## Used Goods Vehicles (contd.)

### TROJAN

#### Trojan Wanted

**WANTED. TROJAN low-mileage models, 1955** onwards. Church Road Motors, Ltd., Hadleigh, Essex. Phone, Hadleigh 57271. 799-453

### VOLKSWAGEN

#### FERRARIS OF CRICKLEWOOD, L. TD.,

200-220 CRICKLEWOOD BROADWAY,

N.W.2.

Gladstone 2234-5-6-7.

**1956 VOLKSWAGEN van in blue, excellent** condition. 799-341

### VULCAN

**1950 VULCAN medium-wheelbase tipper, P6 engine,** wooden drop-side body, £300. Cottee and Edwards, Ltd., Nottingham. Phone 52213. 799-72

**1947 VULCAN P6 long-wheelbase truck, very clean,** good working order £150. Edgware 2572. 799-294

### UNCLASSIFIED

#### CAR MART, LTD.

SIX MONTHS' GUARANTEE

WHERE STATED.

#### 10% DEPOSIT.

**1955 FORD 10-cwt. van, £215.**

**1956 BEDFORD Spurtomility, £375.**

**1956 MORRIS 14-ton van, £345.**

**1958 BEDFORD 25-cwt. diesel (Hawson body) van,** 1,000 miles, guaranteed, £895.

**1950 MORRIS 15-20-cwt. van, £155.**

**1949 THORNYCROFT Sturdy diesel long-wheelbase** drop-side, £225.

**1957 MORRIS 13-seater Minibus, 20,000 miles,** guaranteed, £545.

**1957 MORRIS Cowley van, guaranteed, £465.**

#### THE CAR MART, LTD.,

WELSH HARP,

EDGWARE ROAD, N.W.9.

Hendon 6500. 799-52

#### HAZLEMERE MOTOR CO.

(WALTHAM ABBEY), LTD.

Waltham Cross 2275-7.

**BEDFORD DEALERS, FODEN DISTRIBUTORS,**

PERKINS SIGNHOLDERS.

**1955 B.M.C. 5-ton diesel long-platform truck, in** excellent condition.

**1956 BEDFORD A model drop-side truck,** in excellent condition.

**1956 BEDFORD A model chassis and cab, 25-in** chassis extension, fitted with new 1,450 c.c. Luton body, in light alloy.

**1951 FODEN DG 6-12 platform.**

**1954 FODEN FG 6-wheeler platform.**

**1953 SEDDON 6-ton diesel platform.**

**1955 A40 van, excellent condition.**

**1951 FOX'D 5-cwt. van, reconditioned and painted**

**1952 LAND ROVER standard 88-in. wheelbase,** petrol, excellent condition. 799-39

#### PRALLS (HEREFORD), LTD.,

COMMERCIAL VEHICLE SPECIALISTS.

**A. ATKINSON, A. AUSTIN, D. DODGE, F. FORD.**

**1956 FORD 4-ton diesel platform lorry**

**1956 SEDDON short-wheelbase tipper, 6-yd. wood** body.

**1954 FORD 3-ton 4-cylinder petrol platform lorry.**

**1951 DENNIS Max, 20-ft. flat.**

**1946 VULCAN P6, 16-ft. alloy body, very clean.**

#### PRALLS (HEREFORD), LTD.,

HOLMER ROAD, HEREFORD.

Phone 4221 (six lines). 799-71

**1956 COMMER TS3, 11-ft. 9-in. wheelbase, alloy** platform, extras, excellent condition; choice of

**1952 VULCAN petrol long-wheelbase platform.**

**1953 MORRIS 5-ton diesel double-drop-side.**

**1952 DODGE 6-ton petrol, alloy platform, 8.25 by** 20 tyres, Eaton axle set.

**1951 THORNYCROFT 8-wheeler drop-side.**

**1952 THORNYCROFT 8-wheeler 6LW drop-side.**

**1954 THORNYCROFT Trident long-wheelbase** double-drop-side.

**1953 E.R.F. 4-wheeler, 4LW, air brakes.**

**1954 E.R.F. 8-wheeler, 6LW.**

#### PARRS (LEICESTER), LTD.,

ABBEY LANE, LEICESTER.

Phone 61511 (seven lines). 799-107

## February 20, 1959—THE COMMERCIAL MOTOR 45

(Supplement)

## Used Goods Vehicles (contd.)

### HARRY DANDO.

VAUXHALL AND BEDFORD MAIN DEALERS,

WEST END GARAGE,

CHIPPING SODBURY, BRISTOL.

Chipping Sodbury 2277 (four lines).

**YORK trailers.** We stock the full range of the new British-built York trailers, including Scammell-coupling models, maximum loading 16 tons, double-axle semi-trailers, 25 ft. long. Much the best trailers built today.

**TRADE for Wiltshire, Gloucestershire, Somerset, Devon** and Cornwall.

### NEW VEHICLES.

**A.E.C.** Mustang chassis and cab, takes 21-ft. body, £3,612.

**A.E.C.** Mercury long-wheelbase, takes 21-ft. body, £3,025.

**A.E.C.** Mercury Mark II tractor unit, fitted with trailer, knock-out axle, £4,342.

**A.E.C.** Mercury Mark I tractor unit complete with trailer, used Carrimore 25-ft. platform trailer, S.A.E. coupling, complete outfit, £3,151 13s. 6d.

**BEDFORD 6-ton medium-wheelbase (132-in.), fitted with** Pilot twin front ram tipping gear and wood drop-side body, £2,725.

**BEDFORD Martin Walter ambulance mounted on** 10-12-cwt. chassis, two stretchers, four or five sitting patients, complete in every detail, registered but unused, £775.

### SPECIAL A-LICENCE VEHICLES.

**BEDFORD 1957 R6 Boys trailing-axle 6-wheeler, R6 engine,** platform body, good condition throughout, 4-ton 10-cwt. West Midlands licence, expires October 31, 1959.

**GUY Otter 1953, P6, 2-speed axle, fair condition, 2-ton** 19-cwt. Western Area licence, expires March 24, 1961.

### LONG WHEELBASE.

**BEDFORD 1957 R6 Boys rigid 6-wheeler, exceptionally** good condition throughout, £1,700; choice of two.

**BEDFORD 1953 A-type Luton van, exceptionally good** condition, petrol engine, just fitted with new body, approximately 900 cu. ft., in primer, £1,000.

**ALBION 1950 double-drive 8-wheeler, 24-ft. platform** body, Albion engine, the complete machine is in exceptionally good condition throughout, £1,200.

**GUY Otter 1953, P6, 2-speed rear axle, platform body,** clean machine and good mechanical condition, £550.

**BEDFORD 1953 A-type 5-ton standard long-wheelbase** platform, one very careful owner since new, £400.

**BEDFORD 1954 A-type P6 long-wheelbase, double-** drop-side, one owner, good condition, tyres fair, ready for work, £550.

**E.R.F. 1954 twin-steer, 5LW, 21-ft. drop-side wood** body, one owner, fitted with 10.00 by 20 tyres, in exceptional condition, £2,250.

**SEDDON 1947, P6, long-wheelbase double-drop-side,** £200.

**SEDDON 1948, Gardner 4K engine, very good** condition throughout, £400.

**BEDFORD, September, 1958, 300 diesel engine, long-** wheelbase platform, 36 by 8 tyres, £1,450.

**BEDFORD 1954 long-wheelbase, R6, good condition** throughout, choice of two, £700.

**DODGE 1955 long-wheelbase, P6, £450.**

**A.E.C. Mammoth Major 1947 8-wheeler, 7.7 engine,** 2-stick model, aluminium platform body, wood floor, exceptionally well maintained vehicle in good, clean, ready-to-work condition, £900.

**SEDDON 1953, P6, long-wheelbase platform, fair** condition throughout, £450.

**MAUDSLAY 1948, A.E.C. 7.7 engine, twin steer,** platform body, fair condition, £250.

**GUY Otter 1953, P6 2-speed rear axle, platform body,** clean machine and good mechanical condition, £400.

**THORNYCROFT 1949 long-wheelbase, diesel engine,** poor condition £100.

### TRAILERS AND ARTICULATED.

**YORK new 10-ton 25-ft. platform trailer, S.A.E.** coupling, 9.00 by 20 tyres, £728.

**DYSON new 15-20-ton low-loading semi-trailer, 20 ft.** in well, vacuum brakes, £1,575.

**WILDERLEY 10-ton full-trailer low-loader, 12 ft. in** well, vacuum brakes, complete with drawbar, knock-out axle, £350.

**BEDFORD-SCAMMELL 1948 tractor unit, choice of** two, £100 each.

**TASKER platform double-axle semi-trailer, air brakes,** modern trailer in exceptionally good condition, Tasker coupling, £600.

**CARRIMORE 12-ton 25-ft. platform trailer, excep-** tionally good air brakes, S.A.E. coupling, £450.

**B.M.C. condition, £800.**

**ALBION 1951 4-cylinder diesel, 25-ft. Carrimore close-** coupled semi-trailer, £675.

**E.R.F., 20 ft. in well, 15-20-ton Dyson low-loading** trailer and Tasker 14-ton platform tandem-axle semi-trailer, all S.A.E. coupling, will split, the tractor unit is ex well-known C-lit licence operator, is in outstandingly good condition, complete outfit £3,000.

### TIPPERS.

**LEYLAND Comet, October, 1954, long-wheelbase,** normal control, twin front-ram tipping gear, good body, good condition throughout, £1,250.

**AUSTIN 1955 5-ton steel-body petrol tipper, clean** condition, £425.

**COMMER 1952 P6 standard wood-body tipper, £400.**

**SEDDON, 1956, Pilot underbody gear, wood body,** 1-owner vehicle, in really good condition, £900.

**DODGE 1947 P6 medium-wheelbase wood-body tipper,** £100.

**VULCAN 1949 P6 standard tipper, £100.**

**MAUDSLAY 1949 8-ton tipper, A.E.C. 7.7 engine,** quite good condition throughout, £600.

**BEDFORD, 1953, petrol, 5-ton steel body, excellent** condition throughout, £250.

### HARRY DANDO.

VAUXHALL AND BEDFORD MAIN DEALERS,

WEST END GARAGE,

CHIPPING SODBURY, BRISTOL.

Chipping Sodbury 2277 (four lines). 799-435

R41

### Used Goods Vehicles (contd.)

**ROUTES, L. TD.**  
OFFER FOR SALE THE LARGEST STOCK OF  
**USED VEHICLES**  
FROM THIS SELECTION.

**BIRMINGHAM, 18.**  
GREAT HAMPTON STREET,  
CEN 8411.  
**1948** COMMER 5-ton 18-ft. dropsider, petrol, £100.  
**1954** BEDFORD 7-ton diesel drop-sider, £550.  
**1955** FORD 4D 5-ton drop-sider, £450.  
**1951** BEDFORD 5-ton drop-sider, £175.  
**1949** FORD 2-3-ton drop-sider, £50.  
**1956** AUSTIN A40 van, very good condition, £315.  
**1953** BEDFORD 5-ton tipper, petrol, £300.  
**1950** SENTINEL 8-ton drop-sider, D1 diesel engine, £375.

**MANCHESTER, 15.**  
OLYMPIA, CHESTER ROAD,  
BLA 6677.

**1958** June, 30-cwt. COMMER forward-control diesel, super-capacity van, painted grey, excellent condition, used for demonstration only, mileage 8,000, cost £1,030, £895.  
**1951** August, COMMER 5-ton UFE 15-ft. 6-in. platform lorry, painted yellow, very good condition, £195.  
**1953** November, AUSTIN 5-ton Loadstar 18-ft. platform lorry, painted red, excellent condition, £175.  
**1955** November, 25-cwt. COMMER forward-control van, painted black, good tyres, £325.  
**1957** March, BEDFORD 7-ton drop-sider, fitted B.M.C. Leyland Comet engine, 60,000 miles, painted blue, good tyres, excellent condition, £1,100.  
**1957** August, COMMER 15-cwt. van, diesel engine, painted blue, 23,000 miles only, excellent condition, good tyres, £700.  
**1956** July, COMMER 7-ton TNS 18-ft. flat, high-loading board, good condition, good tyres, 9.00 by 20, painted red, £950.

**MAIDSTONE,**  
LEN ENGINEERING WORKS,  
PHONE 3333.

**1958** AUSTIN A35 van, petrol, black, £375.  
**1955** FORD Thames P6 drop-side truck, diesel, brown, £500.  
**1955** FORD 4D platform truck, diesel, blue, £425.  
**1957** COMMER express delivery van, petrol, antelope and pearl grey, £395.  
**LATE 1958** COMMER 5-ton platform truck, diesel, blue, low mileage, £1,205.  
**1957** COMMER 7-ton platform, diesel, green, £1,385.

**ROCHESTER,**  
HIGH STREET,  
CHATHAM 42331.

**1945** FORDSON 4-ton van, brown, one owner, £99.  
**1950** November, Bradford van, green, mechanically sound, good tyres, £150.  
**1957** COMMER Cob light van, green, one owner, good condition, heater, £399.  
**1957** COMMER ED van, blue, one owner, as new, excellent condition, £590.  
**1947** DODGE Luton van, blue-grey, good engine and good running order, £125.  
**1953** MORRIS van, maroon-silver, very good condition, £369.  
**1948** BEDFORD van, green, mechanically sound, £85.

**CANTERBURY,**  
THE PAVILION,  
PHONE 3322.

**1956** COMMER Cob van, fitted passenger seat, £335.  
**1949** VULCAN 6-ton truck, good condition, £250.  
**1945** BEDFORD 2-3-ton cattle truck, £125. 799-168

**TIPPERS!**  
**SEDDON** Mk. 5L long-wheelbase tipper, 1956, Eaton 2-speed axle, 9.00 by 20 tyres, £1,050.  
**SEDDON** Mark 5L long-wheelbase tipper, 1956, Eaton 2-speed axle, 9.00 by 20 tyres, £980.  
**SEDDON** Mk. 5510 tipper, 1956, new engine recently fitted, tyres good, £975.  
**COMMER** TS3 tipper, 1955, 7-tonner, tyres fair, £925.  
**BEDFORD** 6-ton tipper, 1958, Bedford engine, one owner, £895.  
**BEDFORD** 7-ton tipper, 1957, diesel, very clean, £850.  
**DODGE** 144AR6 tipper, 1954, tyres reasonable, a clean vehicle, £850.  
**COMMER** diesel tipper, 1954, reasonably clean vehicle, £595.  
**DODGE** 103AP6 tipper, 1951, new engine recently fitted, tyres fair, £395.

**LONG-WHEELBASE TRUCKS.**  
**GUY** Otter, 1953 long-wheelbase truck, alloy body, P6 engine, Eaton 2-speed axle, £500.  
**DODGE** 1949 petrol long-wheelbase drop-side, wooden body, £175.

**DEMONSTRATION WITHOUT OBLIGATION!**  
**COVENTRY AND JEFFS, L. TD.**  
COMMERCIAL-VEHICLE SPECIALISTS,  
STAFFORD STREET, BRISTOL, 3.  
Phone 6-4661. 799-118

### Used Goods Vehicles (contd.)

**W. JONES (MANCHESTER), L. TD.**  
RUTLAND STREET GARAGES,  
SWINTON.

AGENTS FOR ALL ATKINSON VEHICLES.

**1958** LEYLAND Comet, 20-ft. Homalloy body, as new, £1,975.  
**1957** MORRIS J2 van, 15-cwt., new look, very clean, £450.  
**1957** BEDFORD 5-ton drop-side truck, Perkins P6 engine, very good, only needs seeing, £725.  
**COMMER** TS3 artic., 25-ft. new Scammell trailer, used on trade plates, unregistered, £1,500.  
**BEDFORD** C.A.V. van, 10-cwt., very clean, £285.  
**1956** AUSTIN B.M.C. Luton van, 24 ft. behind driver, 9-ft. high interior, well tyre, good all round, £895.  
**1956** COMMER TS3 diesel tipper, choice of two, sound condition, each £1,075.  
**1956** B.M.C. diesel Luton van, 1,500 cu. ft., walk-in tailgate, perfect, £875.  
**1956** AUSTIN short-wheelbase tipper, Homalloy B.M.C. diesel, mechanically perfect, choice of two, each £695.  
**1956** October, FORD diesel short-wheelbase tipper, mechanically perfect, £525.  
**1956** November, BEDFORD 7-ton long-wheelbase diesel truck, 17-ft. 6-in. body, 900 by 20 tyres, very clean machine, £845.  
**1955** December, AUSTIN A40 gown van, £425.  
**1955** BEDFORD Luton van, petrol, 850 cubic capacity, £495.  
**1954** BEDFORD A-type diesel artic., with new 25-ft. Scammell trailer, very clean outfit, £875.  
**1954** October, BEDFORD Luton van, P6 engine, £850.  
**1954** SEDDON, P4 engine, roller shutter sides and back, Homalloy body, £450.  
**1954** MORRIS LDI Personnel Carrier, 12 seats, 12,000 miles only, £415.  
**1954** September, BEDFORD 30-cwt. van, A-type petrol, £395.  
**1954** BEDFORD boxvan, separate cab, A-type petrol, platform loader, £395.  
**1950** 51 MORRIS diesel Luton vans, choice of three, 14,000 cu. ft. Bonalock aluminium bodies, under 3 tons unladen, mechanically perfect, each £450.

### SUPER BARGAINS TO CLEAR.

**1955** COMMER boxvan, £255.  
**1955** FORD Consul, excellent condition, one owner, £435.  
**1954** BEDFORD 7-ton diesel, flat, £325.  
**1954** December, BEDFORD Dormobile, immaculate, £300.  
**1953** FORD P6 engine tipper, good condition, £250.  
**1953** FORD 2-ton, 4-cylinder Cost Cutter engine, van, £150.  
**1948** BEDFORD truck, £50.  
**1937** AUSTIN 16 car, £15.

LET US QUOTE FOR A LUTON BODY BUILT TO YOUR OWN SPECIFICATION.

ALL INQUIRIES  
**PHONE, SWINTON 1855.**  
HEAD OFFICE,  
5 MARSDEN COURT, MANCHESTER, 4.  
Phone, Blackfriars 6037.  
After hours, Wigan 46049.  
GARAGE OPEN DAILY (INCLUDING SUNDAY). WEEK-END 10 TO 5. 799-325

**MORTON'S GARAGE, L. TD.**  
81 BINLEY ROAD,  
COVENTRY.  
Phone 53354.

### COMMERCIAL KARRIER DEALERS.

**1956** COMMER 7-ton tipper.  
**1956** BEDFORD 7-ton long-wheelbase tipper.  
**1955** AUSTIN 5-ton short-wheelbase tipper.  
**1956** MORRIS Cowley 3-ton van.  
**1955** BEDFORD 10-12-cwt. van.  
**1948** BEDFORD 10-12-cwt. van.

**PART-EXCHANGES.**  
**HIRE-PURCHASE. INSURANCE.**  
OPEN SEVEN DAYS A WEEK. 799-449

**DON EVERALL, L. TD.**  
**1958** AUSTIN Omnitruck, 2-tone paint, many extras, one owner, £495.  
**1958** AUSTIN Omnicoach, 2-tone paint, heater, 11,000 miles, first-class condition, £535.  
**1957** COMMER Cob van, blue, fitted with rear passenger seat, £325.  
**1957** STANDARD 6-cwt. van, black, heater, £295.  
**1955** November, BEDFORD 7-ton long-wheelbase drop-side lorry, one owner, low mileage, £525.  
**1953** BEDFORD-SCAMMELL 8-ton, 22-ft. straight-frame trailer, £625.  
**1952** LEYLAND Steer, one C-licence owner, good running order, 20-ft. double-drop-side body, at £695.  
**1951** LAND ROVER and trailer, one owner, £260.

**DON EVERALL, L. TD.,**  
34 CLEVELAND ROAD,  
Wolverhampton 23212. 799-155

### Used Goods Vehicles (contd.)

**THE MILLBURN ORGANIZATION,**  
PRESTON: LONGTON 3255.  
GLASGOW: BELL 0073.  
CARLISLE: 25422.

**AUTHORIZED DEALERS.**  
**ALBION, FORD**  
COMMERCIALS.

**NEW** ALBION Reiver 6-wheel chassis, 6-speed gearbox.  
**NEW** ALBION Clyde side chassis, 6-speed gearbox.  
**NEW-TYPE** ALBION Chieftain chassis and cab.  
**NEW** Thames Traders, 6-cylinder diesel engines, 5- and 7-ton models. Other Thames models 5-cwt. upward. New York 25-ft. trailers with Scammell coupling.  
**1956** B.M.C. 7-ton diesel long-wheelbase platform lorry. Also a number of 1948 Albion FT1 lorries, fresh clean vehicles, ex-brewery.  
**1955** September, SEDDON long-wheelbase platform weight 2 tons 18 cwt., very clean, good tyres.  
**1954** ATKINSON long-wheelbase lorry, Gardner 4LK unit, very fine order.  
**1952** VANGUARD 10-cwt. van.  
**1949** MAUDSLAY 8-wheeler, 9.6 A.E.C. power unit, good mechanical order.  
**1948** SEDDON, P6 unit, good order.  
**1948** A Number of ALBION FT3 petrol long-wheelbase platform lorries, ex-brewery, very clean, good tyres.  
**A.E.C.** Matador 4 by 4, unregistered, petrol unit, all as new and complete with winch.  
**A** Number of LEYLAND Beaver lorries, 6-cylinder diesel.  
**A** Number of Cranes and Dyson draw-bar trailers.  
**A** A number of containers.

**FORK-LIFT TRUCKS.**  
**FORK-LIFT** trucks, 3,000- to 6,000-lb. capacity, 9-ft. and 12-ft. lift; also Morris Versatile 5-ton Mobile crane, electrically operated, Perkins P4 diesel unit mounted on 4-wheel chassis, all in new condition.  
**N.B.**—Next motor auction sale, March 5.

**MILLBURN MOTORS (PRESTON), L. TD.,**  
WALMER BRIDGE, LONGTON, PRESTON.  
Phone, Longton, Lanes, 3255-6. 799-67

**STOCKLAND GARAGE, L. TD.,**  
ERDINGTON, BIRMINGHAM, 23.  
Phone, Erd 2488.

**NEW** commercial vehicles in stock for immediate delivery.  
**FORD** 3-5-ton tipper, 4D diesel.  
**FORD** Thames Trader 7-ton long-wheelbase truck, diesel.  
**AUSTIN** 4-ton long-wheelbase drop-side truck, diesel.  
**USED** commercial vehicles.  
**1956** AUSTIN B.M.C. 7-ton tipper, 11½-cu.-yd. body, double drop sides, twin-ram tipping gear, heater, body size 14 ft. 6 in. by 7 ft. by 3 ft., steel floor, good tyres, power steering, Eaton axle, one owner driver, £1,100.  
**1950** AUSTIN 2-3-ton Luton van, £120.  
**1947** AUSTIN 2-3-ton Luton van (choice of two), repainted blue, £120.  
**1947** AUSTIN 2-ton tipper, £50.  
**1947** 8 BEDFORD 5-ton long-wheelbase platform, from £120.  
**1954** COMMER 3-4-ton boxvan, roller back and side entrance, repainted blue, £250.  
**GREENWOOD** electric factory trucks (two) with chargers, £75 lot.  
**MORRISON** 5-ton electric hydraulic tipper, excellent condition, batteries as new, £65.  
**AUSTIN** 5-ton Loadstar platform trucks, ex-brewery vehicles, 1950-1953, from £150-£275.  
**USED** coaches for sale—Under P.S.V. column. 799-131

### USED UNITS

**1944** ATKINSON 8-wheeler double-drive long-wheelbase chassis and cab.  
**1956** ALBION Reiver, 0350 engine, 22-ft. platform.  
**1942** E.R.F. long-wheelbase flat, 5LW, 5-speed box.  
**1946** THORNYCROFT TR6 tractor and trailer.  
**1951** KARRIER Bantam short-wheelbase tipper, hydraulic brake.  
**1952** FORD Sussex, P6 engine, 20-ft. platform body.  
**1953** GUY Otter, P6 engine, 15-ft., platform body.  
**1953** BEDFORD A-type parcels van.  
**1942** LEYLAND TSC18 twin steer 20-ft. platform.  
**1952** BEDFORD 2-ton parcels van; choice of four.  
**1945** FODEN 8-wheeler long-wheelbase tipper, 6LW, 19-ft. body, twin ram gear.  
**1952** AUSTIN Loadstar parcels van.  
**1946** SEDDON, P6 engine, 5-speed box, 17 ft. 6 in. flat.

**PHONE CALL, WRITE**  
**USED UNITS,**  
WHITTLEFIELD,  
BURNLEY,  
LANCS.  
Phone, Burnley 2262. 799-119

## Used Goods Vehicles (contd.)

### S. HUGHES (COMMERCIAL), LTD.

LODGE GARAGE,  
WHITEHALL ROAD WEST,  
GOMERSAL, NEAR LEEDS.  
Phone, Dudley Hill 1144-9.

OFFER FOR IMMEDIATE DELIVERY:—

#### NEW VEHICLES.

**E.R.F.** 6LW, 5-speed, double-drive 8-wheeler.  
**E.R.F.** Twin Steer, 6LW.  
**A.E.C.** Mercury Mark II chassis and cab, fitted with Milshaw tipping gear and latest-type rack stabilizer.  
**FODEN** 8-wheel 6LW double-drive chassis and cab.  
**FODEN** 4-wheeler latest 2-stroke, to take 22-ft. body.  
**ALBION** Chieftain, fitted roller springs, 900 by 20 tyres, chassis and cabs, 19 ft. flats.  
**LEYLAND** Super Comet.  
**LEYLAND** Comets, 20-ft. flats.  
**E.R.F.** 4.4 (G).  
**FODEN** 8-wheel tipper.  
**A.E.C.** 9.6 8-wheeler double-drive chassis and cab.

#### USED 8-WHEELERS.

**1956** LEYLAND 600, fitted with 24-ft. flat, immaculate condition.  
**1955** LEYLAND 600, double drive, 24-ft. flat.  
**1952** FODEN, latest 2-stroke Mark V engine, double drive, first-class machine throughout.  
**1951** LEYLAND 600 engine, double-drive, 24-ft. flat.  
**1950** FODEN 6LW double drive, 24-ft. flat, first-class condition.  
**1950** FODEN 6LW double drive, fitted 24-ft. alloy flat.  
**1950** MAUDSLAY, fitted 9.6 5-speed box, 24-ft. flat, been used from new by C-licence operators.  
**1948** A.E.C. 9.6 double drive.  
**1948** SCAMMELL 6LW; choice of two.  
**1946** ATKINSON 6LW, double drive.  
**1955** A.E.C. 9.6 double-drive.

A Number of cheap 8-wheelers, including E.R.F., A.E.C., etc., from £300 each.

#### USED TIPPERS.

**1956** LEYLAND double-drive, fitted with new Milshaw twin-ram gear with latest-type rack and 22-ft. 6-in. alloy fixed-sided body, as new.  
**1955** THORNycroft, sturdy, medium wheelbase, fitted reconditioned engine, Anthony hoist gear and steel body.  
**1955** BEDFORD A type, petrol, fitted new gears and bodies, choice of three.  
**1952** ALBION HD 8-wheeler, fitted new Milshaw rear and body.  
**1950** FODEN, fitted reconditioned engine, new tipping gear and rack stabilizer and 22-ft. 6-in. alloy fixed-sided body.  
**1948** A.E.C. 9.6 double-drive 8-wheeler, Pilot gear and 20-ft. wood body.  
**1948** DENNIS tipper.  
**1946** 47 E.R.F., 7.7, fitted with tipping gears and new wood bodies, choice of four.  
**1955** ALBION Chieftain medium-wheelbase tipper, fitted new gear and body.  
**1946** FODEN 6LW, fitted with new gear and fixed-sided body, 18 ft. 6 in.

#### USED 6-WHEELERS.

**1951** SENTINEL 6-wheeler, direct-injection engine.  
**1948** DENNIS, fitted 22-ft. 6-in. body, double drive.  
**1947** DENNIS, fitted Boys extension.

#### USED TWIN STEERS.

**1950** MAUDSLAY twin steers, chassis and cab, choice of two, £500 each.  
**1948** MAUDSLAY, fitted 7.7, 5-speed box, choice of four.

#### USED 4-WHEELERS.

**1956** COMMER TS3, fitted 9.00 by 20 tyres, 18-ft. bodies, in first-class condition, choice of two.  
**1955** COMMER TS3, fitted with 19-ft. drop-sided body.  
**1953** E.R.F. 4LK drop-side lorry.  
**1951** MAUDSLAY 7.7 20-ft. flat.  
**1951** ALBION HD, fitted with trailer equipment, all new tyres, in first-class condition.  
**1950** E.R.F. long-wheelbase, fitted with 6LW, immaculate condition.  
**1948** ALBION CX, choice of three.  
**1947** 48 MAUDSLAY, 7.7 engine, 4LW, choice of four.  
**1945** 48 E.R.F., fitted 7.7, choice of five.  
**1950** E.R.F. 6LW, 18-ft. 6-in. drop-sided body, trailer model.  
**1955** A.E.C. Monarch, fitted with 22-ft. alloy body, as new.  
**1951** ALBION Chieftain, drop-sided body.  
**1958** COMMER 2-stroke, overdrive, 9.00 by 20 tyres, as new.  
**1958** BEDFORD (Bedfords own oil engine) 7-tonner.  
**1957** COMMER long-wheelbase, 9.00 by 20 tyres.  
**OVER 50** 4-wheelers to choose from.

#### USED TRACTORS.

**1948** SCAMMELL, 6LW.  
**1943** SCAMMELL, ex-petrol company.

(Continued in next column)

## Used Goods Vehicles (contd.)

**1942** SCAMMELL 6LW 45-tonner.

**1938** SCAMMELL low-loader, fitted with 6LW engine, Carrimore low-loading trailer, knock-out axle, 16-ft. 6-in. well, all new tyres on trailer, 14.00 by 20; cheap to clear.

A Number of all types of trailers.

#### USED ARTICULATED:

**1955** SEDDON, R6, fitted with 22-ft. Carrimore trailer.  
ALL classes of articulated and 4-wheeler trailers.  
SPARES for all types E.R.F., A.E.C., Foden, including engines, 7.7, Perkins P6, Perkins R6, 4, 5 and 6LW, 9.0 A.E.C.  
DIFFERENTIALS and gearboxes for all models, cheap to clear.

NIGHT PHONE.

MIRFIELD 3183, 2160.

WALES:

R. COWDELL, NEWPORT 59866. 799-443

TWO 1955 A.E.C. single-drive 8-wheel tippers, 9.6-litre engines, Pilot U7 underbody tipping gears, 21-ft. wooden bodies on 9.00 by 20 tyres; can be inspected working.  
SEVERAL 1947-50 Maudslay and E.R.F. 20-ft. platform bodies, all in first-class working condition.  
TWO 1950 FODENS, fitted with 6LW Gardner engines, 24-ft. alloy bodies, on 9.00 by 20 tyres, condition immaculate.  
SEVERAL A.E.C., Leyland and Foden 8-wheel tippers, all fitted with 21-ft. wooden bodies, Pilot tipping gears on 9.00 by 20 tyres; these vehicles are working daily and can be inspected by appointment.

APPLY

MURPHY BROS., LTD.

FEATURE BUILDINGS,  
MELTON ROAD, SYSTON, LEICS.  
Phone, Syston 2951. 799-112

W. HAROLD PERRY, LTD.

STATION BRIDGE,  
WEALDSTONE, MIDDLESEX.

**1943** SCAMMELL 8-wheel rigid, 6LW engine, 40 by 8 tyre equipment, good condition, £575.  
**1958** (First registered) DENNIS Max, fitted booster box, well shod, mechanically sound, £350.  
**1950** E.R.F. 6-ton, fitted 4LK engine, mechanically sound, £500.  
**1949** LEYLAND Comet, diesel engine, tipper, 8-cu.-yd. metal body, £575.  
**1955** BEDFORD 7-cu.-yd. tipper, metal body, one owner, good condition, £700.  
**1952** Thames 5-ton ET7D boxvan, metal body with full roller shutter at rear, £385.  
LARGE selection of 5-, 10- and 15-cwt. Thames vans always available.  
MOST of the above are guaranteed for 90 days.

HARROW 1031.

OPEN UNTIL

7 P.M. MONDAYS TO FRIDAYS.

AND TO

5.30 P.M. SATURDAYS. 799-315

OFFERS ARE INVITED FOR THE PURCHASE OF:—

THREE 1947 2-3 TON BEDFORD TRUCKS

WITH 3-WAY TIPPING BODIES.

ONE 1947 15-CWT.

MORRIS COMMERCIAL GOODS VAN.

FOR FURTHER PARTICULARS APPLY:—COUNTY SURVEYOR, (DEPT. A), COUNTY HALL, LEWES. 799-50

DOE BROS. (MOTORS), LTD.

MAIN FORD DEALERS.

1 SPITAL ROAD, MALDON, ESSEX.

Phone, Maldon (Essex) 535-6.

**1955** 7-ton COMMER TS3 diesel tipper, 6-cu.-yd. telescopic metal body, very clean vehicle, one owner, mechanical record available.  
**1950** BEDFORD Scammell artic. unit, converted to diesel P6.  
**1956** 18-ft. trailer with side boards, good unit.  
**1952** MORRIS 5-ton long-wheelbase diesel truck.  
**1952** BEDFORD 5-ton long-wheelbase truck, petrol, one C-licence owner, well tired, clean vehicle.  
**1952** BEDFORD 3-ton long-wheelbase truck, petrol, one C-licence owner, good clean truck. 799-4

H. TAYLOR AND CO., LTD.

**1957** February, AUSTIN 1-ton van, black, one owner, £497.  
**1956** February, AUSTIN A40 van, blue, good tyres, £341.  
**1957** COMMER Cob, blue-red, one owner, wing mirror, £376.  
**1955** MORRIS Minor 1-ton van, grey, extra passenger seat, £206.  
**1954** November, MORRIS J-type van, good order, two extra seats, £252.  
**1956** COMMER Cob, blue, £326.  
**1953** BEDFORD 10-12-cwt. CAV, blue, not sign-written, heater, two seats, £245.  
**1-2** THE CRESCENT, Surbiton, Elmbridge. 799-172

February 20, 1959—THE COMMERCIAL MOTOR 47  
(Supplement)

## Used Goods Vehicles (contd.)

### RUFFORD MOTOR CO., LTD.

OFFER FOR IMMEDIATE DELIVERY.

NEW E.R.F. 8-wheel 21-ft. alloy hydraulic tipper, latest-type Gardner engine.  
NEW DODGE 3145 Boys extension 6-wheel tipper, 18-ft. body, Leyland engine.  
NEW DODGE 3143 14-ft. 6-in. double-drop-sided hydraulic tipper, Leyland engine.  
**1957** DODGE long-wheelbase hydraulic tipper, P6 engine, very clean.  
**1955** ALBION Chieftain, fitted 16-ft. alloy body and 16-ft. alloy container; van, 3 tons 3 cwt. unladen weight.  
**1956** FORD D 12-ft. hydraulic tipper, high sided, choice of two.  
**1950** E.R.F. long-wheelbase 16-ft. drop-sided truck, 4LK Gardner.  
**1954** DODGE Boys, 20-ft. body, 6-wheel, R6 engine, unladen weight 4 tons 10 cwt.  
**1953** DODGE 7-ton 18-ft. drop-sided truck.  
**1956** DODGE 7-ton short-wheelbase hydraulic tipper.  
**1954** DODGE 6-ton flat, P6 engine.  
**1955** DODGE 7-ton long-wheelbase tipper, R6 engine, 15-ft. alloy body.  
**1952** DODGE 6-ton long-wheelbase hydraulic tipper.  
**1950** DENNIS Pax short-wheelbase hydraulic tipper, new P6 engine, respayed.  
**1954** SEDDON 14-ft. hydraulic tipper, choice of two.  
**1956** SEDDON long-wheelbase hydraulic tipper, very clean, choice of two.  
**1956** BEDFORD long-wheelbase 7-ton hydraulic tipper, 15-ft. drop-sided body.  
**1957** September, BEDFORD 7-ton short-wheelbase, steel-bodied hydraulic tipper, 24,000 miles, petrol engine.  
**1951** ALBION Chieftain 16-ft. flat.  
**1953** FODEN 8-wheel double-drop-sided, 21-ft. wood body, Pilot gear.  
**1946** FODEN 8-wheel 24-ft. flat, Gardner engine.  
**1945** ATKINSON 8-wheel 24-ft. flat, 7.7 engine.  
**1947** A.E.C. 6-wheel 2,500-gal. tanker, with pump.

MOST of the above machines can be supplied with Metropolitan, East Midland or North Western Area special A licence.

CHOICE of 50 other trucks from £50 upwards.

### MILE HILL GARAGE.

CHESTERFIELD ROAD NORTH,

MANFIELD.

Phone 2314-S. 799-80

### LES GLEAVE, LTD.

FOURWAYS GARAGE.

ARCLID, NEAR SANDBACH, CHESHIRE.

Smallwood 225, 226

NEW vehicles for early delivery.

**F**ODEN 8-wheeler, 6LW engine, or new LX.  
**F**ODEN 6- or 4-wheeler with Gardner or Foden engine.  
**C**OMMER 7-ton short-wheelbase and long-wheelbase.  
**B**EDFORD trucks or tippers, all models.  
**1955** FODEN 8-wheeler, double drive, 4-speed box.  
**1948** LEYLAND Octopus, very clean, ready for work.  
**1945** SCAMMELL 8-wheeler, 6LW engine, 24-ft. alloy body.  
**1957** AUSTIN B.M.C. Boys extension, 21-ft. 6-in. flat, small mileage, very clean.  
**1952** FODEN 4-wheeler, 4LW engine, 18-ft. 6-in. flat; Atkinson 4-wheeler, 4LW engine, double-body.  
**1947** MAUDSLAY 4-wheeler, sound vehicle, cheap.  
**1951** BEDFORD 5-ton long-wheelbase, Balco extension, 18-ft. 6-in. double-drop-side truck, P6 engine.  
**T**IPPERS.  
**1954** FORD Thames medium-wheelbase wooden-body tipper, P6 engine.  
**D**UE in shortly, several Bedford 5- and 7-ton short-wheelbase diesel tippers; also Dodge and Seddon.

### LES GLEAVE, LTD.

FOURWAYS GARAGE.

ARCLID, NEAR SANDBACH, CHESHIRE.

Smallwood 225, 226. 799-187

### HILLS.

**B**EDFORD 3-4-ton long-wheelbase boxvan, approximate dimensions, 13 ft. 5 in. long, 6 ft. 10 in. wide, 5 ft. 11 in. high, first registered 1946.  
**C**OMMER O4 5-ton long-wheelbase platform lorry, first registered 1951.  
**S**EDDON diesel 6-7-ton long-wheelbase tipper, 14 ft. by 7 ft. with 3 ft. sides, Pilot twin vertical gear, first registered 1948.  
**S**EDDON diesel Mk. 15 71-tonner platform lorry, 18 ft. by 7 ft. 4 in., one owner, first registered August, 1957.  
**T**HORNYCROFT Trident, with separate box van body, length 15 ft. 10 in., width 6 ft. 5 in., height 7 ft. 3 in., unladen weight 4 tons 4 cwt. 58 lb., one owner, first registered 1952.  
**V**ULCAN P6 diesel, long-wheelbase van, approximate interior dimensions, 16 ft. long, 7 ft. wide, 5 ft. 7 in. high, first registered 1949.

### HILLS GARAGES (MANCHESTER), LTD.

80-90 PORT STREET, MANCHESTER, 1.

Central 4111 799-62

R43



**PERRY'S OF SOUTHEND ON SEA**  
OFFER  
A SMALL SELECTION OF OUR  
**USED COMMERCIAL VEHICLES.**

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1956

1955

1954

1953

## TRUCK

1056

1950  
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TIRRELL

**DIFFER**

1953

## 1956

£900.  
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20-ft. platform, choice two.  
**1957 LEYLAND Octopus (9.8-litre) 8-wheel double-**  
**drive 24-ft. platform**, 9,000 by 20.  
**1956 SEDDON (Perkins P6) 6-7-ton 4-wheelers**,  
Milshaw tippers and platform, choice two.  
**1955 LEYLAND twin steer (9.8-litre) 6-wheel**  
**Milshaw twin-rum alloy tipper**, 20-ft. body.  
**1955 BEDFORD A-type (Perkins P6 diesel) 4-wheel**  
**tractor, S.A.E. fifth-wheel coupler**.  
**1954 ALBION HD7L 9.6-litre 8-wheeler**, Northern  
special A, 7½ tons, expiry October, 1960.  
**1954 ALBION Clydesdale diesel, 8-9-ton 4-wheeler**,  
Northern special A, 4½ tons.  
**1954 ALBION FT37L Chieftain diesel 6-7-ton**  
**4-wheeler**, Homalloy cab and body.  
**1954 BEDFORD S-type (Perkins R6 diesel) 7-ton**  
**4-wheeler 16-ft. platform**, exceptional condition.  
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24-ft. platform, 9,000 by 20.  
**1951 50 ALBION HD7L (9.6 diesel) 15-ton 8-wheel**  
**25-ft. platform**, choice of two, 9,000 by 20.  
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**8-wheeler 24-ft. platform**, 40 by 8, choice two.  
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Milshaw tipper, 22-ft. body, 40 by 8.  
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**Milshaw tipper**, 13-ft. body.  
**1946**

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**1951 FODEN 8-wheeler, 6LW engine.**  
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mechanical order, £145.  
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original tyres with 30% tread, a first rate vehicle ex C  
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**FORD Thames 4D medium-wheelbase 4-cu.-yd. tipper**,  
Teleshot underbody gear, body as new and vehicle  
completed thorough overhaul, tyres good all round, first  
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**1956 ALBION Reiver**, 24-ft. alloy-framed body,  
hardwood floor.  
**1956 GUY 8-wheel tipper**, 22-ft. alloy U-shaped  
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**1956 LEYLAND Octopus 600**, 24-ft. 6-in. body.  
**1955** December, LEYLAND Comet 90 tipper, alloy  
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refrigerator.  
**1955 THORNYCROFT Trident tipper**, 12-ft. alloy  
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body.  
**1953 ALBION Chieftain tipper**  
**1952 ALBION HD. 18-ft. body**.  
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in timber.  
**1951 DODGE 5-ton, petrol engine**, 16-ft. body with  
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**1951 Thornycroft, Albion and E.R.F. 8-wheelers**,  
24-ft. 6-in. flat, sound condition, very reason-  
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**1955 3-ton DODGE long-wheelbase truck**, P4 diesel,  
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**1957 BEDFORD van**, low mileage, £355.  
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body, integral cab with drop well, under 3 tons,  
choice of two, from £425.  
**1951 BEDFORD 5-ton**, 1,000-cu.-ft. pantechonicon  
with drop well, plymax body, in first-class  
order, £325.  
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body, integral cab with drop well, under 3 tons,  
£400.  
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with drop well, under 3 tons, in really first-  
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Weston tipping gear, 5-yd. standard body,  
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4-cu.-yd. steel body, excellent condition, £350.  
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**1952 VULCAN with 7-8-tonner platform body**, Mark  
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Gardner 4LW, tyre equipment very good.  
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**8-wheeler**, 9.6 engine, air brakes, 900 by 20  
12-ply tyre equipment, fitted 24-ft. 6-in. Homalloy flat-  
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**1945** SCAMMELL rigid 8-wheeler, 24-ft. 6-in. platform body, 40 by 8 tyres rear and 36 by 8 front.

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**1954** DODGE 7-ton diesel long-wheelbase tipper.  
**1952** DODGE diesel tractor unit with Scammell coupling.  
**1955** FORDSON 3-ton 4D diesel tipper.  
**1955** FORDSON 4-ton 4D diesel tipper.  
**1956** FORDSON 4-ton 4D diesel tipper.  
**H.P.** And insurance facilities available.  
**WINGERWORTH SERVICE STATION, LTD.,** Derby Rd., Wingerworth, Chesterfield, Derbyshire. Chesterfield 3527 and 7833 799-36

**BIRMINGHAM COMMERCIAL OFFER:—**  
**1952** BEDFORD 5-ton Luton, J1,500 c.c., P6.  
**1954** BEDFORD 4-ton Luton, 950 c.c., petrol.  
**1955** BEDFORD 7-ton long-wheelbase S.A.  
**1955** BEDFORD 7-ton long-wheelbase tipper.  
**1952** FODEN 6-wheel tipper, 6LW.  
**1950** LEYLAND 6-wheel Comet.  
**1952** SENTINEL 6-wheel tipper.  
**TERMS AND EXCHANGES.**  
**560 COVENTRY ROAD,**  
BIRMINGHAM, 10.  
Victoria 0437-8. 799-446

846

**Used Goods Vehicles (contd.)**

**BEECH'S GARAGE (HANLEY), L. TD.,**  
DISTRIBUTORS FOR E.R.F., SEDDON, GUY.

- NEW** E.R.F. 8-wheeler, 6LW, double drive, 9.00 by 20 tyres, chassis and cab, bodywork to requirements.  
**A.E.C.** Matador 4 x 4 fixed-side lorries, ex-W.D., power-operated winch, 13.50 by 20 tyres, in excellent condition, choice of two.  
**1953** SEDDON tractor, Perkins P6 engine, 8.25 by 20 tyres, S.A.E. coupling, in excellent condition.  
**1957** COMMER TS3, diesel, fitted Boys 6-wheel conversion, 22-ft. drop-sided body, in first-class condition.  
**1953** BEDFORD 7-tonner, petrol, twin-ram end hydraulic tipper, 14-ft. 6-in. drop-sided body, good condition.  
**1955** E.R.F. 4.4G, 4LW Gardner engine, 21-ft. drop-sided body 9.00 by 20 tyres, in very good condition.  
**1956** A.E.C. Mercury, 21-ft. alloy platform body, 9.00 by 20 tyres, in very clean condition.  
**1951** E.R.F. 4-wheeler, 5-cylinder engine, 18-ft. drop-sided body, air pressure brakes, 9.00 by 20 tyres, in good condition.  
**1957** COMMER TS3 7-ton tipper, short wheelbase, 9.00 by 20 tyres, 2 speed axle, in excellent condition.  
**1952** FORD ET6 short-wheelbase, Anthony hoist, steel-bodied tipper, on 7.50 by 20 heavy-duty tyres, P6 engine in very good condition.  
**FODEN** 5-cylinder, timber tractor, power winch, anchor and crane jib, in excellent condition.  
**THORNYCROFT-COLES** 6-wheeler, 5-ton wire jib, diesel engine, in first-class condition.

**BEECH'S GARAGE (HANLEY), L. TD.,**  
HOPE STREET, HANLEY, STOKE-ON-TRENT.  
S-on-T. 25249 and 25240. 799-439

**HALE MOTORS (TOTTENHAM), L. TD.**  
THE HALE, N.17.  
Tottenham 7771 (four lines).

- VANS.**  
**1956** AUSTIN 152 Omnivan, no lettering, 8,000 miles only, showroom condition.  
**1956** MORRIS 1-ton van, low mileage.  
**1950** BEDFORD 30-cwt. van.  
**1953** COMMER Superiole 2-ton boxvan, with side loading door.  
**TIPPERS.**  
**1938** E.R.F. 8-cu.-yd. hydraulic tipper, Gardner 4LW diesel engine.  
**NEW** vehicles.  
**AUSTIN**, Ford, Rootes Group.  
**COMPREHENSIVE** stocks always held. 799-448

- THE RELIANCE GARAGE CO. (BRIGHOUSE), L. TD.**  
**1955** Wakerley 8.4, Brighouse offer—  
**1954** ALBION Chieftains, choice of three, long-wheelbase, 18-ft. bodies.  
**1951** ALBION Chieftain, long-wheelbase, 17-ft. 6-in. body.  
**1950** ALBION Chieftain, reconditioned 16-ft. body.  
**1950** ALBION Chieftain, long-wheelbase, 17-ft. 6-in. body.  
**1953** FODEN 8-wheeler, beautiful condition.  
**1954** E.R.F. 8-wheeler, beautiful condition.  
**1953** MORRIS 5-ton, diesel, long wheelbase, very clean.  
**1951** MORRIS, petrol, long wheelbase, very clean.  
**1948** MORRIS 2-ton commercial, nice condition.  
**1950** BEDFORD 5-ton long wheelbase.  
**1951** BEDFORD 5-ton, long-wheelbase.  
**1947** BEDFORD 4-ton, long wheelbase.  
**1946** BEDFORD short-wheelbase tipper.  
**1947** SEDDON P6, long wheelbase.  
**1950** SEDDON P6, long wheelbase, choice of two.  
**1948** THORNYCROFT D.I. Star, long wheelbase.  
**1951** THORNYCROFT Sturdy, long wheelbase.  
**1949** AUSTIN Loadstar 3-ton, long wheelbase.  
**1951** NEW Albion and Leyland in stock.  
**NEW** 7-ton BEDFORD long-wheelbase tippers in stock. Immediate delivery.  
**H.P.** Terms to suit purchaser. Phone, Brighouse 1677-8-9. 799-473

**E. RAYMENT, L. TD.,**  
DODGE DISTRIBUTORS FOR EAST LONDON.

- DODGE** 124 AP6 diesel tractor unit, November, 1956, low mileage, with 23-ft. 10-12-ton Scammell trailer.  
**DODGE** 224 AP5 diesel, December, 1957, fitted light alloy platform.  
**DODGE** 103 P6 diesel, August, 1954, hydraulic tipper.  
**DODGE** 103 petrol, December, 1956, hydraulic tipper.  
**BEDFORD** O model, petrol Scammell tractor unit, very good condition.  
**BEDFORD** S model, petrol, Scammell tractor unit, very good condition.  
**BEDFORD** O model, petrol, 5-ton drop-side truck, very good condition.  
**BEDFORD** O model, diesel tractor, fitted 20-ft. Scammell trailer.  
**FORD** 4D platform, 1956, H.D. equipment, 5-ton.

334-340 ROMFORD RD., London, E.7. Maryland 4772-3-4. 799-378

**Used Goods Vehicles (contd.)**

**G. S. OSCROFT AND CO., L. TD.,**  
MAIN BEDFORD DEALERS.

- 1955** DODGE 145 AR6 long-wheelbase platform truck, in excellent condition, diesel engine, good appearance.  
**1955** DODGE long-wheelbase 5-ton drop-sided tipper, twin front-end tipping gear, Perkins P6 engine, in excellent condition.  
**1954** GUY Otter 7-ton, Gardner 4LW engine, alloy platform, 2-speed rear axle, in fair condition.  
**1952** COMMER 7-ton OX medium-wheelbase tipper, underfloor tipping gear, new engine fitted 3,000 miles, 3-speed axle, in exceptional condition throughout.  
**1951** MORRIS-SAURER diesel short-wheelbase tipper, £135.  
**1950** ALBION CX5 6-wheeler tipper, double drive, twin-ram Pilot tipping gear and Pilot body, new engine fitted 18 months, in excellent condition.  
**1950** MAUDSLAY Mustang twin-steer 21-ft. platform truck, new cab just been fitted, in excellent condition.  
**1950** COMMER 5-ton long-wheelbase tipper, under-floor engine, twin-ram tipping gear, in fair condition, £195.

**VANS.**  
**1954** BEDFORD Dormobile, resprayed in primer, dual-purpose vehicle with folding seats to rear.  
**1957** BEDFORD 10-12-cwt. van in immaculate condition.  
**1953** BEDFORD 10-12-cwt. van, resprayed in primer, in good condition.

**G. S. OSCROFT AND CO., L. TD.,**  
DERWENT STREET,  
DERBY.  
Phone, Derby 40171. 799-481

**JACKSONS**  
FOR  
**MODERN SELECTED VEHICLES,**

- BEDFORD** 1956 model 5-ton short-wheelbase tipper, clean and sound.  
**BEDFORD** 1953 long-wheelbase chassis and cab, with Balco extension, repainted and in very good order.  
**BEDFORD** 1952 2-ton streamlined boxvan excellent condition.  
**LEYLAND** Comet 1955 ECO model, semi-forward control, long-wheelbase 18-ft. alloy body, one owner since new.  
**BEDFORD** 1951 5-ton long-wheelbase drop-side truck, ready for work.  
**FORDSON** 1953 medium-wheelbase petrol truck, repainted and in very nice condition.

**O. T. JACKSON MOTORS, L. TD.,**  
855 NEW WOLVERHAMPTON ROAD,  
LANGLEY, NEAR BIRMINGHAM.  
Phone, Birmingham Broadwe 1 2871-2-3. 799-190

**RLHC** BEDFORD, petrol, 4 x 4, painted cream, twin rear, alternative bodies, Bode line spreader, Bedford platform, excellent condition. Keely, Wigmore, Leominster, Herefordshire. Phone, Wigmore 284. 800-14890

- ATKINSON**, 1946, 6W tipper, 7.7 engine, very good, £900.  
**ATKINSON**, 1943, 6-wheel drop-sided truck, 23-ft. body, 5LW engine, very clean.  
**DODGE-SCAMMELL** 10-ton, 1956, P6 engine, 2-speed axle, 24-ft. trailer, £850.  
**W. H. SHORT, LTD.**, Newthorpe, Notts, Kimberley 799-108  
**ONE** 1947 VULCAN, fitted P6 engine, platform body (ex brewery), diesel.  
**ONE** 1946 THORNYCROFT Nippy, platform body (ex brewery), petrol, new tyres.  
**ONE** 1947 BEDFORD platform, petrol (ex brewery), new tyres.  
**ONE** 1954 COMMER articulated, new tyres, ex C licence.  
**ONE** 1948 MAUDSLAY, A.E.C. 7.7 engine, flat platform (ex C licence).  
**ONE** 1956 B.M.C. 5-6-ton, diesel, drop-side body, ex C licence.  
**ONE** 1940 MAUDSLAY Merlin, 4LK Gardner engine, drop-side body.  
**PHILLIPS MOTOR SERVICES (SHEFFIELD), LTD.**, 443 Handsworth Rd., Sheffield, 13. Woodhouse 2541. 799-48

- AUSTIN** Loadstar, 1953 artic., Perkins P6, fitted 22-ft. Handflat trailer, good all-round condition, £465.  
**BEDFORD** 1949 5-ton drop-side, Perkins P6, good running order, £135.  
**BEDFORD** 1947 2-1/2-ton Luton van, fitted drop well, in good order throughout, £125.  
**COMMER** 1953 25-cwt van, excellent condition, £165.

**MARSHALLS (CAMBRIDGE), LTD.,** Airport Garage, Cambridge. Phone 56291. 799-403

**L.A. MITCHELL (MOTORS), L. TD.,**  
DODGE DISTRIBUTORS,  
PERKINS DIESEL SIGNHOLDERS.

- NEW** MORRIS B.M.C. 7-ton diesel short-wheelbase chassis and cab, £1,408 11s. 8d.  
**MORRIS** 1-ton van, £345.  
**1957** September, DODGE 106 CP6 6-ton 16-ft. truck with tilt and fitted Perkins exchange diesel engine, fully guaranteed, £850.  
**1955** 6-ton DODGE diesel tipper, £575.  
**1955** 6-ton DODGE diesel truck; choice of two, £585.  
**1953** BEDFORD 5-ton short-wheelbase tipper, £265.  
**1951** AUSTIN A40 10-cwt. van, £100.  
**1950** 53 6-ton DODGE tippers, choice of four from £150.  
**BEDFORD** Q.L. 4 by 4, with Anthony steel body and tipping gear, very good condition, £165.  
**1** BALHAM HIGH RD., S.W.12, Phone, Bal 2234. 799-117

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**Used Goods V**

**COM**  
**1953** BEDFORD new, £220.  
**1955** BEDFORD order, £220.  
**1952** BEDFORD £195, ch.  
**1954** BEDFORD  
**1954** BEDFORD condition  
**1957** BEDFORD greenfor  
**1956** BEDFORD new, £5  
**1956** BEDFORD new, £5  
**1951** nice ord  
**1948** KARRI  
**UNREGISTERE**  
**lift breakdown**  
**extra for the purpo**  
**ALL these vehic**  
**HIRE-PURCHA**  
**17** HEATH ST  
**PHON**  
**WELCH'S**  
**BEDFOR**  
**IM**  
**BEDFORD** 10-1  
**BEDFORD** 7-10  
**VERY special**  
**21 cc. engine**  
**1956** FORD  
**1953** BEDF  
**1959** BEDF  
**FODEN** 8-whe  
**E.R.F.** 6-w  
**LONDON**  
**PHO**  
**LARGE select**  
**vehicles from**  
**L. H. SPRING**  
**price 7667.**  
**SPURLING M**  
**Corner, W.4**  
**AUST**  
**1955** BED  
**1951** BED  
**1948** 5-ton  
**10** Dyson 4-  
**ONE** Pole tra  
**FUEL** tanks,  
**1947** Perki  
**1947** SED  
**1952** BED  
**ERNEST** B  
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**1956** COM  
**ERRINGTON**  
**VIGO** MOT  
**1947** BED  
**1952** BED  
**VIGO** MOT  
**PETER**  
**NEW** DODG  
**NEW** DODG  
**NEW** DODG  
**NEW** GUY  
**1956** DO  
**1954** DO  
**1955** DO  
**1956** DO  
**1943** 6-  
**1954** DO

COMMERCIAL VEHICLES.  
WM RIPLEY.

- 1953 BEDFORD 30-cwt. O-type drop-side truck, like new, £225.  
 1953 BEDFORD 30-cwt. 3-way van, in first-class order, £325.  
 1952 BEDFORD 2-ton drop-side truck, nice order, £195; choice of four.  
 1952 BEDFORD 3-ton drop-side truck, £375.  
 1954 BEDFORD 3-ton drop-side truck, exceptional condition, £425.  
 1954 BEDFORD 3-ton diesel drop-side truck, with green grocer's ladders, £615.  
 1957 BEDFORD 4-ton drop-side truck, ex C-licence user, £485.  
 1956 BEDFORD 5-ton double-drop-side truck, as new, £550.  
 1951 AUSTIN Loadstar drop-side truck, exceptionally nice order, £225.  
 1948 KARRIER Bantam drop-side truck, new tyres, indicators, first-class order, £95.  
 UNREGISTERED BEDFORD 5-ton crane with double-lift breakdown ambulance, and every conceivable arm for the purpose, recently highly painted and chromed.  
 ALL these vehicles carry our guarantee.  
 HIRE-PURCHASE, part-exchanges welcomed.  
 HEATH ST., Dartford, Kent.  
 PHONE, DARTFORD 5480. 799-375

WELCH'S GARAGE (STAPLEFORD), LTD.  
BEDFORD AND ROVER DEALERS.  
IMMEDIATE DELIVERY.

- BEDFORD 10-ton 300 diesel tractor, Scammell coupling.  
 BEDFORD 7-ton normal control, 300 diesel, drop-sided truck.  
 VERY special offer. New unregistered 88-in. standard 21 cwt. engine LAND ROVER, £100 below list.

## USED VEHICLES.

- 1956 FORD 4D drop-sided truck, small mileage, as new, just fitted set of new 12-ply tyres.  
 1953 BEDFORD A type, P6, tyres as new, good runner.  
 1959 BEDFORD C.A. van, under 1,000 miles, still in primer.  
 FODEN 8-wheeler, must be cleared.  
 6-wheeler, must be cleared.

E.R.F.  
LONDON ROAD, STAPLEFORD, CAMBS.  
PHONE, SHELFORD 3017-8-9. 799-214

- LARGE selection of new and used light commercial vehicles from £100.  
 L. H. SPRING, 108 Alexandra Park Rd., N.10. Enter-prise 7667. 799-320

## SPURLING MOTORS (CHISWICK), LTD., Spurling

- Corner, W.4, Phone, Chl 6741 (five lines).  
 1955 AUSTIN A40 van with windows.  
 1951 BEDFORD-SCAMMELL tractor.  
 1948 5-ton forward-control MORRIS diesel truck. 799-315

- 10 Dyson 4-wheel trailers, £50-£70 each.  
 ONE Pole trailer, 35 ft. long, £80.  
 FUEL tanks, capacity 1,600 gal., £25, as new.

- 1947 Perkins P6, PDC coupling, £150.  
 1947 SEDDON Perkins P6, £140.  
 1952 BEDFORD long-wheelbase flat, petrol, 7-tonner, £175.

- 1952 THORPE AND CO., LTD., Thurgoland, near Sheffield. Phone, Stocksbridge 2152. 799-189

- 1956 COMMER TS3 7-ton long-wheelbase drop-side truck, one owner.  
 ERRINGTONS, Evington, Leicester. Phone 38102-3. 799-289

- VIGO MOTORS.  
 1947 8-cwt. BEDFORD van, £95.  
 1952 BEDFORD 4-ton Luton van, taxed, £285.  
 VIGO MOTORS, Walmer Rd., W.10. Ladbroke 3051. 799-357

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FERRARIS OF CRICKLEWOOD, LTD.  
200-220 CRICKLEWOOD BROADWAY,  
N.W.2.  
Gladstone 2234-5-6-7.

## AUSTIN.

- 1950 AUSTIN A40 van, choice of several.

- 1953 AUSTIN 25-cwt. van; also one other.

## BEDFORD.

- 1956 BEDFORD 10-12-cwt. van; also truck version.

## DODGE.

- 1956 DODGE 5-ton 106A diesel truck, choice of two.

- 1953 DODGE 5-ton short-wheelbase diesel tipper.

## FORDSON.

- 1955 FORDSON 5-cwt. van; choice of several.

- 1956 FORDSON 7-cwt. van; choice of several.

- 1954 FORDSON 10-cwt. gown van; choice of several.

## MORRIS.

- 1957 15-cwt. J2 van, choice of two. 799-340

## R. A. JORDAN, LTD.

MAIN DENNIS DISTRIBUTORS  
(NO CONNECTION WITH ANY OTHER FIRM),  
FOR GOOD USED VEHICLES.

- 1949 AUSTIN 25-cwt. 3-way van, good condition, one owner.

- 1952 DENNIS Max. long wheelbase, 5-speed, 18-ft. platform, one 5-speed box.

- 1952 December, FORD Thames 4D, platform, excellent tyres, nice condition.

- 1949 BEDFORD 5-ton, very sound condition, drop-side body.

## IVEL WORKS,

## BIGGSLWADE.

- Phone 2265. 790-410

- £450. 1955 BEDFORD A-type, long wheelbase, double-drop-sided.

- £170. 1954 BEDFORD 7-ton long-wheelbase platform, petrol.

- £145. 1951 FORD articulated tractor, P6 engine, S.A.E. coupling.

- £140. 1951 BEDFORD 7-ton long-wheelbase platform, petrol.

- £100. 1946 VULCAN long-wheelbase flat platform, P6 engine.

- £150. 1943 E.R.F. long-wheelbase platform, 7.7 engine, 5-speed box.

- £250. 1942 ATKINSON long-wheelbase platform, 4LW engine, 5-speed box.

- £200. 1942 LEYLAND TSC18 long-wheelbase platform, form.

- ENGINES—P6, Gardner 4LK, 4LW, 5LW, A.E.C. 7.7.

- J. W. RATCLIFFE AND SONS, LTD., Low Bank Garage, Ashton-in-Makerfield, Lancs. Phone 7497. 799-264

- BARNARDS of Stowmarket, offer:—

- ALBION, 1953, 8-ton 18-ft. drop-side body, 6-cylinder petrol engine, 8.25 hp, 21 tyres, £425.

- A.E.C., Diesel 4-wheel drive, complete with 40 by 8 wheels, £595.

- BEDFORD-SCAMMELL, 1947, 8-ton petrol-engined tractor unit with 20-ft. trailer, £140.

- BEDFORD-SCAMMELL, 1951, 10-ton S-type petrol-engined tractor unit with 23-ft. 10-ton trailer, £665.

- BEDFORD, unregistered 900-gal. tanker, almost unused, £200.

- BEDFORD tractor unit, petrol-engined, with Carrimore coupling and 8-ton trailer, 23 ft., and Telechoist sack loader, £350.

- DODGE, 1952, 5-ton long-wheelbase diesel tipper, £475.

- VULCAN, fitted with P6 diesel engine, 16-ft. 6-in. drop-side body, choice of three, from £325.

- ALL the above trucks are in very good condition and ready for work.

- PHONE, Stowmarket (Suffolk) 621 (five lines). 799-203

- DECEMBER, 1957, MORRIS-COMMERCIAL 30-cwt. truck, Clarendon grey, heater, low mileage, £465.

- 1958 MORRIS JBI 10-cwt. van in primer, low mileage, o.h.v., £465.

- 1954 December, FORD 10-cwt. van, dark blue, new battery, clean van, £175.

- DICKINSON AND ADAMS (LUTON), LTD., Leagrave Rd., Luton. Phone 51221. 799-165

## PERCY D. SLEEMAN, LTD.

LONDON COMMER DEALERS.

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- £250. 1950 FODEN 6-ton lorry, 4LK engine.

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**1950** SEDDON diesel Mk. 5L tractor unit with Carrimore coupling.

**1950** MAUDSLAY drop-sider, fitted with 7.7 A.E.C. engine.

**1950** SEDDON diesel Mk. 5L articulated tractor unit, complete with semi-trailer, bulk grain carriers, gravity discharge, the complete vehicle in excellent condition throughout.

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**1951** BEDFORD 5-ton short-wheelbase tipper.

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**1957** B.M.C. articulated York 1,550-cu.-ft. van, Metropolitian, South Western and North Western areas.

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**A.E.C.** 1948, 9.6, pre-selector, 33-seater Plaxton body, heater, certificate of fitness April 1961, in very good condition throughout. £400. Green (Coaches), Ltd., 213a Hogg St., London, E.17. Phone, Coppermill 1138. 800-x4888

**1947** Plaxton 35 seat, new body 1953, heater, certified May 1963, excellent condition, £1,400. 56-seat Salmesbury, new body 1950, three years' certificate, choice of three, £325.

**B.E.L.** ROADWAYS, West Hartlepool. Phone 4687. 799-91

**1952** Mark IV, 41-seater Yeates, superior vehicle in excellent condition, certificate of fitness 1962. Apply Richmond 37 South St., Epsom. Phone 532. 799-x5035

**1934** A.E.C. 7.7 33-seater, rebuilt 1948, certificate of fitness September 1960, very clean condition £240 or nearest offer. Phone, Corby Glen, Gantham, 285. 799-x5040

**A.E.C.** Regent 9.6 RT double-decker, fluid transmission, air brakes, automatic chassis lubrication, good order, certified 1961, bargain at £450. Smiths, March, Cambs. Phone, March 3234. 799-x4921

#### ALBION

**FOUR** 1948 double-deck omnibuses, all-metal bodies, certificate of fitness, £200 each.

**SIX** 1947 single-deck omnibuses, certificate of fitness, £108.

**H. GROVES AND SON, LTD.**, Windsor Avenue, G. London, S.W.19. Liberty 3476. 799-161

#### BEDFORD

**1951** 50-49-48 Bedford Duple Vista 29-seater luxury coaches, choice of several all in excellent condition, with good certificates of fitness, prices from £288. Chiltonian Motors, Hungerford, Berks. Phone, Hungerford 89. 799-1145

**1956** Bedford Burlingham 36-seater, radio and heaters, exterior maroon and cream, interior red, excellent condition, certificate of fitness to 18.7.61, £2,500 o.n.o. West's Coaches, 107 Walpole Rd., London, E.18. Phone, Buckhurst 2429. 799-1145

**1948** Bedford O.B. chassis with Kenex 27-seater body, grey coachwork with blue moquette high-back seats, excellent condition throughout, total mileage 75,000, certificate of fitness March 1962, now surplus to requirements. First offer of £250 secures. Ruga, Station Garage, Lewes, Sussex. Phone 1467. 800-7148

**1947** Bedford 28-seat, in very good condition, 38,000 since new, new engine 8,000 miles, one owner, used privately since new, £150. Phone, Cobham, Surrey, 3702. 799-162

**1949** Bedford Vista, 29 seats, exterior green, cream flash, certificate of fitness 30.9.62, five new remould covers, engine and body perfect, low mileage, used for seasonal work only, licensed year, £625. Chatteris Garage, Ltd., Chatteris, Cambs. Phone 138. 799-198

**£450** Only 1950 Bedford 29-seater luxury coach, high-back seats, etc., clean and good, certificate of fitness 1960. Terms and exchange. Wilde and Bonnet, Ltd., 75 Station Rd., Huddersfield, Manchester. Phone, Gloop 2902-3; after hours 2356. 799-192

**1955** Bedford 38 Duple, £2,100. Apply Camping Coaches, Brighton 25493. 799-99

**1954** Bedford Duple Vega, 38 seats, reconditioned modified Perkins R6 engine, heaters, amber lift-up roof vents, autumn tint interior, Formica side panels, cream exterior, vacuum-operated entrance door, certificate of fitness 1964, £2,150. Hire-purchase terms. Moreton's Motors (Ford Dealers), 166-16 Plumstead Common Rd., S.E.18, Woolwich 0313 (four lines). 801-7196

**1952**, July, Bedford 30-cwt. chassis, fitted with used aerodrome for transporting pilots to planes, only been owner, in excellent condition throughout, side £1,100, tax only £12 10s. a year, choice of two, £325 each.

**1950**, December, Bedford coach, as above, the choice of two, £175 each.

**1949** Bedford coach, as above, the choice of two, £150 each. H.P. terms arranged on above vehicles. Iver 947. 799-405

**Bedford** 38-seater, 1952, one owner, certificate of fitness 1962, £1,400 o.n.o. 442 Chessington Rd., W. Ewell, Surrey. Ewell 7176. 799-x4922

#### V. COLEMAN

166 MAIN ROAD,  
SUNDRIDGE, KENT.  
Bristled 291.

**1954** Bedford Yeates Riviera 36-seater, alias quarters, lift-up roof vents, heater, etc., in immaculate condition, one owner, certificate of fitness 1961.

**1953** Bedford Plaxton Ventura 35-seater, radio and heaters, lift-up roof vents, every extra, condition as new, one owner, certificate of fitness 1964.

**1951** 3 Bedford Duple Vegas, 33- and 35-seaters, choice of three, all one owner.

**1950** 47 Bedford Duple Vistas, 29-seaters, many high-back seat conversions, all with current certificate of fitness, choice of six.

**1946** Bedford Duple Mk. II 30-seater, one owner, certificate of fitness 1960.

**1957** Bedford 12-seater, Kenex Aristocrat, sliding roof, heater, racks, etc., immaculate condition, one owner.

**SET** of 33 full luxury seats, Dunlopillo, blue moquette. 799-329

**1954** Bedford Duple Super Vega, 36-seater, heater, radio, microphone, excellent condition, certificate of fitness 1963, £2,000. Elgar 3992. 799-300

### Used Passenger Vehicles (contd.)

**1954** BEDFORD 36-seater, radio, heater, microphone, certificate of fitness 1959, £1,750. Elgar 3992. 799-301

**BEDFORD** full-front Plaxton 30-seaters, certificates of fitness 1959 to 1962, from £475. Boughtons, 1-5 Odessa Rd., E.7. Mar 1888. 799-355

**1958** BEDFORD 41 Plaxton luxury coach, heaters, demisters, radio and microphone, roof lights, floor drilled 37-41 seats, immaculate, £3,200; available after Whitsun or by arrangement. Bristow's Coaches, Ltd., Claxby, Lincoln. Phone, Owersby Moor 224. 799-x4919

**1953** 39-seater BEDFORD Super Vega, certificate of fitness 1963, fitted with heater and radio, immaculate condition inside and out. Thomas Motor Tours, Ltd., Newark 550. 801-x5024

**1952** BEDFORD Super Vega 8-ft. 33-seater, radio, heater, £1,250. Boddy. Phone 265 Camblesforth, near Selby. 799-x5022

#### Bedford Wanted

**BEDFORD** service bus wanted. Hastelow's Coaches, New St., Ladbury 326. 799-7159

**1954** Or 1955 BEDFORD Duple, Burlingham or Plaxton 36-seater, blue interior and blue exterior preferred. Interested in purchasing direct from small operator. C. F. Wood and Son, Ltd., Steyning. 799-206

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**COMMER** Avenger (petrol) 34-seater Churchill body, luxury coach, first reg. July 5, 1950, one owner, colour red-cream, maintained as new with total mileage under 100,000, certificate of fitness 1960, price £750. Red Garages, Llandudno. Phone 7461. 799-79

**HILLS** 1957 COMMER TS3 41-seater Duple, heaters, blue exterior, autumn tint moquette, Formica side panels, certificate of fitness 1964, immaculate, choice of three. 802-7143

**HILLS GARAGES**, Port St., Manchester. Central 4311. 799-61

#### DENNIS

**1949** DENNIS 33 seaters, full luxury, choice of 1949 two, certificates of fitness August 1961. Part-exchanges and terms arranged. Hastelow's Coaches, New St., Ladbury 326. 799-7158

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**DOUBLE-DECKER** buses with Gardner 5LW engines, complete; also A.E.C. 7.7 engines, £175.

**BEAUMONT GARAGE**, 330 Brixton Rd., S.W.9. Bri 5678. 799-7177

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**1950** Registered, FODEN 39-seater, Bellhouse Hartwell body, diesel 2-stroke. Offers. Harrow 6393-4. 799-133

#### GUY

**1945**, July, GUY 55-seater double-decker bus, low bridge, Dunlopillo seating, Metcam body, good battery and tyres, just out of service, £175 or near offer. Betchley, Rising Sun, Upper Beeding, Sussex. 800-x988

#### LEYLAND

**1948** LEYLAND P51, Harrington Dorsal Fin body, certificate of fitness November 1962, price £500.

**1949** LEYLAND P51, Strachan body, price £400.

**1939** LEYLAND TS8, fitted with P51 engine and gearbox, 1949 Harrington body, price £300.

**REGENCY MOTORWAYS (REDDITCH), LTD.**, Alcester St., Redditch, Worcs. Phone 321. 799-93

**1950** P51 35 Duple, unusual condition. Grosvenor Coaches, Enfield 1089. 799-460

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**1954** SENTINEL 41-seater Whitson, excellent condition, used only for private hire, genuine 56,000 miles, heater, radio, exchange for 41-seater Bedford and 29-seater. Terms arranged. Hastelow's Coaches, New St., Ladbury 326. 799-7157

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**1955** Diesel TROJAN personnel carrier, one owner, taxed, £275. Langley Mill 3182, home 3625. 799-117

**KIRKSTALL** 7½- and 5-ton diffs., Moss, Commers. Fords, Maudslays, Austin Langley Mill 3182, Home 3625. 799-116

#### VOLKSWAGEN

**VOLKSWAGEN** Microbus de Luxe, £495. Worthington Motor Tours, Ltd., 61 Hurst St., Birmingham, 5. Midland 4966. 799-7169

#### UNCLASSIFIED

**1950** ALBION Victor, diesel, 31 seats, Reading body, certificate of fitness to December 1959.

**1951** AUSTIN, 32 seats, Strachan body (full front), certificate of fitness to April 1961.

**1949** COMMER, 30 seats, Strachan body, certificate of fitness to May 1959.

**ALL** the above are in excellent condition. For further particulars apply Jack Wingrove, Ltd., 85 Penn Rd., Hazlemere, High Wycombe, Bucks. Phone, Penn 2320. 799-163

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250

SELECTED BUSES AND COACHES  
ACTUALLY IN STOCK AND AVAILABLE FOR  
IMMEDIATE INSPECTION AND TEST.  
SPECIAL H.P. TERMS.

SUPER GUY SUPER  
DOUBLE-DECKERS. 6LW GARDNERS. DOUBLE-DECKERS.

**1949** 47 GUY high-bridge 36-seater double-decker buses, fitted 6LW Gardner engines and Eastern Coachworks bodies, half-sliding windows, in super mechanical and body condition, certificates of fitness to December 1959, and December 1961, price £425-£475.

**1948** 49 BRISTOL 36-seater high-bridge double-decker, fitted Metcam all-metal body, half-sliding windows, 5LW and 7.7 A.E.C. diesel engines in excellent mechanical condition, certificate of fitness 1959-60, price £550.

**1939** 38 BRISTOL K-type 55-seater low-bridge double-decker, choice of 12, with 1947-48 Eastern Coachworks bodies, resealed this year, fitted 5LW Gardner engines, in super mechanical and body and paint condition, certificates of fitness to 1960-59, price £225-£250.

#### COACHES

FOR THE SMALL OPERATOR.  
PART-EXCHANGES.

**1951** 52 A.E.C. fully fronted Beadle, choice of 16 39-seater coaches, in immaculate body and chassis condition, certificates of fitness 1961-62, prices £900-£950.

**1949** A.E.C. fitted 35-seater Thurgood coach body, semi-luxury seating, 7.7 A.E.C. diesel unit in good mechanical and body condition, certificate of fitness expiring 1960, price £395.

**1947** COMMER, 30-seater Plaxton full luxury coach body, fitted servo braking, in excellent mechanical and body condition, certificate of fitness September 1959, price £230.

**1948** LEYLAND P51 34-seater saloons, choice of 16, Brush all-metal bodies, in super mechanical and body condition, certificates of fitness to 1960-62, price £495-£525.

**1947** 48 LEYLAND PDI 56-seater high-bridge double-deckers, bodies by Charles Rowe, in excellent mechanical and body condition, certificates of fitness to 1960-61, price £550-£575.

**1948** BRISTOL, saloons, choice of 10, fitted Eastern Coachworks 35-seater bodies, low-type radiators, low-visibility cab screens, 7.7 A.E.C. diesel units, 5-speed boxes, in excellent mechanical and body condition, certificates of fitness to 1960, price £550 each.

**1950** GUYS, choice of two, fitted 33-seater Duple luxury bodies with full luxury high-backed leather and moquette seating, with 10-litre Meadows 6-cylinder diesel engines, in excellent mechanical and body condition, certificates of fitness to 1959-60, price £295.

**1939** 38 BRISTOLS, choice of 15, with 1948 Eastern Coachworks 35-seater saloon bodies, half-sliding windows, sliding entrance door, fitted 5LW Gardner engines, 5-speed boxes, in super mechanical and body condition, certificates of fitness to 1960, just into stock, price £250 each.

**1947** DENNIS, choice of two, fitted 33-seater Duple and Plaxton coach bodies with high-backed leather and moquette seating, 6-cylinder Mark III Dennis diesel engines, in excellent mechanical and body condition, certificates of fitness to October 1959, price £275 each.

**1946** MAUDSLAY, fitted with 35-seater luxury Burlingham coach body, high-backed leather and moquette full-luxury seating, A.E.C. 7.7 diesel engine, in excellent mechanical and body condition, certificate of fitness to December 1960, price £295.

**LEYLAND** TS8 34-seater service saloon, fitted with 1951 Burlingham all-metal body, Dunlopillo seating, Leyland P51 7.4-litre low-mileage diesel engine, in excellent mechanical and body condition, certificate of fitness to 1960, price £300.

**1939** LEYLANDS, choice of two, fitted 39-seater Alexander all-metal semi-coach bodies, high-backed tubular seating, resealed this year, fitted 7.4 P51-type diesel engines, in excellent mechanical and body condition, certificates of fitness to 1960, price £325 each.

**1940** 39 BRISTOL L-type 35-seater saloons, fitted with 5LW Gardner diesel engines, 5-speed boxes, in good mechanical and body condition, several chassis only, price £175 each.

**1940** 39 LEYLANDS, choice of 25 high- and low-bridge double-deckers, fitted 1948-49 Leyland all-metal bodies, in very good mechanical and body condition, certificates of fitness to end of 1959, will recertify at very little expense, to clear £150-£175 each.

**DENNIS**, choice of 25 low-bridge double-deckers, fitted 1948-49 Park Royal bodies, 5LW Gardner diesel engines, 5-speed gearboxes, in excellent mechanical and body condition, certificates of fitness expiring 1959-60, several with tickets nearly expired, will all recertify, price to clear £175-£200 each.

**LEYLANDS**, A.E.C., Bristol, etc., over 50 high- and low-bridge double-deckers, just come into stock, 1948 bodies, Leyland, Metcam, etc., varying certificates of fitness, to clear, £150-£175 each.

**THESE** are only a few of the vehicles we have in stock available for immediate inspection and test.

**ALL** the above vehicles carry our three months' guarantee.

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COMMERCIAL-VEHICLE SPECIALISTS.

HEAD OFFICE:—  
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**IMMEDIATE** delivery new BEDFORD SB3 (petrol)  
Duple Super Vega 41-seater, 8 ft. wide, glass roof  
quarters, tubular racks, heaters. Radiomobile speech  
amplification, glove nets, special seating with individual  
squabs, headrest covers, K-type exterior moulding scheme,  
finished maroon and grey, list price.

**IMMEDIATE** delivery new BEDFORD SB3 (petrol)  
Plaxton 41-seater coachwork, heaters fitted, radio and  
microphone, dual-tone. Formica panels, finished ivory,  
upholstered in red and grey moquette, list price.

**EARLY** delivery of new BEDFORD petrol-engine  
chassis, 1959 models, Duple 41-seater, Super Vega  
coaches, finished to instructions.

**NEW** BEDFORD petrol-engined chassis, 1959 model,  
Plaxton 41-seater coachwork, early delivery, finished  
to instructions.

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Harrington 41-seater coachwork, finished to instructions.

**NEW** BEDFORD SB3 (petrol), Burlingham Seagull  
41-seater full luxury coachwork, finished to instructions,  
delivery mid-April, list price.

**NEW** A.E.C. Reliance, 41-43-seater Duple Britannia  
coachwork, centre or front entrance, vacuum brakes,  
finished to instructions.

**1957** BEDFORD, petrol, 41-seater Duple Super Vega,  
glass roof quarters, tubular racks, radio, micro-  
phone and heaters, upholstered in red moquette, finished  
cream, certificate of fitness 1964, choice of two.

**1955** LEYLAND Comet 90, mounted with 36-seater  
Duple Super Vega coachwork, lift-up roof  
vents, red moquette, finished maroon and cream, certificate  
of fitness 1960.

**1955** BEDFORD, mounted with 38-seater Duple  
Super Vega coachwork, blue moquette, finished  
light and dark blue, certificate of fitness December, 1959.

**1955** BEDFORD 38-seater Plaxton luxury coach-  
work, upholstered in red moquette, many  
extras, finished blue and cream, certificate of fitness 1960.

**1953** BEDFORD, 35-seater Duple Vega, upholstered  
in red moquette, finished in light and dark blue,  
certificate of fitness 1962.

**1952** 53 BEDFORD, petrol engine, mounted with  
Duple full-luxury 38-seater 8-ft. wide coach-  
work, upholstered in red moquette, finished grey and  
red, certificate of fitness 1962-63, choice of four, these  
coaches are in immaculate condition.

**1952** BEDFORD, petrol, 33-seater Duple Vega coach-  
work, Formica side panels, upholstered in green  
moquette, 7 ft. 6 in. wide, finished maroon and cream,  
choice of six, certificate of fitness 1962, ex well-known  
fleet operator, in immaculate condition.

**1952** BEDFORD, petrol, 33-seater Plaxton coach-  
work, upholstered in blue moquette, finished  
cream and blue, certificate of fitness 1962.

**1952** BEDFORD 35-seater Super Vega, interior in  
green moquette, finished green and grey, radio  
and heater, certificate of fitness 1960.

**1951** 42 A.E.C. Mark IV, mounted with 39-seater  
full-front Burlingham coachwork, heater, fitted  
central entrance, autumn tint moquette, immaculate  
condition, certificate of fitness 1961-62, choice of four, one  
cream and green, three red and cream.

**1950** LEYLAND PS2 33-seater Burlingham coach-  
work, half-cab, red moquette, heater fitted,  
finished cream and maroon, certificate of fitness 1960, very  
clean.

**1949** BEDFORD 29-seater Duple Vista, upholstered  
in green, Formica side panels, finished cream  
and green, certificate of fitness 1959, choice of two, both  
coaches in immaculate condition.

**1948** DAIMLER CIDV6, oil engine chassis, mounted  
with Duple 35-seater full-luxury coachwork,  
upholstered in blue moquette, finished grey and red,  
certificate of fitness 1960, very clean condition, choice of  
two.

**1947** A.E.C. Regal, 7.7 engine, 35-seater Duple  
coachwork, certificate of fitness 1960.

**LEYLAND** TS4-6-7, oil engine and chassis, full floating  
L axle, fitted with new Duple bodies in 1947, 33-seaters  
upholstered in red moquette, certificates of fitness 1960-61,  
choice of six. These vehicles have been completely  
modernized.

**CHOICE** of several 1946-7-8 BEDFORD 27-29-seater  
Duple Vista coaches; Austin 29-33-seaters; Commer  
Commando and Avenger 33-35-seaters; certificates of fit-  
ness; cheap to clear.

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**NEW** BEDFORD Duple Super Vega 41-seaters, early  
delivery.

**1954** BEDFORD Yeates Riviera 36-seater coach,  
quarter lights and tubular racks, Radiomobile  
and heaters, recertified five years, £2,100.

**1952** Daimler Falcon (Bedford 38 petrol engine),  
Duple Vega 36-seater body (exactly as  
Bedford Vega), radio and heaters, excellent condition  
throughout, certified 1962, £1,250.

**1950** A.E.C. 9.6, Burlingham full-fronted body,  
33-seater, radio and heaters, £1,000.

**1950** LEYLAND PS1, Burlingham 33-seater body,  
certified 1960, £800.

**1948** BEDFORD Duple, 29 high-back seats, certified  
1960, £385.

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**A.E.C.** 1946-47-48 SERVICE BUSES. **A.E.C.**  
35-, 36- AND 38-SEATER BODIES BY BRUSH AND  
BURLINGHAM. SOME WITH BOOTS. CERTIFI-  
CATES OF FITNESS TO 1964. CHOICE OF 46  
VEHICLES NOW ARRIVING IN STOCK. PRICE  
FROM £295.

**1958** AUSTIN Minibus, 13-seater, six months' old,  
in excellent condition, fitted radio and heater,  
£560.

**1948** LEYLAND PS1 34-seater saloons, Brush all-  
metal bodies, in excellent condition, certificates  
of fitness 1960-62, priced from £450.

**1949** ALBION CX9 diesel 33-seater coach, very good  
mechanical and body condition, certified, £275.

**1948** 47 LEYLAND PS1 34-seater saloons, Met-  
Cam all-metal bodies, in excellent mechanical  
and body condition, certificates of fitness till the end of  
1961, choice of 15, prices from £330.

**1947** GUY 32-33-seater coaches, 51W engines, cer-  
tified 1960, choice of five from £275.

**1946** 1943 GUY low-bridge double-decker 54-seater,  
51W Gardner, certificate of fitness 1960, choice  
of six, from £250.

**BRISTOL** 32-seater bus, low radiator, 5-speed box,  
certified, £275.

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WEEK.

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OUR WONDERFUL SELECTION UNDER IDEAL  
CONDITIONS.

STOCK EXAMPLES INCLUDE:—

**BEDFORD.**

NOW 2 ONLY 1958 BEDFORD-DUPLE 41-SEATERS,  
UNDER 10,000 MILES, AS-NEW LUXURY COACHES.

CHOICE OF 3 1956 BEDFORD-DUPLE 41-SEATERS,  
2 IN IVORY AND BLUE AND ONE IN GREY AND  
BLUE. ALL TIP-TOP LUXURY COACHES.

1954 AND 1955 BEDFORD-DUPLE AND PLAXTON  
34-, 35-, 36- AND 37-SEATERS. IN THIS RANGE  
WE HAVE A SELECTION OF QUALITY COACHES  
SECOND TO NONE.

1951 AND 1952 BEDFORD-DUPLE 33-SEATERS.  
SPLENDID EXAMPLES OF THIS POPULAR MODEL.

BEDFORD-VISTA 29-SEATERS. CHOICE OF TWO  
VERY FRESH MACHINES, BEING RECERTIFIED  
NOW.

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1955 ALBION-DUPLE 35-SEATER, MAROON AND  
CREAM, SPLENDID CONDITION THROUGHOUT.

1955 ALBION-STRACHAN 39-SEATER, EXCELLENT  
VALUE.

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BLUE AND CREAM, EX-SHOW MODEL, MOST  
ATTRACTIVE COACH AT REASONABLE PRICE.

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CHOICE OF 2 1949 LEYLAND COMET-PLAXTON  
33-SEATERS, ONE IN BLACK AND WHITE, THE  
OTHER BLUE AND CREAM. BOTH EXTREMELY  
REASONABLE IN PRICE.

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**BARNARD AND BARNARD, LTD.**

**1956** BEDFORD, Burlingham Seagull 41-seater full-  
luxury body, fitted with radio, speech amplifier,  
heaters and many other extras, in excellent condition  
throughout, certificate of fitness 1963.

**1955** BEDFORD Duple service bus, fitted with 80  
oil engine, 40-seater, leather seats, good tyres,  
in good clean condition throughout, certificate of fitness  
1960, choice of two.

**1954** BEDFORD, Burlingham Seagull 36-seater full-  
luxury body, good clean condition throughout,  
certificate of fitness 1959, choice of two.

**1953** BEDFORD Super Vega 40-seater, (full luxury  
touring coach, fitted with full-luxury Chapman  
reclining seats, radio and heater, Perspex quarters, in good  
clean condition throughout, certificate of fitness 1963.

**1952** TILLING-STEVENS, fitted with 4-cylinder oil  
engine, 37-seater full-luxury Duple body, heater  
and radio, in good clean condition throughout, certificate  
of fitness 1962, choice of two.

**1952** BEDFORD Countryman, 25-seater full-luxury  
Duple body, in good clean condition throughout,  
certificate of fitness 1962.

**1952** GUY Arab, fitted with a 35-seater full-front  
Thurgood body, fitted with radio, heater, a  
clean condition throughout, certificate of fitness 1962.

**1952** LEYLAND Royal Tiger, fitted with air brakes,  
39-seater full-luxury Yeates body, radio, heater,  
intercom, in good clean condition throughout, certificate  
of fitness 1960.

**1952** LEYLAND Royal Tiger, fitted air brakes,  
41-seater full-luxury Burlingham body, heater,  
good tyres throughout, in excellent condition, certificate  
of fitness 1963.

**1951** BEDFORD Vega, 33-seater full-luxury Duple  
body, fitted with heater, good tyres, in good  
clean condition throughout, certificate of fitness 1962,  
choice of three.

**1951** BEDFORD Vista, fitted with a Perkins P6 oil  
engine, full-luxury Duple body, 29 high-back  
seats, in good clean condition throughout, certificate of  
fitness 1961.

**1951** BEDFORD Vega, 33-seater full-luxury Duple  
body, fitted with heater, Perspex quarters,  
in good clean condition throughout, certificate of fitness  
1961.

**1951** LEYLAND PS2, full front, fitted 37-seater  
full-luxury Allweather body, Perspex quarters,  
twin heaters, certificate of fitness 1961.

**1950** COMMER Avenger 33-seater, Plaxton, good  
tyres, finished in blue, certificate of fitness  
1960.

**1950** SENTINEL Service bus, 40-seater, Dunlop  
seats, trimmed in moquette, under the floor,  
diesel engine, front sliding door, in good clean condition  
throughout, certificate of fitness November, 1961.

**1950** A.E.C., 9.6, crane, full-front half-deck  
luxury, Whitson body, 31-seater, one owner  
since new, good clean condition throughout.

**1950** BEDFORD 29-seater, full-luxury Duple body,  
Perspex quarters, radio and heater, in excellent  
condition, certificate of fitness 1960, choice of five.

**1950** FODEN 2-stroke, 39-seater full-luxury 80-  
house Hartwell, fitted with heater and  
radio, in good clean condition throughout, certificate  
of fitness 1960.

**1950** VULCAN full-front, fitted with Perkins P6 oil  
engine, 31-seater full-luxury Duffield body, in  
excellent condition throughout, certificate of fitness 1960.

**1949** BEDFORD Mark 2 service bus, 31-seater  
Duple body, in good condition throughout,  
certificate of fitness 1960, choice of two.

**1949** LEYLAND PS1, full-front 35-seater full-luxury  
Duple body, fitted with wind-up heater, in  
good clean condition throughout, certificate of fitness 1963.

**1948** BEDFORD Vista, 29-seater Duple body,  
Dunlop seats, in good clean condition  
throughout, certificate of fitness 1962, choice of three.

**1948** FODEN, 61W Gardner engine, 33-seater, full-  
luxury Plaxton body, fitted with heater, in good  
clean condition throughout, certificate of fitness 1960.

**WE** also have a choice of several coaches, suitable for  
workmen and mobile shops, at reasonable prices.

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**1955**, March, AUSTIN (28) luxury coach, glass  
quarters, tubular racks, heater, speech am-  
plification, high back seats; maroon and cream, excellent  
condition.

**1952**, May, BEDFORD Vega (33-35), maroon and  
cream, high-back tubular seats, Formica sides  
certified 19.2.62, £1,295.

**1952**, April, DENNIS Falcon, Thurgood luxury  
coach with glass roof quarters and rear dome,  
certified 12.3.62, £1,075.

**1951**, April, FODEN Windover coach (37) 19  
condition, blue and cream, heater, fog lamp,  
2-stroke engine, 15 m.p.e., certificate of fitness 22.8.60,  
£1,175.

**1951**, June, DENNIS Falcon (35) full-fronted coach,  
light and dark red, heaters, chrome wheels,  
good condition, certificate of fitness to 18.8.61, £875.

**1950**, May, COMMER Avenger (33) Plaxton luxury  
coach with glass roof quarters and rear dome,  
red chair seats, exterior cream and green, one owner,  
certificate of fitness to 1960, £850; another £675.

**1950**, DENNIS full-fronted Duple (35) coach,  
high-back seats, blue inside and out, heater, good  
condition, certificate of fitness to 1960, £1,050.

**1950** BEDFORD Vistas (29), red high-back seats,  
Formica sides, exterior cream and green, choice  
of two, from £675.

**20-SEATER** BEDFORD Vista with reclining armchair  
and grey, in good order, £575.

**30-SEATER** FORD Thurgood coach, Dunlop seats,  
green and cream, certificate of fitness to 31.10.60,  
£375.

**1948-47-46** BEDFORD Vistas (29), green, red and  
grey, choice of three from £325.

**1948** 30 BEDFORD buses, 28-30 blue, green and  
red; choice of four from £275.

**1948** BEDFORD driver-operated door gears for Vistas and  
Duple-deckers, various makes from £100.

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Bus, fitted with 1800 cc. engine, good tyres, certificate of fitness.

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30-seater, full-luxury coach, excellent condition 1961.

25-seater full-luxury coach, excellent condition 1961.

35-seater, full-luxury coach, excellent condition 1961.

30-seater, full-luxury coach, excellent condition 1961.

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PETROL/OIL OR COMMER TWO-STROKE.

FITTED WITH PLAXTON, BURLINGHAM OR DUPLÉ

BODIES.

NEW VEHICLES.

A.E.C. 7.7 30-ft. chassis fitted with 39-seater High-

way body, new and unregistered service bus.

1958 BEDFORD 41-seater, petrol, Super Vega, red interior.

1958 BEDFORD petrol 41-seater Plaxton, red interior.

1958 BEDFORD Super Vega 41-seater (Bedford's own oil engine), 4,000 miles.

1958 BEDFORD petrol 41-seater Super Vega.

1957 BEDFORD petrol 41-seater Plaxton, red interior.

1955 BEDFORD 38-seater, Duplé, red interior.

1955 BEDFORD Super Vega 38-seater, red interior.

1955 BEDFORD Vegas 33-seaters, choice of four.

1953 BEDFORD Vegas 33-seaters, choice of three.

1952 LEYLAND 43-seater Metalcraft body, £1,500.

1950 COMMERCIAL Avenger under-floor, carrying 60.

1950 COMMERCIAL Avenger 33-seater, fitted Albion

Chieftain engine, immaculate.

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bodies, choice of four.

1949 BEDFORD 29-seater Duplé, high-backed seats,

just been certified, choice of three.

1950 LEYLAND, fitted Duplé Ambassador body.

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CHOICE OF 50 Leyland, A.E.C., Crossley, Guy full-

fronts and half-cabs, luxury coaches and service buses,

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luxury saloon, can be finished to customer's instruc-

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1958 ALBION Victor Duplé 35-seater luxury coach.

1955 BEDFORD Duplé 36-seater luxury coach.

1954 LEYLAND Tiger Cub 41-seater full luxury

coach, certificate of fitness 1959-61.

1951 LEYLAND Royal Tiger Burlingham Seagull

37-seater.

1950 July, COMMERCIAL Avenger 34-seater full luxury

coach, radio and heater, certificate of fitness

June, 1959.

1950 BEDFORD Vista 29-seater Duplé.

1950 COMMERCIAL Avenger 32-seater coach, good order.

1949 Duplé luxury seating coach bodies for 17-ft.

6-in. wheelbase, bodies only.

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33-seater luxury.

1949 LEYLAND PDI chassis, 7.4-litre diesel engine,

fitted Brush 53-seater low-bridge-type double-

deck, certificate of fitness from 1959 to 1961.

A.E.C. 7.7 Duplé 35-seater bus, very fine order.

1947 LEYLAND PDI, Northern Counties all-metal

53-seater low-bridge double-deck buses (choice

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double-deck bus, certificate of fitness 1961.

1947 November, LEYLAND PSI Plaxton or Bur-

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1947 LEYLAND PDI Roe 51-seater double-deck,

1946 LEYLAND PSI service buses, 32-seater Roe

bodies, certified to 1959; choice of four.

OVER 100 high- and low-bridge-type double-deck buses

from £100 each, also a number of Leyland 36-seater

buses, Dennis 32-seater buses with 5LW power units,

suitable for contract or public works contractors, price

from £175 each.

FOR EXPORT OR DISMANTLING ONLY.

A Number of LEYLAND T58 chassis with 7.4 diesel

power units and fitted with 1949 Duplé high-back

seating coachwork.

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WALMER BRIDGE, LONGTON, PRESTON.

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200

BUSES AND COACHES

ACTUALLY IN STOCK.

READY FOR IMMEDIATE SERVICE.

1951 A.E.C. Mark IV underfloor-engined 39-seater

full-luxury coach, immaculate, certified 1961,

£1,250.

1946-7-8 A.E.C. 34-seater buses powered by 7.7

diesel engines, all in first-class condition

throughout, very clean and excellent all-metal bodies, good

batteries, etc., £295 each; choice of 20, certified.

1948-9 A.E.C. 34-seater buses, in really super con-

dition, powered by 9.6 diesel engines, good

batteries and ready to go to work, £340 each; choice

of eight, certified.

1949 31W diesel engines and 5-speed gearboxes;

these are genuine coaches with full vision and not rebuilt

or re-registered, a very lovely fleet of vehicles, £450 each.

1947 LEYLAND genuine PSI 32-33-seater buses,

powered by Leyland PSI engines, in 100% condition

throughout, £375 each; choice of eight, certified.

1950 LEYLAND genuine PSI 32-33-seater coaches,

powered by Leyland 6000 diesels, immaculate

condition throughout, a very super fleet, £495 each;

choice of 10.

1948 A.E.C. 9.6 diesels, all-metal bodies, air brakes,

good batteries, ready to go to work immediately, £575

each, certified.

1949 A.E.C. 33-seater coaches, in immaculate con-

dition, fitted with 9.6 engines and air brakes,

£395 each, certified.

1949 GUY 33-seater full-luxury coach, fitted Gardner

5LW engine, Plaxton body, in super condition,

£295, certified.

1949 DENNIS 35-seater coach, fitted Dennis Big 6

diesel, excellent throughout, £295, certified.

1949 FODEN 35-seater coach, fitted with Gardner

5LW engine, certified and in first-class

condition, £325.

1947 FODEN 35-seater coaches, fitted with Gardner

6LW engines, in lovely order throughout, choice

of three, certified 1961, £275 each.

1950 DENNIS 35-seater coach, fitted with Dennis

Big 6 engine, certified 1960, £295.

Buses powered by 7.7 diesels and fitted with

A.E.C. 1950 35-seater bodies; these machines look

and run like new vehicles, £295 each; choice of 18,

certified.

LEYLAND bus, powered by Leyland 8.6 diesels and

L fitted with 1950 35-seater bodies, all in first-class

condition, carrying full guarantee, at £275 each; choice

of £20.

CROSSLEY 33-seater coaches, powered by Crossley 8.6

diesels, all genuine 1949 machines with Burlingham

bodies, £295 each; choice of 18, certified.

LEYLAND 56-seater double-deckers, powered by Leyland

D diesels, all carry full guarantee, £275 each; choice

of 23.

Also 150 single- and double-deckers, all fitted with

A good batteries and ready to go to work, from £150

each.

FRANK COWLEY,

3 BLACKFRIARS ROAD,

SALFORD, 3.

Phone, Manchester Blackfriars 7577 and Blackfriars 1048.

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E. J. BAKER AND CO. (DORKING), LTD.

THE COACH SPECIALISTS OF THE SOUTH.

COACH SHOWROOMS AND SERVICE STATION,

TRADING ESTATE, FARNHAM, SURREY.

PHONE, 8 A.M. TO 6 P.M., FARNHAM 4626-7.

AFTER 6 P.M., FARNHAM 4481.

WE CAN GIVE AN EARLY DELIVERY OF 1959

PETROL AND DIESEL MODELS, FITTED DUPLÉ

AND BURLINGHAM BODIES.

(INQUIRIES WELCOMED.)

1956 BEDFORD Burlingham 41-seater, radio and

intercom, Formica sides, cream and blue

exterior, immaculate vehicle, one owner only, £2,700.

1954 SEDDON Duplé Elizabethan 41-seater, Perkins

R6 Mk. 2 engine, red interior, cream-red

exterior, very clean, £2,450.

1953 BEDFORD Duplé 37-seaters, red interiors,

heaters, choice of two, £1,850.

1952 BEDFORD 37-seater Gurney Nuttings, autumn

interior, heater, cream and maroon exterior,

one owner, above average, certificate of fitness 1962,

£1,600.

1950 June, BEDFORD Vista, 29 seats, glass roof

quarters, blue interior, cream exterior, very

clean, certificate of fitness 1959, £795.

1949 BEDFORD Vista 29-seater, red interior, cream

and maroon exterior, one owner, certificate of

fitness September, 1959, £675.

1951 LEYLAND Royal Tiger, fitted air brakes,

Plaxton 41-seater body, green interior, certificate

of fitness April, 1961, £2,000.

1951 A.E.C. Mk. IV, autumn interior, Plaxton 39-

seater luxury body, £2,000.

1952 (Registered) LEYLAND, fitted PSI engine,

Beadle 35-seater, full-front body, good tyres,

certificate of fitness October, 1961, £750.

1952 DENNIS Falcon Burlingham 33-seater Seagull

body, blue interior, Eaton 2-speed axle, certificate

of fitness 1961, £1,250.

1948-9 DAMLER CDV6, fitted 33-seater luxury

bodies, good runners, choice of two, both

with certificate of fitness, from £350.

1948 DENNIS L-type, 33-seater, red interiors, high-

</

### Used Passenger Vehicles (contd.)

**COACHES AND COMPONENTS, L. TD.**  
469-475 HOLLOWAY ROAD, LONDON, N.7.

**1959** New BEDFORD petrol-engined Duple 41-seater Super Vega, 8 ft. wide; delivery end of January.  
**1955** March, BEDFORD 36-seater diesel-engined Super Vega 8 ft. wide; immediate delivery, grey, clock, radio, heaters, glass roof quarters, certificate of fitness 1960.  
**1955** COMMERCIAL Harrington, grand condition.  
**1952** January, BEDFORD 37-seater Gurney-Nutting body, red upholstery, exterior maroon-cream, certificate of fitness to January, 1962.  
**1950** BEDFORD 29-seater Vista, certificate of fitness 1959-60; choice of two.  
**1950** MAUDSLAY, A.E.C. 7.7 engine, 33-seater Duple, sliding roof, Clayton heater, new tyres all round, red moquette, exterior red, maroon, white, certificate of fitness to December, 1959.  
**1950** July, GUY Vixen, 29-seater Thurogood body, blue moquette, exterior blue, certificate of fitness to 1959.  
**1950** August, BEDFORD, 29-seater Vista body, red upholstery, exterior maroon-cream, certificate of fitness to August.  
**1950** July, FODEN 37-seater Metalcraft body, heater, radio, certificate of fitness to April, 1960.  
**1949** May, CROSSLEY 33-seater Duple body, red-fawn moquette, exterior red-cream, certificate of fitness to May, 1959.  
**1949** June, CROSSLEY 33-seater Whitson coach, red moquette, maroon-cream exterior, clock, heater, top sliding windows, sliding roof, certificate of fitness to July, 1959.  
**1949** July, AUSTIN 29-seater Whitson, autumn tinge moquette, exterior blue-cream, high-back seats, radio, certificate of fitness May, 1959.  
**1949** MAUDSLAY, 33-seater Gurney-Nutting body, grey floral moquette, exterior grey-green, certificate of fitness November, 1959.  
**1949** November, DENNIS 35-seater Duple, blue moquette, exterior blue-tone.  
**1948** FODEN 33-seater luxury coaches; choice of two.  
**1948** May, DENNIS Lancet III 33-seater Duple 1948, luxury, sliding roof, high-back seats, red-fawn moquette, exterior cream-blue, heater, certificate of fitness May, 1959.  
**1947** PSI 33-seater Harrington, certificate of fitness 1961-2.

### SILVER LINE MOTORS,

MOORLANDS,  
WELWYN GARDEN CITY,  
HERTS.

Phone, W.G. 5494.

PLEASE

PHONE OR WRITE  
FOR FULLER DETAILS.

**1956** BEDFORD Plaxton 41-seater, certificate of fitness 1961, cream and green, fitted heater, radio, interior fawn, radio, immaculate.  
**1955** BEDFORD Super Vega 41-seater, cream with autumn tint interior, fitted heater, good tyres, low mileage.  
**1954** SEDDON Duple 41-seater, certificate of fitness 1961, immaculate condition, light and dark blue, tubular heaters, radio and microphone, and extra fog lamp and antenna.  
**1954** A.E.C. Mk. IV 41-seater, Roc bodywork, certificate of fitness 1960, red and maroon with maroon interior, extra-clean vehicle, in 100% mechanical condition.  
**1952** BEDFORD Vega 35-seater, excellent mechanical condition, practically new tyres, certificate of fitness 1961, cream with blue interior.  
**1951** BEDFORD Vega, cream with maroon interior, certificate of fitness 1961, fitted heater, good condition throughout.  
**1951** MAUDSLAY Mk. III 35-seater, certificate of fitness November, 1960, cream with blue, fitted heater, excellent condition.  
**1950** DENNIS J3 Duple 35-seater, full-front coachwork, cream and green, green interior.  
**1950** DENNIS J3 Duple, 33-seater, cream and green with green interior, certificate of fitness May, 1960, sound condition.  
**1949** CROSSLEY 33-seater, certificate of fitness May, 1959, green with green interior, sound motor.  
**1949** DENNIS J3 35-seater, Duple coachwork, certificate of fitness June, 1959, cream and green.  
**1949** BEDFORD Vista 29-seater, cream with brown upholstery, good sound condition throughout.  
**1947** FODEN 33-seater, certificate of fitness May, 1960, cream and blue with fawn interior.  
**NEW** unregistered BEDFORD Martin Walter P.S.V. 12-seater, crimson and almond 2-tone with red interior, immediate delivery.  
**PART-EXCHANGES** welcomed, hire-purchase arranged.

### STOCKLAND GARAGE, L. TD.,

MARSH HILL,  
ERDINGTON, BIRMINGHAM, 23.

Phone, Erd 2488.

**ALBION** 1949 6-cylinder petrol engine full-fronted 29-seater Santus, with heater, the bodywork and mechanical condition of this vehicle are exceptionally good, four years' recent certificate of fitness, for sale, £325, low deposit, would consider monthly hire.  
**AUSTIN** 1950 6-cylinder petrol-engined 29-seater Salmasbury show body, heater and radio, excellent condition throughout, body and chassis, certificate of fitness 1960, for sale, £325, low deposit, would consider monthly hire.  
**A.E.C.** 7.7 engine, full-fronted Burlingham body, 33-seater, registered March, 1950, perfect condition.  
**LEYLAND** PSI, 1948, new Duple body fitted 1954, 35-seater, £1,700.  
**FOR** immediate delivery:—  
**NEW** FORD 11-seater P.S.V., cream-maroon trim.  
**NEW** AUSTIN 12-seater, all face-forward seats, not P.S.V.  
**NEW** and used commercials for sale under Commercial Column.

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### Used Passenger Vehicles (contd.)

**KIRKBY AND SONS (SALES), L. TD.**

CROSS ROAD GARAGE,  
ANSTON, NEAR SHEFFIELD.  
BEDFORD MAIN DEALERS.

NEW BEDFORD.

**DUPLE, PLAXTON, HARRINGTON.**

COMPARE OUR USED VEHICLE PRICES.  
FOR EARLY DELIVERY.

**1956** 41-seater BEDFORD Duple, red pattern quarter roof lights, tubular racks, radio and heaters, £2,750. Choice of two.  
**1956** BEDFORD Burlingham, red interior, maroon and ivory exterior, fitted radio and heater, £2,000. Choice of two.  
**1954** 36-seater BEDFORD Burlingham, red interior, maroon and ivory exterior, fitted radio and heater, £2,000. Choice of two.  
**1953** 36-seater BEDFORD Yeates, blue exterior, fitted quarter roof lights, tubular racks, £2,000.  
**1952** 34-seater and courier BEDFORD Duple, red interior, grey exterior, fitted heaters, £1,650.  
**1952** 35-seater BEDFORD, blue interior, very nice condition, £1,300.  
**1951** BEDFORD 33-seater Duple, quarter roof lights, tubular racks, very nice condition, £450.  
**1951** LEYLAND Royal Tiger 41-seater Plaxton, £2,150.  
**1951** LEYLAND Royal Tiger, 41-seater Plaxton, £2,150.  
**1949** AUSTIN 29-seater Plaxton, certificate of fitness 1961, very clean, £350.  
**1948** MAUDSLAY (A.E.C. 7), rebodied by Duple 1955, immaculate condition, £750.  
**1948** AUSTIN 29-seater Plaxton, certificate of fitness June, 1962, exceptionally clean, £380.

**ALL MACHINES ARE WORKS CHECKED AND ARE IN FIRST-CLASS CONDITION THROUGHOUT.**

**SPECIAL OFFERS OF HALF-CAB COACHES.**

**TERMS AND EXCHANGES.**

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NIGHT PHONE, KIVETON 220. MANSFIELD 5395.  
DINNINGTON 577. 799-476

### COMBERHILL GARAGES, L. TD.,

INGS ROAD, WAKEFIELD.

**NEW** 1959 COMMERCIAL TS3 diesel 41-seater Plaxton Consort Mark IV, completed to own specification.  
**NEW** 1959 model BEDFORD (petrol) 41-seater Plaxton Consort, heater, radio, contemporary moquette.  
**NEW** 1958 ATKINSON L544 (Gardner 4LW) 37-seater Plaxton Highway omnibus, coach seats.  
**NEW** BEDFORD SBI (Bedford diesel engine) 41-seater Plaxton Consort Mark IV, finish to detail.  
**NEW** A.E.C. Reliance (7.75-litre) 41-seater Plaxton Consort Mark IV, finish to detail.  
**1958** BEDFORD SB (petrol) 41-seater Duple Super Vega, 2,000 only, heater, radio.  
**1957** BEDFORD SB (petrol engine) 41-seater Duple Super Vega, ivory-black, heater fitted.  
**1955** COMMERCIAL TS3 diesel 41-seater Plaxton, amber quarter lights, heater, radio, ivory-black.  
**1953** BEDFORD SB (petrol) 33-seater Yeates, heater, radio, cream-brown, certified 1963.  
**1953** LEYLAND PS2-3 (9.8-litre) 35-seater Harrington full-front, radio, heater, certified 1960.  
**1953** BEDFORD (petrol) 37-seater Plaxton Ventura, heater, radio, certified 1963.  
**1951** A.E.C. Regal Mark IV, 9.6 underfloor, 41-seater Yeates, heater, radio, ivory-black.  
**1951** A.E.C. Regal Mark IV, 9.6 underfloor, 37-seater Burlingham Seagull, reconditioned engine.  
**1951** ALBION IV (petrol diesel FT3) 31-seater Cawood full-front, choice of two, cream-blue.  
**1950** LEYLAND PSI (7.4-litre) 35-seater Burlingham full-front, heater, radio, ivory-black.

**HIRE-PURCHASE FACILITIES.**

**COMBERHILL GARAGES, L. TD.,**

Phone, Wakefield 6051-5.

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### SAVILLE MOTOR SALES, L. TD.,

MAIN VAUXHALL AND BEDFORD DEALERS.

HARVESTER HOUSE,  
STRATFORD-ON-AVON.

Phone 3681 (10 lines).

**1959** BEDFORD Duple 30-seater bus, 300 cu. in. diesel engine, finished in red and grey, fitted heater, etc., ex-demonstration model, immediate delivery, certificate of fitness 1960.  
**1955** (Model) BEDFORD Duple 36-seater coach, radio, heater, petrol engine, finished in red and cream, certificate of fitness October, 1959.  
**1953** BEDFORD Burlingham 36-seater coach, radio, heater, petrol engine, finished in blue and red, certificate of fitness April, 1963.  
**1951** FODEN Whitson 41-seater coach, heater, diesel engine, finished in cream, certificate of fitness May, 1961.  
**1951** BEDFORD Metalcraft 33-seater coach, heater, petrol engine, finished in green.  
**1950** TILLING-STEVENS, 33-seater full-front Bell-house Hartwell body, radio, heater, cream and red, certificate of fitness 1960.  
**1949** BEDFORD Duple 29-seater coach, heater, petrol engine (choice of two), certificate of fitness.  
**1949** MAUDSLAY Whitson 33-seater coach, heater, diesel engine, finished in black and white, certificate of fitness June, 1959.  
**MONMOUTH DEPOT.**

**1948** DAIMLER Duple 33-seater coach, heater, diesel engine, finished in cream, certificate of fitness August, 1960.  
**1949** A.E.C. Burlingham 33-seater coach, heater, diesel engine, finished in blue, certificate of fitness July, 1959.

**PART-EXCHANGES. HIRE-PURCHASE FACILITIES.**

**REDBROOK, MONMOUTH.**

PHONE 336 AND 7.

799-104

### Used Passenger Vehicles (contd.)

**DON EVERALL, L. TD.**

34 CLEVELAND ROAD,  
WOLVERHAMPTON.

PASSENGER TRANSPORT SPECIALISTS.

**NEW** BEDFORD Duple 41-seater coaches, petrol and diesel, available for early delivery.  
**1959** BEDFORD petrol, 41-seater, Duple Super Vega coach, glass roof quarters, heaters, radio, many other extras, 1,000 miles only, £3,400.  
**1957** BEDFORD petrol 41-seater Duple coach, red patterned interior, cream exterior, heaters, etc., £2,850.  
**1956** BEDFORD, petrol, 41-seater Duple Super Vega coach, heaters, etc., £2,700.  
**1954** BEDFORD, petrol, 36-seater Duple coach, glass roof quarters, heaters, Formica panels, etc., £2,050.  
**1953** BEDFORD, petrol, 36-seater Yeates Riviera coach, glass roof quarters, new tyres, many extras, certified 1963, £1,900.  
**1952** SENTINEL 6-cylinder 44-seater diesel, all metal service bus, front entrance, jack-knife doors, heaters, immaculate condition, certificate 1964, £1,500.  
**1951** August, BEDFORD petrol 34-seater Plaxton coach, certified 1961, £1,350.  
**1951** SENTINEL 6-cylinder diesel 44-seater Beadle service bus, jack-knife door, new tyres all round, immaculate condition, certified 1961, £1,500.  
**1951** A.E.C. Mk. IV 39-seater Burlingham coach, certified 1961, £2,100.  
**1950** GUY 6LW Gardner 46-seater, half-deck coach, certificate of fitness 1960, £700.  
**1949** BEDFORD 27-seater Duple Vista coach, fitted with glass roof quarters, Formica panels, heater, radio, etc., new tyres all round, certified 1962, £675.

**50** Coaches and buses always in stock.

**SEND** for list giving full particulars, prices, etc.

**HIRE.**

**33-SEATER** half-cab diesel coaches for hire on monthly terms.

PHONE, WOLVERHAMPTON 25212.

NIGHTS AND WEEK-ENDS, 32347 AND 22293.

**DON EVERALL, L. TD.**

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### PERCY D. SLEEMAN, L. TD.,

LONDON COMMERCIAL DEALERS.

**COMMERCIAL** Rootes diesel Burlingham 41-seater, finished to choice, 14 days' delivery.  
**A.E.C.** Reliance Duple and Burlingham 41-seaters, finished to choice.  
**1955** COMMERCIAL TS3, Plaxton bodies, 39- and 41-seaters, certificate of fitness 1959.  
**1955** TS3, Duple bodies, 39-seaters, certificate of fitness 1960.  
**1954** BEDFORD, 36 ft., upholstered in red, painted to choice, certificate of fitness 1959.  
**1954** BEDFORD Burlingham, 36 seats, upholstered in blue, finished blue and grey, certificate of fitness 1959.  
**1953** BEDFORD 35-seater Yeates, trimmed in red, finished cream and red.  
**1952** BEDFORD, 37 seats, Gurney Nutting body, certificate of fitness 1962.  
**1951** LEYLAND Royal Tiger 41-seater, mounted Strachan body, 8 ft. wide, air brakes, choice of two, certificates of fitness 1961.  
**1948** BEDFORD 29-seater Vista, from £250.

**PART-EXCHANGES** and hire-purchase terms to suit individual requirements.

**38 UXBRIDGE ROAD,**

EALING, W.5.

Phone, Ealing 7987; Uxbridge 5022.

After hours, Western 1321.

799-412

### BIRDS COMMERCIAL MOTORS, L. TD.,

BIRMINGHAM ROAD,

STRATFORD-ON-AVON.

Phone 3222-3-4 and 2136. Grams, "Quicksale."

### USED PASSENGER VEHICLES.

**10** A.E.C. Regent Mark III double-decker buses, year of registration 1948-49, fitted with A.E.C. 9.6-litre diesel engines, full air brakes, fluid transmission, automatic chassis lubrication, 56-seater with Dunlopillo seating, metal bodies, 9.00 by 20 tyre equipment, current certificate of fitness.  
**10** LEYLAND double-decker buses, year of registration 1948-49, fitted with 0.600 Leyland diesel engines, full air brakes, fluid transmissions, automatic chassis lubrication, 56-seater with Dunlopillo seating, metal bodies, 9.00 by 20 tyre equipment, current certificate of fitness.  
**THE** above vehicles are as new in every respect.

**HIRE-PURCHASE** willingly arranged at low deposit.

**TWO** LEYLAND PDI double-decker buses, 56-seaters. All-metal bodies by Leyland, excellent condition.

**FURTHER**

**PARTICULARS AND PRICES**

**ON APPLICATION.**

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**1948** MAUDSLAY, Duple body, certificate of fitness four years, 33-seater.  
**1949** AUSTIN, Mann Egerton body, certificate of fitness 1957, 32-seater.  
**1951** BEDFORD Vista, Duple, certificate of fitness two years, 29-seater.  
**ALL** these coaches are in first-class condition.

**PHONE, Wallington 6648-9.**

799-15

### Used Passenger Vehicles (contd.)

**LANCASHIRE**

LIVERPOOL

Phone, Pendleton

**1956** BEDFORD blue-cream, red interior, price, August, 1959.  
**1951** A.E.C. 7.7 engine, 33-seater Duple, certificate of fitness 1960, price, August, 1959.  
**1948** BEDFORD 29-seater Vista, certificate of fitness 1961, price, August, 1959.  
**1954** BEDFORD 29-seater Vista, certificate of fitness 1961, price, August, 1959.

**MORRIS** Minicar, finished in immaculate condition, available from new at customer's cost, cash.  
**FORD** Thames F1000, finished in black-blue with red interior, absolutely as new, asking £1,000.  
**PART-EXCHANGES** and hire-purchase terms to suit individual requirements.

**RIGHT-ANGLED** age, Coshman.

**GARNER'S** CO.

**MAUDSLAY** I rate of fitness.

**AUSTIN** 1951

**AUSTIN** 1949

**AUSTIN** 1949

**READY** for sale

**37** SOUTH EAST

**1947** LEYLAND

**A.E.C.** 7.7 engine, 33-seater Duple, certificate of fitness 1960, price, August, 1959.

**BACK** from exchange. Terms

**COLBRO, L.**

**COACHES**, certain

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Used Passenger Vehicles (contd.)

**LANCASHIRE MOTOR TRADERS, LTD.**  
OLYMPIA GARAGE,  
LIVERPOOL STREET, SALFORD, 5.

Phone, Pendleton 5201; evenings, Oldham Main 2461.

**1956** BEDFORD PLAXTON 41-seater, red interior, blue-cream exterior, immaculate, price £2,750.  
**1953** A.E.C. Mark IV Burlingham Scapall 41-seater, red interior, grey-maroon exterior, used on only, price, £2,450.

**1951** August, BEDFORD Duple 33-seats, green interior, green-cream exterior, fitted glass windows and tubular racks, £1,300.  
**1950** A.E.C. 9.6, Burlingham full-front, 33 seats, red interior, cream-orange exterior, certificate 1960, price £950.

**1948** LEYLAND PS1, 33 seats, autumn tint interior, red and cream exterior, good tyres, certificate late 1960 choice of two, from £275.  
**1954** BEDFORD Plaxton, 38 seats, fitted lift-up roof vents, red interior, blue-cream exterior, price £1,850.

**MORRIS** Minibus, 12-seater, first registered April, 1958, finished in black, 4,900 miles only; this vehicle is immaculate, both interior and exterior and is indistinguishable from new. Demonstrations arranged anywhere at customer's convenience and without obligation, £550, cash for cash.

**FORD** Thames 12-seater on 15-cwt. chassis, finished in black-blue with red upholstery, 2,400 miles only, absolutely as new throughout, owner has no further use and is asking £625 o.n.o. for cash.  
**PART-EXCHANGE** and hire-purchase facilities.

**RIGHTON AND BENNETT, LTD.**, Portsdown Garage, Cosham, Portsmouth. Phone, Cosham 75372.  
799-1

**GARNER'S COACHES, LTD.**, offer:—

**MAUDSLAY** 1949 33-seater, A.E.C. 7.7 diesel, certificate of fitness, choice of two from £500.

**AUSTIN** 1951 33-seater, petrol, certificate of fitness, choice of two from £650.

**AUSTIN** 1949 29-seater, petrol, certificate of fitness, £285.

**RADY** for service. Hire-purchase.

**37** SOUTH EALING RD., London, W.5. Ealing 9046.  
799-328

**1947** LEYLAND PDIs, 56-seaters, high-bridge, Burlingham bodies, certificates of fitness, immaculate, £450 each.

**A.E.C.** Mk. III 1950 36-seaters, all-metal Roe bodies, service buses, 9.6 engines, certificate of fitness, exceptional value at £525 each.  
**DISTOL**, Leyland, Dennis, A.E.C. single and double-deck from £100 each, old vehicles taken in part-exchange. Terms, etc.

**NORTHS.**

**PONTEFRAC ROAD,**

**LEEDS, 10.**

Phone, Leeds 76809. 799-7166

**COLBRO, LTD.**, offer: Two 1947 A.E.C. Regal single-deckers, certificate of fitness, 4-60, fitted reconditioned engines, £250. One Leyland TD7 1942 double-decker, certificate of fitness, 2.50, £175. Two Bristol double-deckers, 1945, fitted A202 engines, £175 each. Leyland 1940, Quan, Bristol 15G, £150. Quan, Guy 6LW, £120. Al-o buses by Dennis, Albion, etc. Engines and only for cash.

**COLBRO, LTD.**, Wood Lane, Rothwell, Leeds. Phone, Rothwell 3258. 799-7131

**1949** MAUDSLAY 33-seater, certificate of fitness May, 1962, £335.

**1948** BEDFORD 41-seater, certificate of fitness September, 1959, £225.

**H.P.** if required.

**H. SHAW**, Maxey, near Peterborough, Market Deeping 799-7137

**J. W. FIELDSEND, LTD.**

OFFER THE FOLLOWING

**USED COACHES:—**

**NEW BEDFORD** petrol 41-seater Plaxton; delivery February.

**1957** 8 COMMER TS3 41-seater Plaxton and Duple; choice of several.

**1958** BEDFORD petrol 41-seater Plaxton, many extras, finished in maroon and cream, low mileage.

**NEW KARRIER** 14-seater Plaxton full-luxury, fitted heater and radio; delivery, April.

**1957** BEDFORD petrol 41-seater Plaxton.

**1957** BEDFORD petrol 41-seater Duple.

**TERMS** and exchanges, full list sent on request. Phone for appointment to view.

**160 CROSS LANE,**

**SALFORD, 5.**

Phone, Pendleton 5331. 799-418

**1956** 57 BEDFORD 41 Super Vegas, from £2,650.

**1950** LEYLAND PS1 35-seater Burlingham full-front, £1,050.

**1954** BEDFORD Duple Super Vega 38-seater, low mileage, very well kept, £2,150.

**1952** BEDFORD Super Vega 33-seater, quarter lights, magnificent condition.

**1949** FODEN, 5-cylinder Gardner, Duple 33-35-seater, new retrim, certificate of fitness 1962.

**1950** BEDFORD Duple 29-seater Vista, good tyres and excellent condition, high-back seats, £575.

**1950** COMMER, 33-seater Strachans body, beautiful condition.

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**EIGHT** A.E.C. Matador 7.7 diesel, ex-M.O.S., small mileage, £70 each, cased.  
**A. R. LEWIS**, 74 New Summer St., Birmingham, 19. A. Phone, Aston Cross 1943; after 6 p.m., Harbottle 3915. 799-102  
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862

### Miscellaneous Advertisements (contd.)

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**HAULAGE** business (limited company), Blackburn, consisting of 1958 A.E.C. Mammoth Major 6-wheeler with special A licence 7 tons, and 1952 Bedford 7-ton long-wheelbase lorry with B licence, with very good conditions, £5,750. Hire-purchase available.

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Phone, Glossop 2902-3.  
AFTER HOURS 2356. 799-197

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**2,000** First-class remould tyres including 1400 x 20, standard treads at £15 each. 1100 x 20 remoulds, standard treads at £6 each.

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**NEW 9.00** by 20 Dunlop Track Grip at £17 10s.

**REMOULD:** 10.50 by 20, £12 10s.; 11.00 by 20, £12 10s.

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#### WILDE AND BENNETT, L T D.

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AFTER HOURS, 2356. 799-196

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**BEST market**

**or quarter**

brand-new tyres,  
Garages (Ken),  
799-503  
including 1400 x 20,  
each, 1100 x 20  
6, at £4 each.  
400 x 20, 24-ply  
18 each. Money  
St. Philips Rd.,  
800-7193  
5 and 1200 x 20,  
ing, Ltd., Charville  
799-288

**Wanted**

ould, second-hand,  
Yorks. Shipley  
222-695  
sizes, spot cash,  
ell Rd.,  
222-979  
wheels, complete,  
cherry Iron and  
12, 800-7175

HAM), LTD., the  
effective crumblers,  
mer Rd., London,  
222-938

Works, Springfield  
ham 89023, under-  
electric requirements  
d colour finish,  
810-6811

by (phone 2262),  
222-979  
and most others,  
000 in stock,  
Paddocks, Pope's  
222-843

el tyre and tube),  
at £17 10s.  
11.00 by 20,  
Staffs. Phone,  
803-7133

y, as new, at £25  
Rd., Fimbleing,  
ley 203-204,  
799-180

uts, 4-in. B.S.F.  
Pelham Mess,  
799-5026

SIONS  
y, used, including  
r, Dodge, Ford,  
329 High Rd.,  
222-888

standing windows,  
ick frame repair  
Co., Ltd., 205  
Shoreditch, 327-24,  
222-619  
Denny, Ltd., 13  
1426, 222-816

(contd.)  
Old-established  
ness, 2 X vans  
lease (renewable)  
and Son, Ltd.,  
799-13

Etc., Wanted  
pecial A licences,  
in any part of  
LTD.,  
ER.  
799-196

or more coaches,  
considered. Box  
tor., 799-274  
A or B licence,  
all particulars in  
The Commercial  
799-25038

**Miscellaneous Advertisements (contd.)**

**CONVERSIONS**

**HENDY FOR FORD.**

BRITAIN'S FIRST FORD DEALER.

DIESEL 4D AND 6D ENGINES WITH REAR-END

CONVERSIONS AT LOW COST.

LET US QUOTE YOU.

**PERCY HENDY, LTD.,**

SOUTHAMPTON 28331 (EIGHT LINES). 222-882

**CONVERT YOUR VEHICLES**

**TO FORD 4D AND 6D POWER.**

ALL MAKES OF VEHICLES UP TO 7 TONS CAN

NOW BE FITTED WITH THE COST-CUTTING FORD

4D AND 6D ENGINES, MEANING:—

LOWER INITIAL COST,

MORE M.P.G.,

CHEAPER MAINTENANCE.

And the cost of conversion is very reasonable.

Let us quote you for your vehicles.

**QUICKS FOR FORDS.**

INDUSTRIAL UNIT SALES DEPT.,

221 DEANSGATE, MANCHESTER, 3.

Phone, Blackfriars 2468. 222-7053

**PETROL ENGINES.**

**PETROL**

**TO**

**DIESEL**

CONVERSION UNITS.

**PRILLS (HEREFORD), LTD.,**

HOLMER ROAD, HEREFORD.

Phone 4221 (six lines). 799-815

**CUT YOUR FUEL COSTS**

**WITH**

**SEARLS 4D INSTALLATION**

**FOR**

**OMODEL BEDFORDS.**

"DO IT YOURSELF" KITS FROM

**£45.**

OR COMPLETE CONVERSIONS.

**SEARLS ENGINEERING, LTD.,**

MANCHESTER ROAD,

LONDON, E.14.

East 3685. 799-76

**MANSFIELDS, LTD.,** Vauxhall, Bedford main dealers,  
offer new 1958 Martin Walter conversions, 2- and 4-  
ton Dormobile caravans, utilabrace, workbus, etc., for  
immediate delivery. Phone 3425-6; night, 3365. 15 High St., Chard,  
Somerset. 806-7045

**MANSFIELDS, LTD.,** 17 Cornfield Rd., Eastbourne.  
Phone 3003. 801-7185

**HAULAGE AND BACK LOADS**

**GUARD TRANSPORT CO., LTD.,** for all types of  
traffic to and from the West Country, serving London,  
Hemel, Southampton, Midlands, Liverpool, Manchester,  
etc. Phone 3425-6; night, 3365. 15 High St., Chard,  
Somerset. 806-7045

**HAULAGE** required for quantities fertilizer, Middle-  
brough and Scunthorpe to Oswestry area. Box  
CM9910, care of "The Commercial Motor." 799-204

**OWNER-DRIVER** seeks contract for 1,500-gal. tanker,  
any work or distance considered. Box CM9913, care  
of "The Commercial Motor." 799-xB4920

**HIRE-PURCHASE**

**H.P.** Finance available, private deals, prompt settle-  
ment. Finextra, Ltd., 58 Jermy St., S.W.1.  
Phone until 9 p.m., Hyde 1391. 222-980

**FINANCE** immediately available for hire-purchase, credit  
sale on all types of equipment, accessories, tyres,  
batteries, etc. Chancery Lane Credit Co., Ltd., 27 Chan-  
cery Lane, London, W.C.2. 801-7107

**INSURANCE**

**PAUL CHILDS, LTD.,**

58 BIRCH GROVE,

LONDON, W.3.

Acorn 2398.

**BEST** market rates—no-claim bonus to 40%. Monthly  
or quarterly payments accepted. 222-634

**Miscellaneous Advertisements (contd.)**

**SAVE** money on insurance. Lower rates, larger bonus.  
Progressive Brokers. "Cheapest Rates Specialists."  
257 The Vale, London, W.3. See 9231-2. 222-5034

**COMMERCIAL** vehicle and car insurance, expert advice,  
competitive rates, no-claim bonus to 40%, cash 10  
50%; deferred premiums. Obtain the best terms from  
specialists motor insurance brokers.

**INSURANCE ACCEPTANCES, LTD.,** 63-64 Broad St.  
Avenue, London, E.C.2. London Wall 7641-3  
222-630

**MISCELLANEOUS**

**11,000** Heavy iron shod timber mats (deck panels),  
approximately 48 in. by 36 in. by 3 in.  
thick by 80 lb. each, 4s. each, ex depot near Stratford-  
on-Avon, minimum 100 lots. Harford Engineering Co.,  
Ltd., Whiting Rd., Norwich. 804-7182

**Miscellaneous Wanted**

**ALUMINIUM** scrap urgently required. Lowton Metals,  
Ltd., Lowton Saint Mary, near Warrington. Leigh  
(Lancs) 1444-5. 799-455

**NOTICES**

**TO WHOM IT MAY CONCERN.**

WE THE UNDERMENTIONED WISH IT TO BE  
KNOWN THAT WE HAVE NO CONNECTION  
WHATSOEVER WITH ANY OTHER COMPANY OF  
THE SAME NAME.

WE ARE:—

THE PREMIER TRANSPORT.

HAROLD A. RUSSETT, LTD.,

1-4 SUSSEX STREET,

SAINT PHILIPS, BRISTOL, 2.

THIS IS OUR ONLY ADDRESS. 799-7187

**SITUATIONS VACANT**

**A.M.I.E.I.,** City and Guilds, A.M.I.Mech.E., etc., on  
details of exams, and courses in all branches of auto.,  
diesel, aero, mechanical eng., etc. Write for 148-page  
handbook, free. B.I.E.T. (Dept. 725), 29 Wright's Lane,  
London, W.8. 222-812

**COMMERCIAL** Vehicle Salesman (senior) to organize  
new department for Ford and Commer sales in S.W.  
London and Surrey. Essential qualifications are extensive  
sales experience in these makes; comprehensive knowledge  
of body construction and design; existing contacts in this  
area; staff administration in canvassing. Candidates aged  
35-45 seeking an opportunity to prove ability in this field  
are invited to apply in writing to Morden Station Garage,  
London Rd., Morden, Surrey, stating past experience, age  
and salary required. 799-7149

**ASSISTANT** Manager required by Grimshy haulage and  
clearing-house company. Good prospects and salary  
for capable person fully experienced in haulage industry.  
Apply, with references, to Box CM983, care of "The  
Commercial Motor." 799-7156

**BEDFORD** Main Dealers, Home Counties, require  
experienced C.V. salesman, good salary and com-  
mission, car and other incentives. Reply: Box CM998,  
care of "The Commercial Motor." 799-134

**FIRST-CLASS** diesel motor mechanic required for S.E.  
London transport contractors; must be keen man with  
initiative. Box CM9824, care of "The Commercial  
Motor." 800-x4700

**COMMERCIAL** Vehicle Sales Manager required by  
large Ford Distributors London. Applicants must  
have experience in similar capacity, with good sales record  
and first-class testimonials. Liberal salary and com-  
mission basis plus use of car. Write in confidence to Sales  
Director, Box CM9912, care of "The Commercial Motor." 799-212

**TRANSPORT** manager required for Northern haulier's  
office in the London area. Salary £800 to £1,000  
per annum. This position is suitable for a man with  
good connections and experience of clearing house pro-  
cedure. Write full details in confidence to Box CM9826,  
care of "The Commercial Motor." 799-7174

**ROAD HAULAGE ASSOCIATION.**

EASTERN AREA.

**YOUNG MAN** REQUIRED IMMEDIATELY AS  
ASSISTANT AREA SECRETARY IN THE  
EASTERN AREA OFFICE IN CAMBRIDGE WITH  
A VIEW TO ADVANCEMENT TO AREA SECRE-  
TARY AFTER A PERIOD OF SATISFACTORY  
SERVICE.

APPLY IN WRITING, STATING AGE, DETAILS  
OF EXPERIENCE AND SALARY REQUIRED  
TO AREA CHAIRMAN.

**ROAD HAULAGE ASSOCIATION.**

56 HILLS ROAD,

CAMBRIDGE. 799-37

**COMPANY** manufacturing specialist equipment for  
commercial and passenger service vehicles require  
additional sales personnel; some knowledge of bulk  
handling of liquids and powders advantageous, also con-  
tacts with builders of P.S.V. and specialist goods vehicle  
bodies; position based London but a fair amount of  
travel involved. Full details of past career to Box  
CM996, care of "The Commercial Motor." 799-87

**DIESEL** mechanic with thorough knowledge and experi-  
ence of tractors and earth moving equipment required  
for banana plantations in British Cameroons; salary up  
to £1,000 per annum according to age and experience;  
tours of 18 months with three months' leave; free  
passages and accommodation. Write Box CM997, care  
of "The Commercial Motor." 799-86

**Miscellaneous Advertisements (contd.)**

**DRIVER** for public service vehicles, good wages and  
conditions, knowledge of mechanics preferred, but not  
essential. Apply: Regent Motorways (Redditch), Ltd.,  
Alcester St., Redditch, Worcs. 799-94

**HAULAGE** contractors have a vacancy for traffic  
manager in their London depot; wages are £14 per  
week, plus commission. Box CM994, care of "The Com-  
mercial Motor." 799-83

**REQUIRED** by diesel engine manufacturers, a senior  
sales engineer with extensive experience and contacts  
in the heavy vehicle industry. Applicants are requested  
to give precise details of their experience. A good salary  
will be paid to the right man. Box CM993, care of  
"The Commercial Motor." 799-84

**TRANSPORT** manager for important South London  
transport firm, removals experience advantage;  
superannuation scheme. Write full details past experience  
and present salary to Box CM992, care of "The Com-  
mercial Motor." 799-85

**ASSISTANT** engineer required, large fleet of heavy com-  
mercial vehicles; good prospects. Apply in writing,  
giving details of experience, qualifications, etc., to General  
Manager, Bulwark Transport, Ltd., Chippenham, Wilts.  
799-100

**COMMERCIAL** vehicle and car salesman required by  
Dorset Dodge distributors. Other agencies include  
Morris and Ford commercials and Morris, Wolsley and  
Ford cars.

**THIS** is a really excellent opportunity with sound  
prospects for an enthusiastic salesman of proved  
ability with a thorough knowledge of commercial vehicle  
selling and used vehicle prices. Apply in first instance  
giving fullest particulars as to age, experience, etc., in  
confidence to Camkin Motors, Ltd., Sherborne, Dorset.  
799-14

**OLD-ESTABLISHED** HEAVY COMMERCIAL  
VEHICLE MANUFACTURERS

**REQUIRE EXPERIENCED**  
SALES REPRESENTATIVE  
FOR THE BIRMINGHAM AREA.

Permanent appointment and excellent prospects for the  
right man.

Please give details of age, education and business  
experience.

**APPLICATIONS WILL BE TREATED IN STRICT**  
**CONFIDENCE AND SHOULD BE ADDRESSED TO**  
**THE SECRETARY, FODENS, LTD., ELWORTH**  
**WORKS, SANDBACH, CHESHIRE.** 799-17

**WADHAMS, LTD.,** B.M.C. distributors and body-  
builders, require a fully experienced commercial  
vehicle sales representative whose sales record warrants  
above average remuneration; this is an important position  
which affords unusual scope and promotion prospects; a  
car is provided and life insurance, sick pay and pension  
schemes are in operation. All applications will be treated  
in strict confidence. Apply in writing in first instance to:  
Local Director, Wadhams, Ltd., The Avenue, Southamp-  
ton. 799-2

**EXPERIENCED** petrol and diesel fitter required for  
distributor in Croydon area. Good pay and con-  
ditions, etc. Box CM976, care of "The Commercial  
Motor." 799-380

**SKILLED** diesel fitters required for fleet of A.E.C.,  
Commer and Bedford vehicles. Permanent position  
with good wages. Bungalow accommodation available.  
Box CM9917, care of "The Commercial Motor." 800-x5021

**P.S.V.** Driver/mechanic, accommodation available.  
Details to White Lion Motorways, Wotton-  
u-Edge, Glos. 799-454

**COMPETENT** Diesel Mechanic required for Dorchester-  
Weymouth area, but willing if necessary to travel  
to different parts of the country to inspect and repair  
P.S. vehicles. Single man with no ties preferred. Please  
state wages required and experience. Box CM990, care  
of "The Commercial Motor." 800-7180

**EXPERIENCED** petrol and diesel fitter required for  
distributor in Croydon area. Good pay and con-  
ditions, etc. Box CM976, care of "The Commercial  
Motor." 799-380

**SKILLED** diesel fitters required for fleet of A.E.C.,  
Commer and Bedford vehicles. Permanent position  
with good wages. Bungalow accommodation available.  
Box CM9917, care of "The Commercial Motor." 800-x5021

**P.S.V.** Driver/mechanic, accommodation available.  
Details to White Lion Motorways, Wotton-  
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**SKILLED** diesel fitters required for fleet of A.E.C.,  
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to different parts of the country to inspect and repair  
P.S. vehicles. Single man with no ties preferred. Please  
state wages required and experience. Box CM990, care  
of "The Commercial Motor." 800-7180

**SITUATIONS WANTED**

**FOREMAN** Mechanic, experienced all types petrol and  
diesel, workshop administration, good disciplinary,  
requires change. Freeston, 54 St. Georges Square,  
London, S.W.1. 799-x5046

**STORAGE ACCOMMODATION**  
**SHEFFIELD** Extensive storage accommodation avail-  
able. Fork lift and crane facilities. Direct Motor  
Service (Sheffield), Ltd., Road Hauliers, Petre St. Sheffield  
386254. 222-883

**STORAGE** and distribution service available near Man-  
chester, centrally situated for Lancashire and York-  
shire areas, fork lift and crane facilities, 45,000 sq. ft.  
covered space and three acres open storage in totally  
enclosed premises. James H. Prescott and Son, Canal  
Warehouse, Manchester Rd., Heywood 69552. 803-7176

**TENDERS**

**COUNTY BOROUGH OF BLACKPOOL.**

**TENDERS** ARE INVITED FOR THE SUPPLY AND  
DELIVERY OF:—

**ONE 2-TON DIESEL-DRIVEN VEHICLE.**

**FITTED WITH WORKSHOP AND SIMON**

**HYDRAULIC PLATFORM.**

Specification and Form of Tender obtainable from The  
Illuminations and Public Street Lighting Officer, Rigby  
Rd. East, Blackpool.  
Returnable by 2.30 p.m. March 9, 1959.

**ERNEST C. LEE,**  
Town Clerk. 799-22  
863



### Miscellaneous Advertisements (contd.)

#### ULSTER TRANSPORT AUTHORITY.

CONTRACT FOR  
24 SINGLE-DECK DUAL-PURPOSE OMNIBUS BODIES.

THE AUTHORITY INVITES TENDERS FROM BODY-BUILDING FIRMS FOR:—

24 SINGLE-DECK DUAL-PURPOSE OMNIBUS BODIES

TO U.T.A. SPECIFICATION MOUNTED ON CHASSIS COLLECTED AT MANUFACTURER'S WORKS.

Tender forms, specifications and other details of the contract may be had on application to the Stores Superintendent, Duncrue St., Belfast.

Tenders will not be considered unless submitted on the Authority's Form of Tender.

Applications for Tender Forms, drawings and specifications must be accompanied by a deposit of £5, which will later be returned provided a bona fide tender is lodged (whether successful or not), or provided the Authority is satisfied there was some good reason for not lodging a tender.

Tenders should be sent to the Secretary, Ulster Transport Authority, 21 Linen Hall St., Belfast, in a sealed envelope endorsed "Tender for Single-deck Dual-purpose Omnibus Bodies," so as to be delivered not later than 5 p.m. on Monday March 16, 1959.

An official receipt must be obtained for each Tender delivered by hand. Tenders sent by post should be registered.

The Authority does not bind itself to accept the lowest or any Tender. 799-21

#### METROPOLITAN BOROUGH OF CAMBERWELL.

TENDERS ARE INVITED FOR SUPPLYING

(A) 7 HOUSE REFUSE COLLECTION VEHICLES

(SEDDON CHASSIS WITH EAGLE COMPRESSMORE BODY).

(B) 1 2-3-TON DIESEL LOW-LOADING TIPPING TRUCK.

- (C) 1 10-15-CWT. VAN.
- (D) 1 20-25-CWT. DUMPER.
- (E) 1 AUSTIN TAXI-TYPE SALOON CAR.
- (F) 4 MOBILE SITE HUTS.
- (G) 2 20-IN. RANSOME "MATADOR" MOWERS.
- (H) 1 VEHICLE-WASHING PLANT (SPRAY TYPE).

Tender forms on application to the Borough Engineer and Surveyor. Closing date Monday, March 2, 1959. 799-62

#### ULSTER TRANSPORT AUTHORITY.

CONTRACT FOR  
TWENTY-FOUR SINGLE-DECK OMNIBUS CHASSIS.

THE AUTHORITY INVITES TENDERS FROM CHASSIS MANUFACTURERS FOR:—

24 SINGLE-DECK OMNIBUS CHASSIS TO U.T.A. SPECIFICATION.

Tender forms, specifications and other details of the contract may be had on application to the Stores Superintendent, Duncrue Street, Belfast.

Tenders will not be considered unless submitted on the Authority's form of tender.

Applications for tender forms and specifications must be accompanied by a deposit of £5, which will later be returned provided a bona fide tender is lodged (whether successful or not), or provided the Authority is satisfied there was some good reason for not lodging a tender.

Tenders should be sent to the Secretary, Ulster Transport Authority, 21 Linen Hall Street, Belfast, in a sealed envelope endorsed "Tender for Omnibus Chassis," so as to be delivered not later than 5 p.m. on Monday, March 2, 1959.

An official receipt must be obtained for each tender delivered by hand. Tenders sent by post should be registered.

The Authority does not bind itself to accept the lowest or any tender. 799-20

### FOR FIFTH-WHEEL COUPLINGS AND TRAILER SUPPORT GEARS

contact  
**DAVIES MAGNET WORKS LTD.**  
LONDON ROAD, WARE, HERTS  
Ware 489 or Hertford 3334

### Miscellaneous Advertisements (contd.)

#### COUNTY OF THE WEST RIDING OF YORKSHIRE. TENDERS ARE INVITED FOR THE PURCHASE OF

NINE  
MORRIS PETROL UTILITY VANS  
(FLEET Nos. 1, 2, 3, 4, 5, 6, 7, 8 AND 9),  
BEING SURPLUS PLANT.

Forms of tender and further particulars may be obtained from the County Engineer and Surveyor, County Hall, Wakefield.

The County Council do not bind themselves to accept the highest or any tender.

Scaled tenders must be addressed to me, the undersigned, and received at Room 87, County Hall, Wakefield, not later than 12.00 noon on Wednesday, March 4, 1959.

County Hall, Wakefield. **BERNARD KENYON.**  
Clerk of the County Council. 799-170

### BOX NUMBER ADVERTISEMENTS

Are available to advertisers for an extra charge of 4/4.

REPLIES are forwarded immediately upon receipt and all received up to 5 p.m. despatched the same day.

URGENT REPLIES may be sent by telephone or telegram and readers should telephone Terminus 3636 for this service.

BOX NUMBERS should be copied accurately, printed clearly, and envelopes addressed correctly to "The Commercial Motor," Bowling Green Lane, London, E.C.1.

MONEY SHOULD NEVER BE ENCLOSED WITH REPLIES TO BOX NUMBER ADVERTISEMENTS

#### SHIPLEY URBAN DISTRICT COUNCIL.

I.C. AND C.I. FUELS—FINANCIAL YEAR 1959-60.

TENDERS ARE INVITED FOR THE SUPPLY OF FUELS FOR VEHICLES AND PUBLIC WORKS PLANT FOR THE 12 MONTHS COMMENCING APRIL 1, 1959.

ESTIMATED DEMAND:—  
PETROL (COMMERCIAL, IN BULK 500 GAL.), 18,500.  
PETROL (PREMIUM IN CANS, 100 GAL.), 1,200.  
DERV (BULK, 500 GAL.), 2,500.  
GAS OIL (BULK, 500 GAL.), 2,000.

Conditions of contract may be inspected at the office of the undersigned, to whom tenders, endorsed "TRANSPORT—TENDERS FOR FUEL SUPPLIES," should be submitted by Friday, March 6.

**ERNEST PEARS.**  
Clerk and Solicitor. 799-46

#### CITY OF LIVERPOOL FIRE SERVICE.

TENDERS INVITED FOR THE PURCHASE AND REMOVAL OF:—

ONE REDUNDANT FIRE ENGINE.

Tender forms from Chief Fire Officer, Haughton Garden, Liverpool, 3; returnable by March 9, 1959.

**THOMAS ALKER,**  
Town Clerk. (J.5513). 799-12

### TYRES!!! TYRES!!! TYRES!!!

**H. MATTHEWS LTD.**  
10,000 Tyres always in stock.  
ALL MAKES AND SIZES SAVE MONEY  
Phone, write or call  
87-97 Stockwell Road, London, S.W.9.  
BRIXTON 2826 (8 lines)  
Export Dept. 2027.

### Miscellaneous Advertisements (contd.)

SECOND-HAND plant for sale. Lorries, van, high lift shovels, 10-ton steam roller, trailers and sundry plant. Tender forms obtainable from the County Surveyor, County Offices, Aylesbury. Closing date for the receipt of tenders Saturday, March 14, 1959. 799-4

#### WORKS TRUCKS

WANTED, fork-lift truck. C. M. Keyworth, Ltd. Leek, Staffs. Phone, Leek 734. 799-7

### BOOKS AND PUBLICATIONS

**MAINTENANCE RECORD** (Charnwood Series No. 59). A life history of each vehicle with tyre records, petrol and oil consumption, 4s. 7d. post free.  
**DIESEL oil stock books.** Cost books, etc. Send for descriptive lists.  
**CHARWOOD PUBLISHING CO., LTD.** Coalville, Leicestershire. 222-941

**THE ROAD TRANSPORT ENGINEER.** Edited by G. Mackenzie Junner, Editor of "The Commercial Motor." This textbook deals with the specialized work of the engineer in the maintenance, repair and overhaul of commercial vehicles employed in transport of goods and passengers. Its contents range from the basic principles of vehicle maintenance to the economics of operation and embrace insurance, road transport law and management, 196 pages. Illustrated. 21s. net from booksellers, or 22s. by post from the publishers, Temple Press Limited, Bowling Green Lane, London, E.C.1. 222

**THE OPERATOR'S GUIDE TO THE TRANSPORT ACT, 1953.** Explains the process of denationalization of road goods transport and the rights and liabilities of road transport operators. 32 pages. 1s. 6d. net from booksellers, or 1s. 8d. by post from the publishers, Temple Press Limited, Bowling Green Lane, London, E.C.1. 222

**MOTOR VEHICLE MECHANICS' TEXTBOOK.** By F. K. Sully. Based on the syllabus of the City and Guilds of London Institute Examination in Motor Vehicle Service Mechanics Works, this book is designed for students entering for the award of the National Craftsman's Certificate in Automobile technology. It deals with many of the mathematical problems which a motor-vehicle mechanic is likely to encounter in his work. Illustrated. 237 pages. 12s. 6d. net from booksellers, or 13s. 4d. by post from the publishers, Temple Press Limited, Bowling Green Lane, London, E.C.1. 222

**"MODERN CLEANSING APPLIANCES."** by Ashley Taylor. A survey of the municipal cleansing field, consisting of the most up-to-date information on its practices and appliances both in Great Britain and abroad. Illustrated. 160 pages. 12s. 6d. net from booksellers, or 13s. 4d. by post from the publishers, Temple Press Limited, Bowling Green Lane, London, E.C.1. 222

**"THE COMMERCIAL MOTOR." TABLES OF OPERATING COSTS** (43rd Edition). Containing 11 operating cost tables for petrol, oil and electric goods and passenger-carrying vehicles. Explanatory articles provide an introduction to the Tables, show how to record tyre costs and explain a system of cost recording. New features in this edition are a useful address section and tables showing wage rates, the cost of fuel and a 10-year comparison of costs. 56 pages. 3s. net from booksellers, or 3s. 6d. by post from the publishers, Temple Press Limited, Bowling Green Lane, London, E.C.1. 222

**LONDON WHARVES AND DOCKS** (2nd Edition). A guide to the wharves and docks lining the river from Teddington to Gravesend. Detailed facilities afforded by the wharves together with 17 full-page maps showing their positions. A pull-out map, indicating the main routes to London's dock area is included. Illustrated. 104 pages. 7s. 6d. net (draminated paper board) and 6s. net (draminated card covers) from booksellers, or 8s. 2d. and 6s. 7d. respectively by post from the publishers, Temple Press Limited, Bowling Green Lane, London, E.C.1. 222

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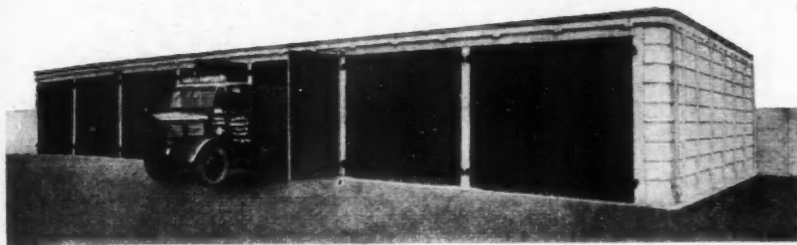


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